

**Clean Version of Public Hearing Draft  
Ordinance**

***Title 21 Text Amendment to Site Access Regulations***

**Public Hearing Draft  
PZC Case No. 2023-0011**

***Anchorage 2040 Land Use Plan  
Implementation Action 4-3***



# Clean Version of Proposed Amended Zoning Code

This version of the proposed amendments shows amended code sections as they would look in the published code book after final adoption. It includes indications of where sections of code are removed. For additional detail, please refer to the *Annotated (Narrated) Version of the Public Hearing Draft Ordinance* (Attachment 3).

The text amendments are arranged in the order they appear in Anchorage’s *Title 21 Zoning Ordinance*. Only the sections of Title 21 that are being added or amended are included. Where a full subsection of current code is deleted, a note to the reader in italics and parentheses indicates the proposed deletion.

Asterisks (\*\*\*) indicates that all intervening parts of the chapter are unchanged between the section header above the asterisks and the language shown below the asterisks.

Proposed new sections are marked “(New)” in the table of contents below:

Zoning Code Section Affected by Proposed Changes	Page
21.04. Zoning Districts ( <i>district-specific standards</i> )	1
21.07.010D. Alternative Equivalent Compliance ( <i>applicability</i> )	1
21.07.010E. Urban Neighborhood Development Context Area ( <i>new subsection E.</i> )	2
21.07.060E. Standards for Pedestrian Facilities ( <i>sidewalks and walkways</i> )	5
21.07.060F. Pedestrian Frontage Standard ( <i>new subsection F.</i> )	6
21.07.060G. Pedestrian Amenities Menu ( <i>renumbered from F to G; various amendments</i> )	8
21.07.090H.2. Parking and Loading Facility Design Standards – Applicability	13
21.07.090H.8. - 11. Vehicular Access and Circulation ( <i>renumbered to H.8. through H.11.</i> )	13
21.07.090H.12. Dimensions of Parking Spaces and Aisles ( <i>renumbered from H.9. to H.12.</i> )	17
21.07.110C. Residential Design Standards – Multifamily and Townhouse	18
21.07.110D. Residential Design Standards – Single-family and Two-family	18
21.07.110F. Residential Design Standards – Site Design ( <i>driveway width; alley access</i> )	18
21.15.020P. Rules of Construction and Interpretation ( <i>new subsection P.</i> )	18
21.15.040. Definitions	19

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1 **CHAPTER 21.04: ZONING DISTRICTS**

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3 **21.04.020 Residential Districts**

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5 **H. R-3A: Residential Mixed-Use District.**

6 \*\*\* \*\*

7 **2. District-Specific Standards**

8 \*\*\* \*\*

9 **e. Enhanced Street Sidewalk**

10 An enhanced street sidewalk may be provided in lieu of required sidewalks and site  
11 perimeter landscaping, as defined in 21.07.060G.20.

12 \*\*\* \*\*

13 **21.04.030 Commercial Districts**

14 \*\*\* \*\*

15 **G. Standards for Mixed-Use Development in the B-1A and B-1B Districts**

16 \*\*\* \*\*

17 **5. Enhanced Street Sidewalk**

18 An enhanced street sidewalk may be provided in lieu of required sidewalks and site perimeter  
19 landscaping, as defined in 21.07.060G.20.

20  
21 *(Note: Subsection 21.04.020G.7., Building Orientation and Placement, is deleted.)*

22  
23 **H. Standards for Mixed-Use Development in the B-3 District**

24 \*\*\* \*\*

25 *(Note: Subsections 21.04.020H.3., Building Placement and Orientation, is deleted.)*

26  
27 **3. Enhanced Street Sidewalk**

28 An enhanced street sidewalk may be provided in lieu of required sidewalks and site perimeter  
29 landscaping, as provided in 21.07.060G.20.

30 \*\*\* \*\*

31 **21.04.070 Overlay Zoning Districts**

32 \*\*\* \*\*

33 **C. Commercial Center Overlay**

34 \*\*\* \*\*

35  
36 **5. Dimensional Standards**

37 Uses in the B-1B district are exempt from the gross floor area limitations of subsection  
38 21.04.030C.2.b., provided that an individual use occupies no more than 60,000 square feet of gross  
39 floor area.

40  
41 *(Note: Subsection 21.04.070C.5.a. is deleted.)*

42 \*\*\* \*\*

43 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

44 \*\*\* \*\*

45 **21.07.010 General Provisions.**

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47 **D. Alternative Equivalent Compliance.**

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2. *Applicability.* The alternative equivalent compliance procedure shall be available only for the following sections of this title:

\*\*\*      \*\*\*      \*\*\*

c. Subsection 21.07.060F., Pedestrian Frontage Standard;

d. Subsection 21.07.060G., Pedestrian Amenities;

(Note to code revisor: re-number subsections c. - i. as subsections d.- j.)

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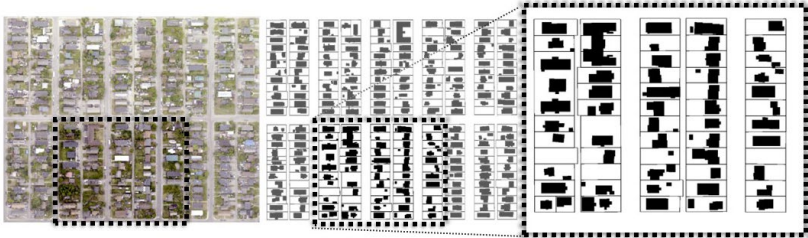
**E. Urban Neighborhood Development Context Area.**

**1. Purpose.**

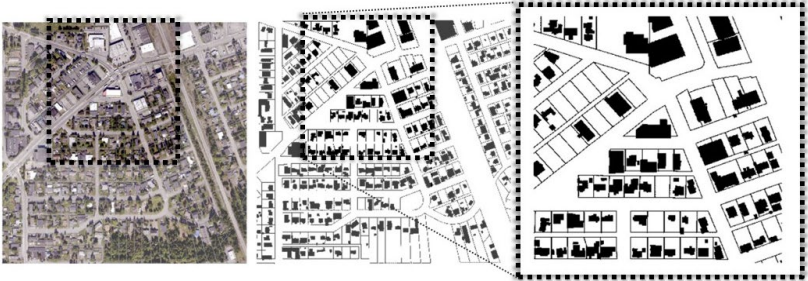
This section establishes an Urban Neighborhood Development Context Area that reflects the existing and desired characteristics of Anchorage’s older, urban neighborhoods identified by the Comprehensive Plan, Land Use Plan Map as Traditional Neighborhood Design areas. Some are also identified in neighborhood and district plans. This section provides a basis for development regulations tailored to the characteristics of these neighborhoods and fulfills their role as recommended in the Comprehensive Plan.

a. This area is intended to include Anchorage’s original urban neighborhoods of Government Hill, South Addition, and Fairview, and post-war era (1950s-1960s) neighborhoods such as Spenard, Airport Heights, and Russian Jack Park. It also includes transit-supportive development corridors and designated mixed-use centers.

b. This area recognizes urban neighborhood characteristics including a more highly interconnected street system, smaller lot and block sizes, and access to pedestrian facilities and public transit. Some neighborhoods have consistent or frequent sidewalks and alleys. Buildings and residences often have moderate front setbacks and orient to the street, with landscaping or walkways (instead of parking) comprising most of their street frontage. Transit-supportive development corridors and mixed-use centers contribute to a compact, accessible land use pattern.



*Regular street grid of city blocks and alleys in Mountain View.*



*More relaxed street grid along a transit corridor in Spenard.*

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**2. Applicability.**

The urban neighborhood development context area shall apply as a basis for area-specific development standards in this chapter 21.07. It shall not be used as a basis to change allowed uses or review and approval procedures of the underlying zoning district, district-specific dimensional standards, or any other standards elsewhere in this title.

**3. Urban Neighborhood Development Context Area Established.**

The urban neighborhood development context area is delineated in map 21.07-1, with the following clarifications:

- a. The Downtown (DT) zoning districts are not included because they are addressed in chapter 21.11.
- b. The university and medical campuses of the UMED District are not included.
- c. The properties of Merrill Field Airport, Alaska Railroad Terminal Reserve, and Port of Alaska are not included.
- d. Broadmore Estates, Benzen Addition, and Creekside Park Addition Subdivisions are not included.
- e. Birchwood Park and Green Acres Subdivisions are included.



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1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

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3 **21.07.060 Transportation and Connectivity**

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5 **E. Standards for Pedestrian Facilities**

6 \*\*\* \*\*

7 **2. Sidewalks**

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9 a. All sidewalks shall be designed to comply with the standards of the Design Criteria Manual (DCM) and Municipality of Anchorage Standard Specifications (MASS) and shall be improved in accordance with subsection 21.08.050H.

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11 b. Street improvement projects and new streets in subdivisions in all class A zoning districts except for industrial districts shall install sidewalks on both sides of all streets (local, collector, arterial, public or private, including loop streets). Where indicated in the comprehensive plan, a pathway may replace a sidewalk on one side. Street improvement projects in industrial zoning districts shall install a sidewalk on one side of all local streets, and on both sides of local streets if the new sidewalks would connect to existing sidewalks on both ends and the needed sidewalk length is no greater than one quarter mile.

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13 \*\*\* \*\*

14 f. Development on lots along existing streets in class A zoning districts shall install sidewalks on all lot frontages abutting streets in the following situations:

15 i. In R-4, R-4A, commercial, and DT zoning districts.

16 \*\*\* \*\*

17 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

18 \*\*\* \*\*

19 **21.07.060 Transportation and Connectivity**

20 \*\*\* \*\*

21 **E. Standards for Pedestrian Facilities**

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23 **4. On-Site Pedestrian Walkways**

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25 **b. On-Site Pedestrian Connections**

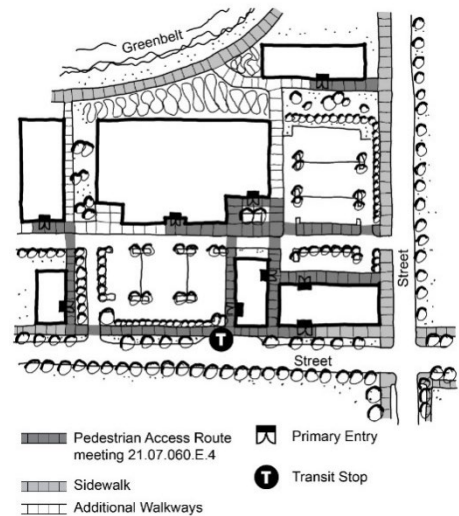
26 \*\*\* \*\*

27 ii. All primary building entrances on a site shall be connected to the street by a convenient system of walkways. This includes multiple primary entrances into one building, and primary entrances in separate buildings on a site.

28 \*\*\* \*\*

29 iv. The primary front entrance of a residential dwelling shall be connected to the street by a walkway as provided in i. through ii. above, or by the dwelling's individual driveway, or by a shared parking courtyard meeting 21.07.060G.21.

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**c. Walkway Clear Width and Improvements**

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- i. The minimum width of a required pedestrian walkway shall be five feet of unobstructed clear width, except where otherwise stated in this title. A walkway that provides access to no more than four residential dwelling units may provide an unobstructed clear width of three feet.
- ii. Walkways shall be improved in accordance with subsection 21.08.050H.

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**F. Pedestrian Frontage Standard**

**1. Purpose**

The pedestrian frontage standard requires site planning and building orientation toward neighborhood streets and sidewalks to facilitate pedestrian access and reduce automobile parking congestion. Objectives include to:

- a. Organize and orient buildings around public streets and associated frontages in a way that frames streets as positive public space, promotes pedestrian activity, and connects to multiple modes of transportation.
- b. Provide clearly defined, safe pedestrian access to building entries that invites people of all abilities and minimizes conflicts with vehicles and parking.
- c. Place active indoor spaces, entrances, and windows on street-facing building facades to improve the visual connection to the street and promote a safe, secure neighborhood.

**2. Applicability**

Subsection 3. applies to development in the urban neighborhood development context area established in 21.07.010E. Subsection 4. applies to development in the other areas of the municipality. The following are exempt:

- a. Changes of use and other developments that comprise building modifications of less than 50 percent of the total improvement value of the building(s) on the site.
- b. Development in Girdwood, Downtown (DT), CE-DO, CE-EVO, PLI, PR, TA, or industrial zoning districts.
- c. Single-family and two-family dwellings constructed prior to January 1, 2016, or on lots of 20,000 square feet or larger, or in Class B zoning districts.
- d. Uses without habitable floor area, such as utility substations.

**3. Administrative Adjustments**

- a. The Alternative Equivalent Compliance procedure in 21.07.010D. may be used to propose alternative means of complying with the intent of this section.
- b. The director shall approve administrative relief if the applicant demonstrates the adjustment is necessary to compensate for some practical difficulty of the site. This includes reducing the window area requirement by the amount needed to comply with 5-star or affordable housing (21.15.040) energy rating requirements. The department shall keep record of the approved exception with written findings supporting the exception on file.

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**4. Standard or urban neighborhood contexts**

The standards of table 21.07-2 apply to the primary frontage and one secondary frontage.

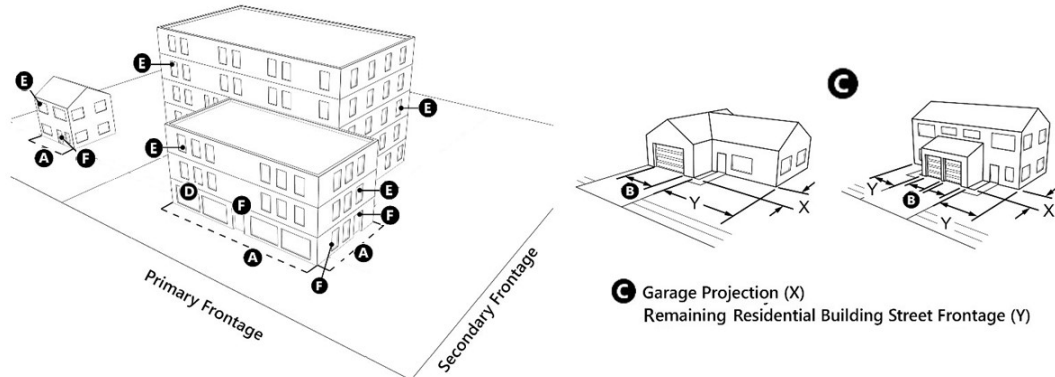


Table 21.07-2: Pedestrian Frontage Standard for Urban Neighborhood Contexts	
Site Elements for Pedestrian-Oriented Frontages	Standard
A. Minimum required ground-floor, street-facing building elevation without driveways or parking facilities in front	50% of building elevation width <sup>1</sup> Lots with alley access: no parking allowed in front of building except in driveways for individual dwellings
B. Maximum allowed width of residential garage entrance(s) on ground-floor street-facing building elevation	40% of building elevation width <sup>1</sup>
C. Maximum allowed distance a garage may project out in front of the rest of the street-facing residential building elevation	None allowed on lots with alley access No more than 50% of the width of the non-garage portion of the street-facing building, on other lots
D. Minimum required visual access windows or primary entrances on non-residential ground-floor street-facing building elevation <sup>2</sup>	25% of wall area on primary frontage 15% of wall area on second frontage
E. Minimum required windows or primary entrances on residential and upper-floor non-residential street-facing elevations <sup>2</sup>	15% of wall area on primary frontage 10% of wall area on second frontage
F. Requirement for primary entrance facing the street or visible from the street via an unobstructed line of sight	Residential: Meet 21.07.060G.14., Covered, Visible Residential Entrance Other uses: at least one entrance
G. Minimum required number of pedestrian amenities from 21.07.060G. (in addition to G.14.) in multifamily, mixed-use, townhouse, group housing, and non-residential developments.	2 pedestrian amenities
<sup>1</sup> Exception: On lots without alley access in R-2A, R-2D, and R-2M zoning districts, two-family (duplex) developments may have a minimum of 25% in A. above and a maximum of 67% in B. above. <sup>2</sup> Visual access windows shall have a sill height of no more than four feet above finished grade. Rules for measuring window area as a percentage of building wall area are provided in 21.15.020P. Visual access windows ("Window, providing visual access"), primary entrance, and ground-floor wall area are defined in 21.15.040.	

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- 5. **Standard for areas outside of urban neighborhood contexts**  
The standards of table 21.07-3, apply to the primary frontage and one secondary frontage.

<b>Table 21.07-3: Pedestrian Frontage Standard – Outside of Urban Neighborhood Contexts</b>	
Site Elements for Pedestrian-Oriented Frontages	Standard
A. Minimum required ground-floor, street-facing building elevation without driveways or parking facilities in front	20% of building elevation width, on one frontage
B. Maximum allowed width of residential garage entrance(s) on ground-floor street-facing building elevation	67% of building elevation width
C. Maximum allowed distance a garage may project out in front of the rest of the street-facing residential building elevation	No more than the width of the non-garage portion of the street-facing building elevation
D. Minimum required visual access windows or primary entrances on non-residential ground-floor street-facing building elevation <sup>2</sup>	15% on primary frontage 10% on secondary frontage
E. Minimum required windows or primary entrances on residential and upper-floor non-residential street-facing elevations <sup>2</sup>	10% on primary frontage 5% on secondary frontage
F. Requirement for primary entrance facing the street or visible from the street via an unobstructed line of sight	Residential: Meet 21.07.060G.14., Covered, Visible Residential Entrance. Other uses: at least one entrance.
G. Minimum required number of pedestrian amenities from 21.07.060G. (in addition to G.14.) in multifamily, mixed-use, townhouse, group housing, and non-residential developments.	1 pedestrian amenity
<sup>1</sup> Exception: Non-residential buildings located more than 100 feet from the applicable street rights-of-way are exempt from A., D., E., and F. above. <sup>2</sup> Visual access windows shall have a sill height of no more than four feet above finished grade. Rules for measuring window area as a percentage of building wall area are provided in 21.15.020P. Visual access windows (“window, providing visual access”), primary entrance, and ground-floor wall area are defined in 21.15.040.	

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**G. Pedestrian Amenities Menu**

**1. Purpose**

This section defines and provides standards for pedestrian amenities that are used in developments to satisfy a requirement, menu choice, or incentive in this title. The standards in this section give predictability for applicants and the public for the minimum acceptable standards for pedestrian amenities. It encourages amenities that will improve and enhance the community and respond to Anchorage’s northern latitude climate.

**2. Applicability**

Pedestrian amenities shall meet the minimum standards of this section to be credited toward a requirement, menu choice, or incentive of this title, except where specifically provided otherwise in this title. The standards of this section do not apply to amenities that are not counted toward a requirement, menu choice, or incentive under this title.

**3. Administrative Adjustments**

The alternative equivalent compliance procedure set forth in subsection 21.07.010D. may be used to propose alternative means of complying with the standards of this subsection 21.07.060G.

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2 (Note: Current subsection 3., Walkway, is deleted.)  
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4 **4. Enhanced On-Site Walkway**

5 An enhanced on-site walkway is intended to provide an option for applicants to receive credit for  
6 exceeding the minimum development standards for walkways and improving pedestrian  
7 convenience, comfort, and safety on the site. Enhanced on-site walkways provide additional width  
8 for pedestrian movement and peripheral space that accommodates landscaping, furniture, and  
9 utilities.

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- 11 **a.** An enhanced on-site walkway shall have a pedestrian movement zone with a continuous,  
12 unobstructed walkway clear width of at least eight feet, or six feet where providing access  
13 only to four or fewer residential dwelling units. Where adjacent to a ground-floor building  
14 elevation it shall also have a building interface zone a minimum of two feet in width for  
15 building foundation landscaping or space for opening doors or seating and transition  
16 pedestrian spaces. In addition to the pedestrian movement zone and any building interface  
17 zone, the enhanced on-site walkway shall have a buffer space of at least two feet in width  
18 where abutting motor vehicle parking lots, circulation aisles, or driveways. The buffer space  
19 shall accommodate landscaping beds, fencing or bollards, light poles, utilities, benches,  
20 and other furnishings.
  - 21
  - 22 **b.** A minimum of two pedestrian features as defined by this title (21.15.040) shall be provided  
23 along the enhanced walkway, with at least one for every 50 feet of the walkway length.
  - 24
  - 25 **c.** Enhanced on-site walkways shall provide continuous, direct connections from building  
26 primary entrance(s) to surrounding public streets and sidewalks and be publicly  
27 accessible or available to all residents of the development.

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2 (Note: Pedestrian Amenities G.4. through G.13 are not shown because they are not being amended.  
3 Existing subsection 15, Bicycle Parking Amenities, is deleted.)  
4

5 **13. Separated Walkway to the Street**

6 The development shall connect the building primary entrances to the street with a clear and direct  
7 walkway that is not routed through a parking facility or across vehicle driveways or circulation aisles.  
8 The minimum clear width portion of the walkway shall be separated from the parking facility by at  
9 least five feet.

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11 **14. Covered, Visible Residential Entrance**

12 A porch, stoop, or landing sheltered by a roof is intended to give visual emphasis to the building  
13 entrance as an aid in wayfinding, and help provide safe, convenient access to residential buildings  
14 from the street. The entrance shall meet the following standards:  
15

- 16 a. The entrance shall incorporate a porch, stoop, or landing with an internal dimension of at  
17 least 16 square feet, and a permanent, sheltering roof covering at least 12 square feet.
- 18 b. The porch, stoop, or landing shall be distinguished from adjoining areas and vehicle  
19 parking by vertical separation or a change in surfacing material.
- 20 c. The building entrance shall also be visible (via an unobstructed line of sight) from a street  
21 or face a common private open space (21.07.030), a plaza or courtyard (21.07.060G.6.),  
22 a housing courtyard (21.07.060G.7.), or a shared parking courtyard (21.07.060G.21.) that  
23 is visible from a street.  
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27 **15. Enhanced Primary Entrance**

28 An enhanced building entry is intended to provide an option for applicants to receive credit for a  
29 more prominent and inviting primary pedestrian entrance. To receive credit, the enhanced primary  
30 entrance shall incorporate at least three of the following features:  
31

- 32 a. Outdoor sheltering roof feature projecting from the building façade such as an overhang,  
33 portico, canopy, marquee with an inside dimension of at least 16 square feet;
- 34 b. Recessed and/or projected entrance or other building wall modulation with projections or  
35 recesses in the building wall plane;
- 36 c. Changes in the building’s main roofline such as arches, peaked roof forms, or terracing  
37 parapets;
- 38 d. Changes in siding material or exterior finishes, or façade detail features such as tilework  
39 that emphasize the entrance;
- 40 e. Entrance plaza, patio, or similar common private open space;
- 41 f. Landscaping not otherwise required by this title, such as integrated planters, landscape  
42 accent lighting, or special paving treatments; or
- 43 g. One or more pedestrian features (21.15.040) such as pedestrian-scale lighting or seating.  
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51 **16. Enhanced Façade Transparency.**

52 Increase the windows and/or primary entrances on street-facing building elevations which are  
53 subject to the window requirement in 21.07.060F., by an additional five (5) percent of the wall area.  
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1           **17. Pedestrian Frontage Free of Parking in Front.**

2           In urban neighborhood development contexts (21.07.010E.), provide a street frontage without  
3           driveways, parking, or loading facilities in front of at least 75% of the ground-floor, street-facing  
4           building elevation. In other areas, provide such a frontage in front of at least 33% of the street-  
5           facing building elevation.  
6

7           **18. Site Entry Feature**

8           Highlight and define a pedestrian entrance to a development site using three or more of the  
9           following elements:

- 10           **a.**       Landscape treatment with seasonal color and trees, which clearly distinguishes and  
11                   highlights the site entry.
- 12           **b.**       Plaza or courtyard as described in subsection 21.07.060G.6.
- 13           **c.**       Identifying building primary entrance form including a covered entry, when the primary  
14                   entrance is within 50 feet of the site entrance.
- 15           **d.**       Special paving, pedestrian walkway area lighting, ornamental lighting, and/or bollards.
- 16           **e.**       Ornamental gate and/or fence.

17           **19. Pedestrian-Interactive Building**

18           A pedestrian-interactive building is intended to provide interior spaces that engage the sidewalk  
19           with street-facing windows and entrances and activities and services that support neighborhood  
20           residents, and contribute to the activity level and quality of the pedestrian environment of the  
21           neighborhood or district.  
22

- 23           **a.**       A pedestrian-interactive building shall provide a primary entrance facing the street.  
24                   Entrances at building corners facing a street may be used to satisfy this requirement.
- 25           **b.**       A pedestrian-interactive building shall contain habitable floor area at least 24 feet deep  
26                   extending along a minimum of 50 percent of the length of the ground-floor, street-facing  
27                   building elevation in Urban Neighborhood Contexts, and 30 percent elsewhere. The  
28                   habitable floor area may include pedestrian entrances, entry lobbies or atriums, and  
29                   stairwells.
- 30           **c.**       At least 50 percent of the street-facing building elevation width of a pedestrian-interactive  
31                   building shall have no parking facilities in front.
- 32           **d.**       Street-facing wall areas of a non-residential use shall be 50 percent visual access windows  
33                   or primary entrances on the ground floor, and 20 percent windows above the ground floor.
- 34           **e.**       Street-facing wall areas for residential uses shall be at least 20 percent windows or primary  
35                   entrances.
- 36           **f.**       Where a building has three or more street frontages, these criteria apply along only two of  
37                   the frontages.

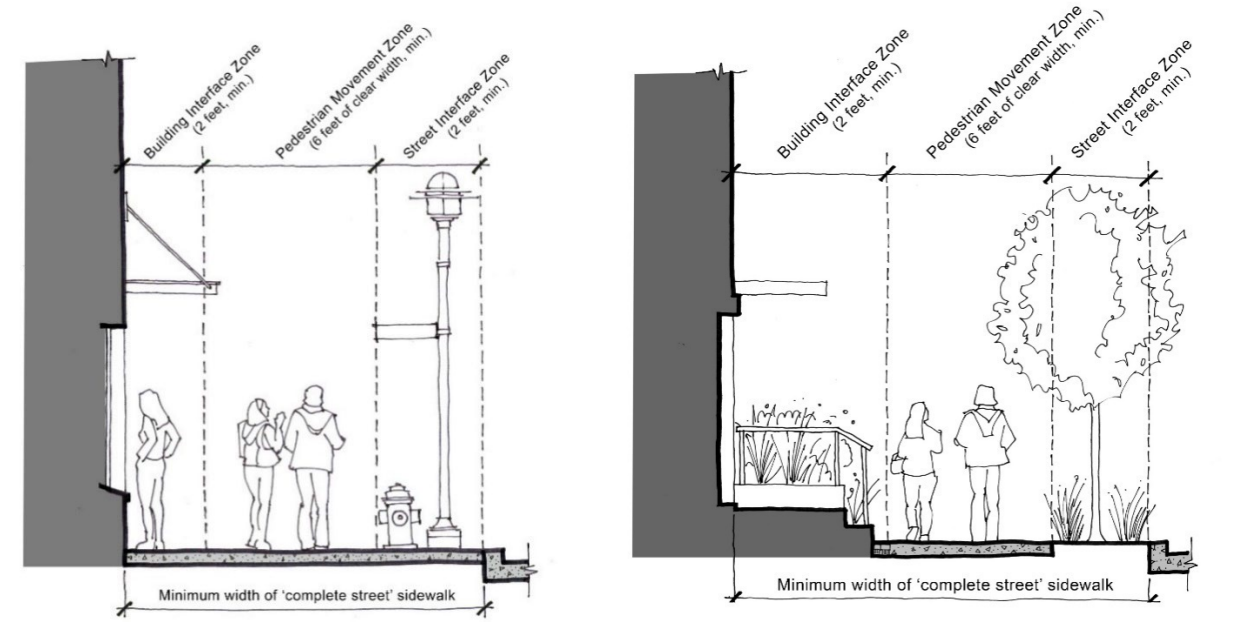
38           **20. Enhanced Street Sidewalk**

39           An enhanced street sidewalk is intended to provide an option for applicants to receive credit for  
40           exceeding the minimum development standards for sidewalks. An enhanced street sidewalk  
41           promotes sidewalk widening and streetscape enhancements to support higher levels of pedestrian  
42           activity, comfort, and safety in the district. An enhanced street sidewalk with “complete street”  
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(21.15.040) style amenities may be provided in lieu of required site perimeter landscaping as determined through an administrative site plan review, and subject to the following:

- a. The enhanced street sidewalk cross-section shall be at least 12 feet wide, and include a pedestrian movement zone, building interface zone, and street interface zone (21.15.040). The pedestrian movement zone shall have a walkway clear width of at least six feet. The street interface zone shall be at least two feet wide from back-of-curb, and four feet wide along major arterials. Where a building adjoins the sidewalk, there shall be a building interface zone at least two feet wide.

(Note: Existing illustration replaced with the two illustrations below.)



*Enhanced Street Sidewalks in Commercial and Residential Settings*

- b. The enhanced street sidewalk shall provide at least half the number of trees and shrubs that would otherwise have been required for site perimeter landscaping. Shrubs are not required if perennials are substituted for shrubs on a three to one basis or if the tree planting bed is provided as a suspended pavement system with a minimum of 300 cubic yards of soil per tree.
- c. The enhanced street sidewalk may be placed wholly or in part within a right-of-way, subject to approval of the traffic engineer and municipal engineer.
  - i. The enhanced street sidewalk shall be subject to the applicable requirements of title 24, including sections 24.30.020., *Permit to use Public Places*, and 24.90, *Encroachment Permit*.
  - ii. Existing improvements that meet the standards of the enhanced street sidewalk may be counted towards the requirements of this section, subject to approval by the director.
  - iii. The owner shall maintain landscaping and amenities for the enhanced street sidewalk within the right-of-way.



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iv. Where the right-of-way is not adequate or cannot be configured to accommodate the enhanced street sidewalk, then the development shall be set back from the street frontage as necessary to accommodate part of the improvements within the property. A public use easement shall be recorded for any part of the designated sidewalk width to be located within the subject parcel.

\*\*\* 20. Shared Parking Courtyard \*\*\*

1 **21.07.090 Off-Street Parking and Loading**

2  
3 **H. Parking and Loading Facility Design Standards**

4 \*\*\* \*\*

5  
6 2. *Applicability.* These standards apply to all areas used for off-street parking, and any parking facility  
7 or loading facility in a development, except where stated otherwise. A temporary parking lot shall  
8 comply with all applicable development requirements of this title for surface parking lots and parking  
9 lot landscaping, except when associated with another temporary use permitted pursuant to section  
10 21.05.080.

11  
12 **8. Vehicular Access and Circulation – General**

13 Parking lots and structures access, layout, and dimensions shall be designed for a safe and orderly  
14 flow of traffic throughout the site, as provided in subsections 9. through 12. that follow. Vehicular  
15 access and circulation patterns and the location and traffic direction of all circulation aisles,  
16 driveways, and queuing lanes shall be designed and maintained in accordance with accepted  
17 principles of traffic engineering and safety as determined by the traffic engineer.

18  
19 *(Note: Existing subsection 8.a. Key Elements, 8.b. Circulation Patters, and 8.c., Parking Spaces*  
20 *Along Major Entrance Drives, are deleted.)*

21  
22 **9. Vehicular Access Location**

23  
24 **a. Street Access Location**

25 The number and spacing of driveways, including minimum distance from street  
26 intersections, shall be as provided in the *Municipal Driveways Standards* and this section.  
27 Access to streets owned by the state of Alaska requires compliance with state driveway  
28 standards, department of transportation and public facilities approval and driveway permit.

29  
30 **b. Alley Access Requirement**

31 Where a residential use is served by an alley in the Urban Neighborhood Development  
32 Context (21.07.010E.), vehicle access shall take place from the alley, except for direct  
33 vehicle access to the street that is limited to the following:

- 34  
35 i. One driveway no wider than 12 feet at any point, except that lot frontages 100 feet  
36 or wider on a primary street frontage may have one driveway per 50 feet of lot  
37 frontage on that street.
- 38  
39 ii. One additional driveway no wider than 12 feet providing access to the secondary  
40 street frontage on corner lots for multifamily, townhouse, or two-family  
41 developments.
- 42  
43 iii. The director and traffic engineer may approve additional driveway access to the  
44 primary street frontage for multifamily or townhouse developments, provided the  
45 alley is unimproved and the traffic engineer determines that alley improvement and  
46 vehicle access are not feasible or would create a traffic impact or safety hazard.  
47 The additional driveway access shall be the minimum the traffic engineer  
48 determines is necessary to provide access for the development. The department  
49 shall keep record of the approved additional driveway access with written findings  
50 supporting the approval on file and available for public inspection.

51  
52 **c. Cross-Access and Shared Access with Adjacent Sites**

53 Parking facilities serving a site, whether located on that same lot or on an adjacent lot, may  
54 be connected by means of a common access driveway within or between the interior of  
55 such lots. Where a property receives its access to the street through a shared driveway

1 with another lot, a shared access easement shall be provided on the plat, or a shared  
2 access agreement running with the land shall be recorded by the municipality, as approved  
3 and executed by the director, guaranteeing the continued availability of the shared access  
4 between the properties.  
5

6 **d. Paired Residential Driveways Allowed**

7 Driveways for two single-family, two-family, or townhouse units may be attached across a  
8 property line, provided the maximum combined width of the driveway is 24 feet.  
9

10 **e. Residential Garage Setback**

11 Street-facing garages accessed from the driveway of an individual dwelling shall be set  
12 back from the street by the length of a standard parking space (as defined in table 21.07-  
13 8, Parking Angle, Stall, and Aisle Dimensions). The traffic engineer may approve  
14 exceptions to relieve hardship on narrow lots.  
15

16 **10. Access to Parking Spaces**

17 **a. Access to Parking Spaces**

18 Each parking space shall open to a parking aisle or driveway of such width and design as  
19 provided in section 21.07.090H.12. to provide safe and efficient means of vehicular access  
20 with no more than a standard two-movement entrance or exit from the parking space and  
21 without having to move another vehicle. Stacked and tandem parking spaces are allowed  
22 exceptions as provided in 21.07.090H.12.  
23

24 **b. Backing Distances**

25 Adequate ingress and egress to each parking space shall be provided without backing  
26 more than 25 feet.  
27

28 **c. Dead-End Parking Aisles**

29 Any parking bay that does not provide two means of vehicle egress shall provide, at the  
30 closed end, an extension of the parking aisle at least nine feet in depth, designated and  
31 signed as a "No Parking" turn-around area. Dead-end parking bays longer than 100 feet  
32 shall be subject to discretionary review and approval by the traffic engineer.  
33

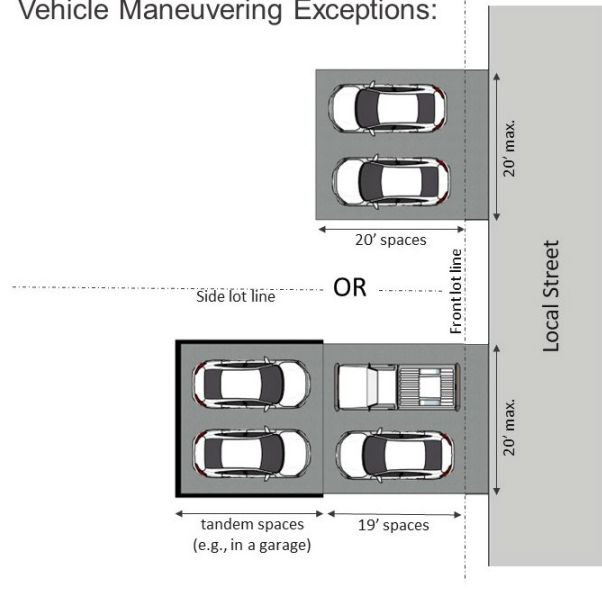
34 **d. Vehicle Maneuvering**

35 Off-street parking facilities shall be designed so that all vehicle maneuvers occur on the  
36 development site and not in the public right-of-way, and vehicles enter and exit the right-  
37 of-way in a forward motion, except the following are exempt:  
38

39 **i.** Parking for single-family, two-family, and mobile home dwellings on individual lots,  
40 accessing a local street.  
41

42 **ii.** For other developments, parking comprising only one or two parking spaces whose  
43 only access is to a local street, provided that the vehicle area occupies no more  
44 than 20 feet of the lot frontage in the front setback. The number of spaces may be  
45 increased to four spaces if arranged in tandem for residential dwellings as provided  
46 in 21.07.090K.12.k. Additional spaces for multifamily developments with up to four  
47 dwelling units may be approved by the traffic engineer in appropriate  
48 circumstances such as lots on dead-end streets, cul-de-sacs, or other local streets  
49 with low traffic volumes.  
50  
51  
52

Vehicle Maneuvering Exceptions:

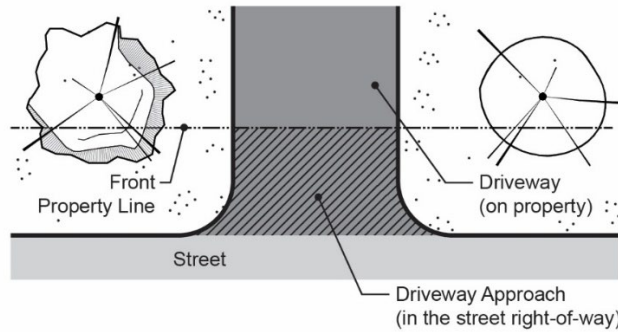


- iii. The usable portion of an alley may be credited as circulation and/or parking aisle space for parking areas. The maneuvering area between the end of the parking space and the opposite side of the improved alley shall meet the parking aisle width in table 21.07-8, *Parking Angle, Stall, and Aisle Dimensions*.

11. Driveway Design and Dimensions

a. Parking Lot Entries/Driveway Approaches

Entries and driveway approaches providing access from the street edge to the front property line shall conform to the municipal driveway standards and this section 21.07.090H.11. Access to streets owned by the state of Alaska requires compliance with state driveway standards as provided in 21.07.090H.9.



b. Curb Openings and Public Walkway Crossings

- i. Curb cut and curb returns at driveway openings to the street shall be provided as prescribed in the municipal driveway standards.
- ii. Public walkways shall be maintained or restored to the maximum running slope and cross-slope prescribed by M.A.S.S. and A.D.A., except that in the urban

neighborhood context areas (21.07.010E.), public walkways on local streets shall be restored to a level running grade to the extent reasonably feasible.

**c. Driveway Approach (in ROW) as a Percentage of Lot Frontage Width**

The total width of the driveway approach from a street shall not exceed 40 percent of the frontage of the lot, or 33 percent of the frontage if the platting authority or traffic engineer finds that conditions warrant it, provided the following:

- i. The driveway approach is always allowed to have the minimum driveway width provided by subsection d. The traffic engineer may approve more than the minimum driveway width, provided the traffic engineer determines that snow storage, traffic flow and safety, and the neighborhood context are addressed.
- ii. The driveway approach shall not exceed the maximum driveway width established in the municipal driveway standards.
- iii. Flag lots are exempt from the percentage limitations but shall have a maximum driveway approach width of 20 feet. Abutting flag lots may share a driveway approach up to 24 feet wide (12 feet per lot).

**d. Minimum Driveway Width**

The minimum required width of driveways including both the driveway approach within the street ROW and the portion of the driveway on the development property is as follows:

- i. The minimum width of parking aisles shall be as set forth in 21.07.090H.12.
- ii. The minimum width of the driveway approach shall be as set forth in the municipal driveway standards, except as modified or clarified by subsections iii. through vi.
- iii. The minimum width of a driveway providing access to a single-family, two-family, townhouse, mobile home, or other individual dwelling from a local street or alley is 12 feet.
- iv. The minimum width of a driveway providing access for up to 10 parking spaces serving a townhouse or multifamily residential use from a local street or alley is 12 feet, except that straight (non-curving) driveway segments on the development property (i.e., not in the driveway approach) may be 10 feet.
- v. The minimum width of a driveway providing access for more than 10 parking spaces serving a townhouse or multifamily use from a local street or alley is 12 feet for one-way traffic and 20 feet for two-way traffic.
- vi. The traffic engineer may reduce the driveway width to relieve hardship on small or narrow lots or increase the required width to meet vehicle turning and maneuvering needs, in accordance with accepted principles of traffic engineering and safety as determined by the traffic engineer.

**e. Driveway Throat Length**

The driveway throat shall be of sufficient length to enable the street and walkways in the ROW and the circulation, parking, and walkways in the development site to function without interfering with each other, as provided in the municipal driveway standards.

**f. Sight Distance**

Ingress and egress to parking facilities shall be designed as prescribed in the municipal driveway standards to maintain adequate sight distance and safety.

1 **g. Circulation Definition**

2 Circulation patterns within parking facilities shall be well defined with pavement marking  
 3 and signage, curbs, landscaping, and/or other similar features. Curbed end islands shall  
 4 be provided at the end of each row of parking spaces to define circulation and provide sight  
 5 distance at internal intersections of parking aisles, driveways, and/or on-site roadways.  
 6 Shared parking courtyards meeting 21.07.060G.21. may depart from this requirement  
 7

8 *(Note: Existing subsections 21.07.090H.8.e., Parking and Maneuvering, through 21.07.090H.8.h., Cross*  
 9 *Access and Joint Access with Adjacent Sites, are deleted.)*

10  
 11 **12. Dimensions of Parking Spaces and Aisles**

12  
 13 **a. Minimum Dimensions for Required Parking**

14 Parking shall meet or exceed the STANDARD parking space and aisle dimensions set forth  
 15 in table 21.07-8, except as follows:

- 16  
 17 i. Parking facilities in the Downtown (DT) zoning districts, and in the Urban  
 18 Neighborhood Contexts delineated in subsection 21.07.010E., may meet the  
 19 SMALL parking space and aisle dimensions in table 21.07-8. Up to 30 percent of  
 20 parking in the Downtown (DT) zoning districts may meet the COMPACT parking  
 21 dimensions, if signed for compact vehicles only.  
 22  
 23 ii. In all other areas of the municipality, up to 50 percent of employee-only parking,  
 24 structured parking facilities, or parking for uses with a low parking turnover rate  
 25 may meet the SMALL parking space and aisle dimensions in table 21.07-13. Uses  
 26 with a low parking turnover rate comprise multifamily and mixed-use household  
 27 living uses, office uses, and educational facility uses other than instructional  
 28 services. Required parking shared between low-turnover and non-low-turnover  
 29 uses shall meet STANDARD dimensions.  
 30

TABLE 21.07-8 PARKING ANGLE, STALL, AND AISLE DIMENSIONS									
Parking Angle (Degrees)	Parking Space Size	Space Width	Curb Length (Width Projection)	Space Depth (Vehicle Projection)	Aisle Width 1-way	Aisle Width 2-way	Typical Parking Bay Width (Module)	Interlock Reduction	Overhang Allowance
***	***	***							
<sup>1</sup> Developments in the Urban Neighborhood Development Context Areas delineated in 21.07.010E. may use the following alternative STANDARD parking space depth (Vehicle Projection): 19' 0" for 90-degree parking angle; 20' 8" for 75-degree parking angle; 20' 11" for 60-degree parking angle, and 19' 6" for 45-degree parking angle. Parking bay width may decrease as a result.									

31 \*\*\* \*\*

32  
 33 **21.07.110 Residential Design Standards**

34  
 35 **A. Purpose**

36  
 37 *(Note: Subsections 4. and 5. of the purpose statement for the residential design standards are deleted,*  
 38 *and the remaining subsections re-numbered.)*

39 \*\*\* \*\*

40 **C. Standards for Multifamily and Townhouse Residential**

41  
 42 **1. Purpose**

43 \*\*\* \*\*

(Note: Subsections c. and g. of the purpose statement for Multifamily and Townhouse Residential design standards are deleted, and the remaining subsections re-numbered.)

\*\*\* \*\*

**3. Pedestrian-Oriented Street Frontage Standards.**

Provisions for site planning and building orientation in relationship to street frontages and pedestrian access are provided in subsection 21.07.060F., *Pedestrian Frontage Standards*.

(Note: The following subsections of the Multifamily and Townhouse Residential design standards are deleted: 21.07.110C.3., *Windows Facing the Street*; 21.07.110C.5., *Pedestrian Access*; 21.07.110C.6., *Building and Site Orientation Menu*; and 21.07.110C.9., *Entryway Treatment*, are deleted. The remaining subsections of 21.07.110C. are re-numbered accordingly.)

**D. Standards for Some Single-Family and Two-Family Residential Structures**

**2. Pedestrian-Oriented Street Frontage Standards.**

Provisions for site planning and building orientation in relationship to street frontages and pedestrian access are provided in subsection 21.07.060F., *Pedestrian Frontage Standards*.

(Note: Subsections 3., 4., and 5. are deleted.)

\*\*\* \*\*

(Note: Subsection 21.07.110E., *Prohibited Structures*, is deleted, and *Site Design* (below) is re-numbered.)

**E. Site Design**

(Note: *Site Design* subsections 3., *Driveway Width*, and 4., *Alleys*, are deleted.)

\*\*\* \*\*

**21.15.020 Rules of Construction and Interpretation**

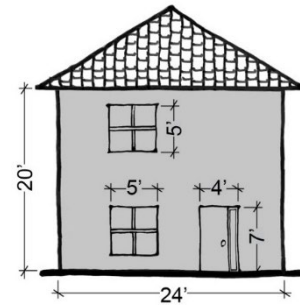
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**P. Measurement of Windows and Entrances as a Percentage of Building Wall Area**

The percentage of building elevation wall area that is window and entrance openings is determined by dividing the total square footage of the windows and primary entrances on the building elevation by the gross square footage of the building elevation wall area (except for parts of the building exempted in 3.), and then multiplying the resulting quotient by 100.

**1. Illustrated Example**

In the illustration, the building elevation wall area is 20 feet of height x 24 feet of length = 480 square feet. The window and primary entrance area is the area of the two windows (5 x 5 feet x 2) plus the area of the primary entrance and its sidelight (4 x 7 feet) = 78 square feet. The percentage of the building elevation wall area is found by dividing 78 by 480 then multiplying by 100, to yield 16%.



**2. Measuring Window and Primary Entrance Area**

“Window area” is defined in section 21.15.040, Definitions. Primary entrance area includes the entrance opening and door frame but excludes trim, molding, and other features beyond the door frame. “Primary entrance” is defined in 21.15.040, Definitions.

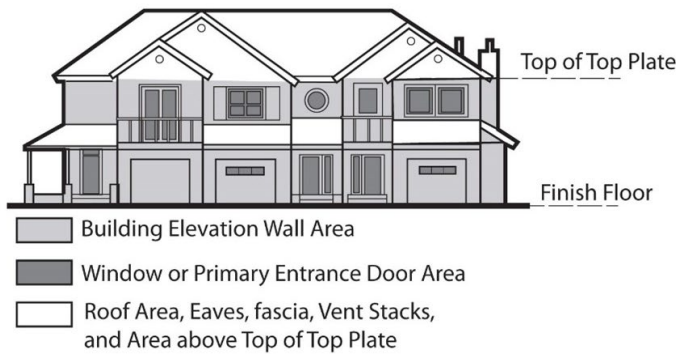
**3. Exempt Parts of the Building Elevation Wall Area**

The building elevation wall area is measured as the exterior wall plane surface area from finished grade to the top of the wall on the building except that, for the purposes of measuring windows and

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entrances as a percentage of building wall area, the following parts of the building are not included in the wall area calculation:

- a. Wall area above the topmost story's top plate, such as gable ends of a roof enclosing a cold attic, or parapet walls.
- b. Roof surfaces, eaves, fascia, vent stacks, mechanical equipment and rooms, rooftop access enclosures, and other roof appurtenances.
- c. Wall area of stories below grade plane, such as below grade parking, up to the finished floor of the story above grade plane.
- d. Parts of a street-facing building elevation below the grade plane of the abutting sidewalk (or edge of street pavement where there is no sidewalk).



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\*\*\* \*\*

**21.15.040 Definitions**

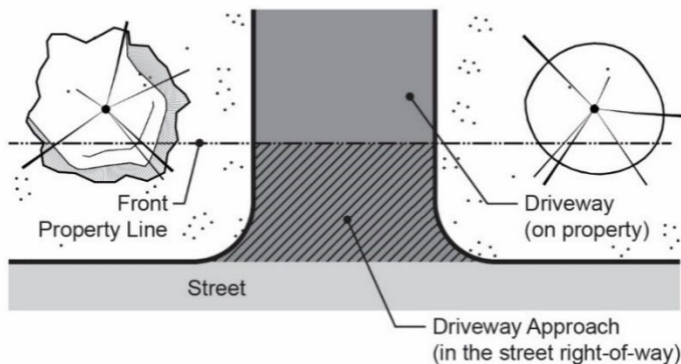
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**Complete Street.** See *Street, Complete*.

\*\*\* \*\*

**Driveway Approach**

The portion of a driveway providing direct vehicle ingress and egress over public right-of-way to a property. The driveway approach extends from the street edge to the front property line.



24  
25

\*\*\* \*\*



1 **21.15.040 Definitions**

2 \*\*\* \*\*

3 **Pedestrian Feature**

4 A permanent object that provides pedestrians with increased convenience, comfort, and utility, and which is publicly  
5 accessible and not limited to a tenant or establishment such as seating for a restaurant. Pedestrian features include:

- 6 • Seating such as benches accommodating several people;
- 7 • Secondary/informal seating opportunities such as steps, pedestals, low walls, or edges of fountains,  
8 accommodating several people;
- 9 • A space for standing with objects to lean against, such as bollards, short fences, or irregular building  
10 facades, accommodating several people;
- 11 • A tree or raised planter;
- 12 • A work of art such as a water feature, sculpture, cultural exhibit, or clock feature;
- 13 • Pedestrian-scale area lighting designed to light outdoor walkways or pedestrian spaces;
- 14 • Ornamental lighting designed to illuminate architecture and/or landscaping for decorative and aesthetic  
15 effect;
- 16 • A cross-country ski rack that can accommodate a ski lock and is located at building entrances within 500  
17 feet of a ski trailhead;
- 18 • A winter city urban design feature such as a wind screen, or outdoor stove or space heater; or
- 19 • Other object supporting pedestrian utility, such as a gazebo or kiosk.

20  
21 **Pedestrian Movement Zone**

22 The middle portion of an enhanced street sidewalk (21.07.060G.19.), comprising its walkway clear width located  
23 between the sidewalk’s street interface and building interface zones. The pedestrian movement zone provides for  
24 the primary function of sidewalks and is kept clear of any obstructions to pedestrian movement.

25 \*\*\* \*\*

26 *(Note: The definition of Primary Pedestrian Walkway is deleted.)*

27 \*\*\* \*\*

28 **Primary Entrance**

29 See **Entrance, Primary.**

30 \*\*\* \*\*

31 **Street, Complete**

32 A street that is designed, used, and operated to enable safe access for all users, including pedestrians, bicyclists,  
33 motorists, and public transportation users of all ages and abilities, and for all users to safely move through the  
34 transportation network. For example, Anchorage Metropolitan Area Transportation Solutions (AMATS) adopted a  
35 complete streets policy in 2018 for its urbanized planning area. The AMATS Complete Streets Policy is available  
36 on the municipal website.

37 \*\*\* \*\*

38 **Walkway Clear Width**

39 That portion of the total width of a walkway, trail, pathway, or sidewalk cross-section which is unobstructed,  
40 continuous (without interruption or being split by obstructions), and kept clear for pedestrian movement.

41  
42 *(Note: The definition reference “Walkway, Primary Pedestrian” is deleted.)*

43 \*\*\* \*\*



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[www.muni.org/Planning/2040Actions.aspx](http://www.muni.org/Planning/2040Actions.aspx)