

Public Hearing Draft Ordinance

Title 21 Text Amendment to Site Access Regulations

**Public Hearing Draft
PZC Case No. 2023-0011**

***Anchorage 2040 Land Use Plan
Implementation Action 4-3***

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Submitted by: Chair of the Assembly at
the Request of the Mayor
Prepared by: Planning Department
For reading: _____, 2023

ANCHORAGE, ALASKA
AO No. 2023-___

1 **AN ORDINANCE OF THE ANCHORAGE MUNICIPAL ASSEMBLY AMENDING**
2 **ANCHORAGE MUNICIPAL CODE TITLE 21 CHAPTERS 21.04, ZONING**
3 **DISTRICTS; 21.07, DEVELOPMENT AND DESIGN STANDARDS; AND 21.15,**
4 **RULES OF CONSTRUCTION AND DEFINITIONS, IN ORDER TO AMEND THE**
5 **SITE ACCESS DEVELOPMENT STANDARDS AND ACHIEVE**
6 **COMPREHENSIVE PLAN GOALS FOR INFILL AND REDEVELOPMENT,**
7 **HOUSING, AND ACCESSIBLE LAND USE.**

8
9 (Planning and Zoning Commission Case No. 2023-0011)
10

11 **WHEREAS**, the *Anchorage 2040 Land Use Plan (2040 Plan)* assesses the
12 housing and employment needs of current and future Anchorage residents and
13 includes goals, policies, and actions to address those needs; and
14

15 **WHEREAS**, Goal 3 of the *2040 Plan* defines Anchorage’s commercial centers and
16 corridors as the places to accommodate new business growth and mixed-use
17 housing opportunities through infill and redevelopment in a more efficiently
18 developed, walkable community; and
19

20 **WHEREAS**, Goal 4 of the *2040 Plan* defines Anchorage’s neighborhoods as the
21 places to provide a range of additional housing opportunities, meeting the housing
22 needs of residents of all income levels and household types; and
23

24 **WHEREAS**, Goal 7 of the *2040 Plan* recommends infill and redevelopment that is
25 compatible with the valued characteristics of surrounding neighborhood; and
26

27 **WHEREAS**, the *2040 Plan* as well as neighborhood and district plans recognize
28 older, urban neighborhoods and transit-supportive development corridors in
29 certain parts of the Anchorage Bowl as having stronger street grid patterns, greater
30 access to public transit and sidewalks, lower rates of car ownership, and shorter
31 distances between trip destinations, which merit alternative site access, pedestrian
32 frontage, and driveway and regulations tailored to their urban contexts; and
33

34 **WHEREAS**, wider than necessary off-street driveway access and vehicle
35 circulation aisle standards are often among the costliest and most land-consuming
36 zoning requirements for multi-unit housing and mixed-use residential
37 developments; and
38

39 **WHEREAS**, inadequate treatment of pedestrian site access affects the cost and
40 range of housing types and business developments, and choices for travel mode;
41 and

1 **WHEREAS**, Action 4-6 of the *2040 Plan* calls for amendments to Title 21 for
2 internal site circulation for vehicles and private lanes for compact infill housing; and

3
4 **WHEREAS**, the *Metropolitan Area Transportation Solutions (AMATS) 2040*
5 *Metropolitan Transportation Plan* sets forth policies and actions under Goal 3 to
6 develop an efficient multi-modal transportation system to reduce congestion,
7 promote accessibility, and improve system reliability, including Action 3E-3 to
8 support municipal review of development projects to ensure non-motorized
9 amenities are provided and Policy 3I-2 to support initiatives that increase bicycle,
10 pedestrian, and public transit mode share; and

11
12 **WHEREAS**, neighborhood and district plans such as the *Spenard Corridor Plan*
13 and *Fairview Neighborhood Plan* promote designing developments for walkability
14 and a pedestrian-scaled walking environment; and

15
16 **WHEREAS**, amending multifamily residential driveway access and circulation
17 requirements will facilitate development within urban contexts that is true to
18 neighborhood character and desired outcomes; and

19
20 **WHEREAS**, the Planning Department incorporated feedback from dozens of
21 consultations and public meetings with the general public, private-sector
22 development experts, design/architectural professionals, business organizations,
23 neighborhoods, public agencies and officials, and municipal committees and
24 commissions; and

25
26 **WHEREAS**, the Anchorage Assembly adopted A.O. No. 2022-80(S), which carried
27 out Action 4-3 of the *2040 Plan* and eliminated minimum off-street parking
28 requirements and increased bicycle parking requirements; now, therefore,

29
30 **THE ANCHORAGE ASSEMBLY ORDAINS:**

31
32 **Section 1.** Anchorage Municipal Code Chapter 21.04, Zoning Districts, is
33 hereby amended to read as follows (*the remainder of the chapter is not affected*
34 *and therefore not set out*):

35
36 **21.04.020 Residential Districts**

37 *** **

38 H. *R-3A: Residential Mixed-Use District.*

39 *** **

40 2. District-Specific Standards

41 *** **

42 e[F]. *Enhanced street sidewalk* [OPTION]. An enhanced street
43 sidewalk [ENVIRONMENT] may be provided in lieu of
44 required sidewalks and side perimeter landscaping, as
45 defined[PROVIDED] in 21.07.060G.20[F.17].

46 *** **

47 (AO 2012-124(S), 2-26-13; AO No. 2015-100, § 1, 10-13-15; AO No. 2017-
48 176, § 3, 1-9-18; AO No. 2019-58, § 2, 5-7-19)

1
2 **21.04.030 Commercial districts.**

3 *** **

4 G. *Standards for Mixed-Use Development in the B-1A and B-1B*
5 *Districts.*

6 *** **

7 5[6]. *Enhanced street sidewalk* [OPTION]. An enhanced street
8 sidewalk [ENVIRONMENT] may be provided in lieu of
9 required sidewalks and side perimeter landscaping,
10 defined[PROVIDED] in 21.07.060G.20[F.17].

11
12 [7. *BUILDING PLACEMENT AND ORIENTATION.* BUILDINGS
13 SHOULD BE PLACED AND ORIENTED TO THE STREET,
14 LINING SIDEWALKS AND PUBLIC SPACES WITH
15 FREQUENT SHOPS, ENTRANCES, WINDOWS WITH
16 INTERIOR VIEWS, AND ARTICULATED GROUND-LEVEL
17 FACADES. THE FOLLOWING STANDARDS APPLY:

18
19 A. BUILDINGS SHALL HAVE VISUAL ACCESS
20 WINDOWS AND/OR PRIMARY ENTRANCES ON
21 STREET-FACING BUILDING ELEVATIONS (UP TO
22 A MAXIMUM OF TWO ELEVATIONS) FOR AT
23 LEAST 15 PERCENT OF THE NON-RESIDENTIAL
24 GROUND FLOOR WALL AREA. QUALIFYING
25 WINDOWS SHALL BE NO MORE THAN FOUR FEET
26 ABOVE FINISHED GRADE. WINDOWS SHALL
27 COMPRISE AT LEAST TEN PERCENT OF THE
28 WALL AREA OF THE UPPER FLOOR BUILDING
29 ELEVATION (ABOVE THE GROUND FLOOR).
30 EXTERIOR WALL AREAS OF BUILDING
31 MECHANICAL ROOMS ARE EXEMPT. AN
32 ELEVATION THAT IS MORE THAN 150 FEET AWAY
33 FROM THE FACING STREET SHALL BE EXEMPT,
34 UNLESS IT IS THE ONLY APPLICABLE
35 ELEVATION.

36
37 B. EITHER:
38 I. PROVIDE AT LEAST ONE PRIMARY
39 ENTRANCE WITHIN 60 FEET OF A STREET
40 SIDEWALK, OR 90 FEET FOR BUILDINGS
41 OVER 25,000 SQUARE FEET OF GROSS
42 FLOOR AREA, AND CONNECTED TO THE
43 STREET BY A CLEAR AND DIRECT
44 WALKWAY; OR
45
46 II. PROVIDE A PROMINENT AND INVITING
47 PRIMARY ENTRANCE THAT IS VISIBLE
48 FROM THE STREET, CONNECTED BY A

1 DIRECT WALKWAY TO THE STREET, AND
2 HIGHLIGHTED BY TWO OF THE
3 FOLLOWING:
4

- 5 (A) PORTICO, OVERHANG, CANOPY, OR
6 SIMILAR PERMANENT FEATURE
7 PROJECTING FROM THE WALL;
- 8 (B) RECESSED AND/OR PROJECTED
9 ENTRANCE THAT COVERS AT LEAST
10 80 SQUARE FEET;
- 11 (C) ARCHES, PEAKED ROOF FORMS,
12 TERRACING PARAPETS, OR OTHER
13 CHANGE OF BUILDING ROOFLINE;
- 14 (D) CHANGES IN SIDING MATERIAL, OR
15 DETAIL FEATURES SUCH AS
16 TILEWORK, TO SIGNIFY THE
17 ENTRANCE; OR
- 18 (E) ENTRANCE PLAZA, PATIO, OR
19 SIMILAR COMMON PRIVATE OPEN
20 SPACE.

21
22 C. BUILDINGS SHALL COMPLY WITH THE MAXIMUM
23 SETBACKS ESTABLISHED IN SECTION 21.06.020,
24 TABLES OF DIMENSIONAL STANDARDS, AND
25 SUBSECTION 21.06.030C.5. THE MAXIMUM
26 SETBACK MAY BE REDUCED OR ELIMINATED
27 WITH THE CONCURRENCE OF THE DIRECTOR.]
28

29 H. *Standards for Mixed-Use Development in the B-3 District.*
30 *** *** ***

31 [3. *BUILDING PLACEMENT AND ORIENTATION.* BUILDINGS
32 ARE SUBJECT TO THE BUILDING PLACEMENT AND
33 ORIENTATION STANDARDS FOR MIXED-USE
34 DEVELOPMENT IN THE B-1A/B-1B DISTRICTS IN
35 SUBSECTION 21.04.030G.7. ABOVE.]
36

37 3[5]. *Enhanced street sidewalk* [OPTION]. An enhanced street
38 sidewalk [ENVIRONMENT] may be provided in lieu of
39 required sidewalks and side perimeter landscaping,
40 defined[PROVIDED] in 21.07.060G.20[F.17].
41

42 (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO No. 2017-58, § 1,
43 4-11-17; AO No. 2019-11, § 2, 2-12-19)
44 *** *** ***

45 **21.04.070 Overlay Zoning Districts.**

46 ***

47 C. *Commercial Center Overlay.*

48 ***

1 5[6]. *Dimensional standards.*

2
3 [A. THE MINIMUM FRONT SETBACK IS REDUCED TO
4 FIVE FEET FOR BUILDING ELEVATIONS THAT
5 COMPLY WITH 21.04.030G.7., BUILDING
6 PLACEMENT AND ORIENTATION.]

7
8 [B.] Uses in the B-1B district are exempt from the gross
9 floor area limitations of subsection 21.04.030C.2.b.,
10 provided that an individual use occupies no more than
11 60,000 square feet of gross floor area.

12 *** *** ***

13 (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13)

14
15 **Section 2.** Anchorage Municipal Code Chapter 21.07, Development and Design
16 Standards, is hereby amended to read as follows (*the remainder of the chapter is*
17 *not affected and therefore not set out*):

18
19 **21.07 Development and Design Standards**

20
21 **21.07.010 General Provisions.**

22 *** ***

23 D. *Alternative Equivalent Compliance.*

24 *** *** ***

25 2. *Applicability.* The alternative equivalent compliance
26 procedure shall be available only for the following sections of
27 this title:

28 *** *** ***

29 c. Subsection 21.07.060F., Pedestrian Frontage
30 Standard;

31
32 d. Subsection 21.07.060G., Pedestrian Amenities;

33
34 (*Note to code revisor: re-number subsections c. - i. as*
35 *subsections d.- j.*)

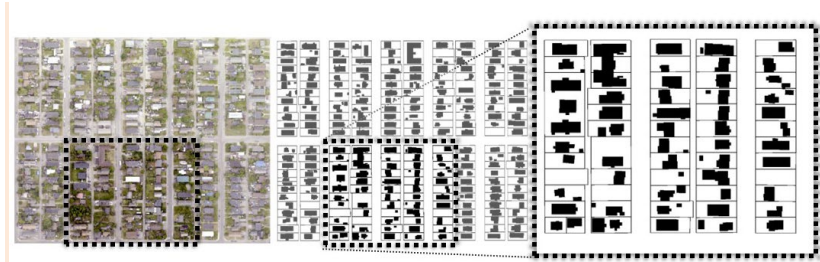
36 *** *** ***

37
38 E. Urban Neighborhood Development Context Area.

39
40 1. Purpose. This section establishes an Urban Neighborhood
41 Development Context Area that reflects the existing and
42 desired characteristics of Anchorage's older, urban
43 neighborhoods identified by the Comprehensive Plan, Land
44 Use Plan Map as Traditional Neighborhood Design areas.
45 Some are also identified in neighborhood and district plans.
46 This section provides a basis for development regulations
47 tailored to the characteristics of these neighborhoods and
48 fulfills their role as recommended in the Comprehensive Plan.

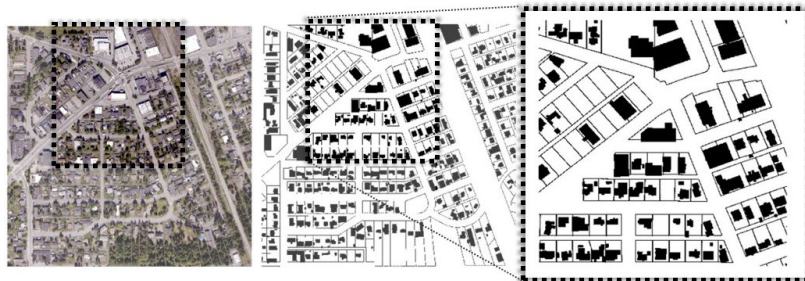
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- a. This area is intended to include Anchorage’s original urban neighborhoods of Government Hill, South Addition, and Fairview, and post-war era (1950s-1960s) neighborhoods such as Spenard, Airport Heights, and Russian Jack Park. It also includes transit-supportive development corridors and designated mixed-use centers.
- b. This area recognizes urban neighborhood characteristics including a more highly interconnected street system, smaller lot and block sizes, and access to pedestrian facilities and public transit. Some neighborhoods have consistent or frequent sidewalks and alleys. Buildings and residences often have moderate front setbacks and orient to the street, with landscaping or walkways (instead of parking) comprising most of their street frontage. Transit-supportive development corridors and mixed-use centers contribute to a compact, accessible land use pattern.



Regular street grid of city blocks and alleys in Mountain View.

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More relaxed street grid along a transit corridor in Spenard.

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- 2. **Applicability.** The urban neighborhood development context area shall apply as a basis for area-specific development standards in this chapter 21.07. It shall not be used as a basis to change allowed uses or review and approval procedures of the underlying zoning district, district-specific dimensional standards, or any other standards elsewhere in this title.

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3. Urban Neighborhood Development Context Area Established.
The urban neighborhood development context area is delineated in map 21.07-1, with the following clarifications:
- a. The Downtown (DT) zoning districts are not included because they are addressed in chapter 21.11.
 - b. The university and medical campuses of the UMED District are not included.
 - c. The properties of Merrill Field Airport, Alaska Railroad Terminal Reserve, and Port of Alaska are not included.
 - d. Broadmore Estates, Benzen Addition, and Creekside Park Addition Subdivisions are not included.
 - e. Birchwood Park and Green Acres Subdivisions are included.



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(AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2020-11, 2-25-20;
AO 2020-38, 4-28-20; AO 2022-36, 4-26-22)

*** *** ***

1 **21.07.060** **Transportation and Connectivity**

2 *** **

3 E. **Standards for Pedestrian Facilities.**

4 *** **

5 2. **Sidewalks.**

6
7 a. All sidewalks shall be designed to comply with the
8 standards of the Design Criteria Manual (DCM) and
9 Municipality of Anchorage Standard Specifications
10 (MASS) and shall be improved in accordance with
11 subsection 21.08.050H.

12
13 b. Street improvement projects and new streets in
14 subdivisions i[l]n all class A zoning districts except for
15 industrial districts[,] shall install sidewalks [SHALL BE
16 INSTALLED] on both sides of all streets (local,
17 collector, arterial, public or private, including loop
18 streets). Where indicated in the comprehensive plan, a
19 pathway may replace a sidewalk on one side. Street
20 improvement projects i[l]n industrial zoning districts
21 shall install[,] a sidewalk [SHALL BE INSTALLED] on
22 one side of all local streets, and on both sides of local
23 streets if the new sidewalks would connect to existing
24 sidewalks on both ends and the needed sidewalk
25 length is no greater than one quarter mile.

26 *** **

27 f. Development on lots along existing streets in class A
28 zoning districts shall install sidewalks on all lot
29 frontages abutting streets in the following situations:

30
31 i. In R-4, R-4A, commercial [DISTRICTS], and
32 DT zoning districts.

33 *** **

34 4. **On-site pedestrian walkways.**

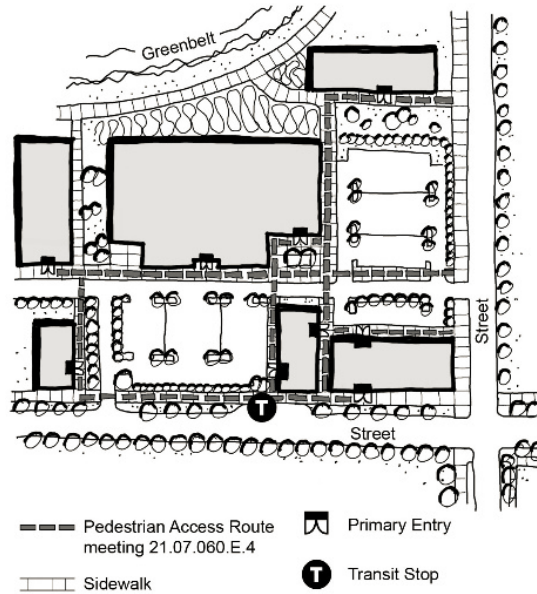
35 *** **

36 b. **On-site pedestrian connections.**

37 *** **

38 ii. All primary building entrances on a site shall be
39 connected to the street by a convenient system
40 of walkways. This includes multiple primary
41 entrances into one building, and primary
42 entrances in separate buildings on a site.

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(Note to code revisor: replace existing illustration with illustration above.)

*** **

- iv. The primary front entrance of a residential dwelling shall be connected to the street by a walkway as provided in i. through ii. above, or by the dwelling's individual driveway, or by a shared parking courtyard meeting 21.07.060G.21.

c. Walkway clear width and improvements.

- i. Walkway clear width. The minimum width of a required pedestrian walkway shall be five feet of unobstructed clear width, [EXCLUDING VEHICULAR OVERHANG,] except where otherwise stated in this title. A walkway that provides access to no more than four residential dwelling units may provide an unobstructed clear width of three feet.

- ii. Walkways shall be improved in accordance with subsection 21.08.050H.

*** **

F. Pedestrian frontage standard.

- 1. Purpose. The pedestrian frontage standard requires site planning and building orientation toward neighborhood streets and sidewalks to facilitate pedestrian access and reduce automobile parking congestion. Objectives include to:

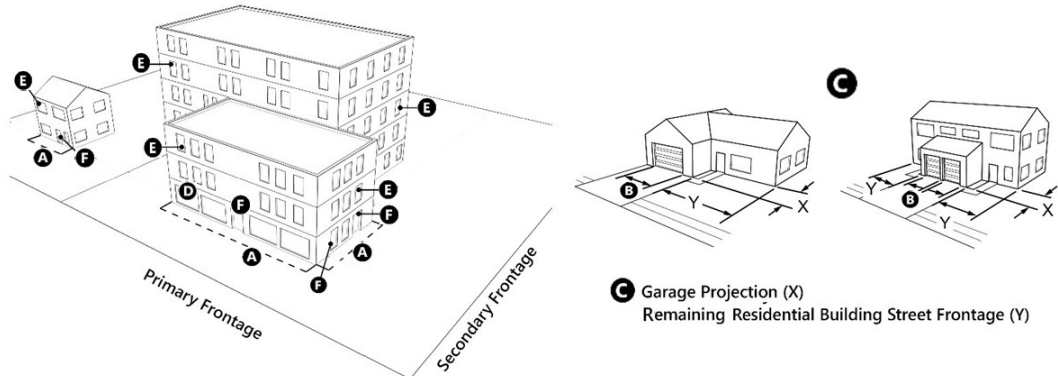
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- a. Organize and orient buildings around public streets and associated frontages in a way that frames streets as positive public space, promotes pedestrian activity, and connects to multiple modes of transportation.
 - b. Provide clearly defined, safe pedestrian access to building entries that invites people of all abilities and minimizes conflicts with vehicles and parking.
 - c. Place active indoor spaces, entrances, and windows on street-facing building facades to improve the visual connection to the street and promote a safe, secure neighborhood.
2. Applicability. Subsection 3. applies to development in the urban neighborhood development context area established in 21.07.010E. Subsection 4. applies to development in the other areas of the municipality. The following are exempt:
- a. Changes of use and other developments that comprise building modifications of less than 50 percent of the total improvement value of the building(s) on the site.
 - b. Development in Girdwood, Downtown (DT), CE-DO, CE-EVO, PLI, PR, TA, or industrial zoning districts.
 - c. Single-family and two-family dwellings constructed prior to January 1, 2016, or on lots of 20,000 square feet or larger, or in Class B zoning districts.
 - d. Uses without habitable floor area, such as utility substations.
3. Administrative adjustments.
- a. The Alternative Equivalent Compliance procedure in 21.07.010D. may be used to propose alternative means of complying with the intent of this section.
 - b. The director shall approve administrative relief if the applicant demonstrates the adjustment is necessary to compensate for some practical difficulty of the site. This includes reducing the window area requirement by the amount needed to comply with 5-star or affordable housing (21.15.040) energy rating requirements. The department shall keep record of the approved exception with written findings supporting the exception on file.

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4. Standard for urban neighborhood contexts. The standards of table 21.07-2 apply to the primary frontage and one secondary frontage.



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Table 21.07-2: Pedestrian Frontage Standard for Urban Neighborhood Contexts	
<u>Site Elements for Pedestrian-Oriented Frontages</u>	<u>Standard</u>
<u>A. Minimum required ground-floor, street-facing building elevation without driveways or parking facilities in front</u>	50% of building elevation width ¹ <u>Lots with alley access: no parking allowed in front of building except in driveways for individual dwellings</u>
<u>B. Maximum allowed width of residential garage entrance(s) on ground-floor street-facing building elevation</u>	40% of building elevation width ¹
<u>C. Maximum allowed distance a garage may project out in front of the rest of the street-facing residential building elevation</u>	<u>None allowed on lots with alley access</u> <u>No more than 50% of the width of the non-garage portion of the street-facing building, on other lots</u>
<u>D. Minimum required visual access windows or primary entrances on non-residential ground-floor street-facing building elevation ²</u>	25% of wall area on primary frontage 15% of wall area on second frontage
<u>E. Minimum required windows or primary entrances on residential and upper-floor non-residential street-facing elevations ²</u>	15% of wall area on primary frontage 10% of wall area on second frontage
<u>F. Requirement for primary entrance facing the street or visible from the street via an unobstructed line of sight</u>	<u>Residential: Meet 21.07.060G.14., Covered, Visible Residential Entrance</u> <u>Other uses: at least one entrance</u>
<u>G. Minimum required number of pedestrian amenities from 21.07.060G. (in addition to G.14.) in multifamily, mixed-use, townhouse, group housing, and non-residential developments.</u>	<u>2 pedestrian amenities</u>
¹ Exception: On lots without alley access in R-2A, R-2D, and R-2M zoning districts, two-family (duplex) developments may have a minimum of 25% in A. above and a maximum of 67% in B. above. ² Visual access windows shall have a sill height of no more than four feet above finished grade. Rules for measuring window area as a percentage of building wall area are provided in 21.15.020P. Visual access windows ("Window, providing visual access"), primary entrance, and ground-floor wall area are defined in 21.15.040.	

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5. Standard for areas outside of urban neighborhood contexts.
The standards of table 21.07-3 apply to the primary frontage
and one secondary frontage.

Table 21.07-3: Pedestrian Frontage Standard – Outside of Urban Neighborhood Contexts	
<u>Site Elements for Pedestrian-Oriented Frontages</u>	<u>Standard</u>
<u>A. Minimum required ground-floor, street-facing building elevation without driveways or parking facilities in front</u>	<u>20% of building elevation width, on one frontage</u>
<u>B. Maximum allowed width of residential garage entrance(s) on ground-floor street-facing building elevation</u>	<u>67% of building elevation width</u>
<u>C. Maximum allowed distance a garage may project out in front of the rest of the street-facing residential building elevation</u>	<u>No more than the width of the non-garage portion of the street-facing building elevation</u>
<u>D. Minimum required visual access windows or primary entrances on non-residential ground-floor street-facing building elevation ²</u>	<u>15% on primary frontage</u> <u>10% on secondary frontage</u>
<u>E. Minimum required windows or primary entrances on residential and upper-floor non-residential street-facing elevations ²</u>	<u>10% on primary frontage</u> <u>5% on secondary frontage</u>
<u>F. Requirement for primary entrance facing the street or visible from the street via an unobstructed line of sight</u>	<u>Residential: Meet 21.07.060G.14., Covered, Visible Residential Entrance.</u> <u>Other uses: at least one entrance.</u>
<u>G. Minimum required number of pedestrian amenities from 21.07.060G. (in addition to G.14.) in multifamily, mixed-use, townhouse, group housing, and non-residential developments.</u>	<u>1 pedestrian amenity</u>
¹ <u>Exception: Non-residential buildings located more than 100 feet from the applicable street rights-of-way are exempt from A., D., E., and F. above.</u>	
² <u>Visual access windows shall have a sill height of no more than four feet above finished grade. Rules for measuring window area as a percentage of building wall area are provided in 21.15.020P. Visual access windows ("window, providing visual access"), primary entrance, and ground-floor wall area are defined in 21.15.040.</u>	

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G. Pedestrian amenities menu.

- Purpose. T[HE PURPOSE OF T]his section [IS TO]defines and provides standards for pedestrian amenities that are used in developments to satisfy a requirement, menu choice, or incentive in this title[MAY BE REQUIRED OR INCLUDED IN A MENU OF CHOICES TO MEET A REQUIREMENT, OR LISTED AS A SPECIAL FEATURE THAT CAN COUNT TOWARD A BONUS INCENTIVE ANYWHERE IN THIS TITLE. FOR EXAMPLE, ANOTHER SECTION OF THIS TITLE MAY LIST A PEDESTRIAN AMENITY AS A SPECIAL FEATURE FOR WHICH BONUS FLOOR AREA MAY BE GRANTED.] The standards [CONTAINED] in this section give predictability for applicants and the public[, DECISION-MAKERS, AND THE COMMUNITY] for the minimum acceptable standards for pedestrian amenities. It [ALSO

1 ENSURES THE] encourages amenities that will improve and
2 enhance the community[TO THE BENEFIT OF ALL,] and
3 respond to Anchorage's[THE] northern latitude climate. [THIS
4 TITLE PROVIDES FLEXIBILITY TO ENCOURAGE AND
5 ALLOW FOR CREATIVITY AND UNIQUE SITUATIONS
6 THROUGH THE ALTERNATIVE EQUIVALENT
7 COMPLIANCE AND MINOR MODIFICATIONS PROCESS.]
8

9 2. *Applicability.* Pedestrian amenities shall meet the minimum
10 standards of this section [IN ORDER]to be credited toward a
11 requirement, menu choice, or [AS A SPECIAL FEATURE
12 BONUS] incentive of this title, except where specifically
13 provided otherwise in this title. The standards of this section
14 do not apply to amenities that are not counted toward a
15 requirement, menu choice, or incentive under this title.
16

17 3. *Administrative Adjustments.* The alternative equivalent
18 compliance procedure set forth in subsection 21.07.010D.
19 may be used to propose alternative means of complying with
20 the standards of this subsection 21.07.060G[F].
21

22 [3. WALKWAY. A WALKWAY IS A SURFACE THAT
23 CONNECTS TWO POINTS FOR PEDESTRIAN USE, AS
24 DEFINED IN CHAPTER 21.15. A WALKWAY MAY BE IN A
25 PUBLICLY DEDICATED PEDESTRIAN EASEMENT.
26 EXAMPLES INCLUDE PEDESTRIAN CONNECTIONS
27 WITHIN ONE DEVELOPMENT SITE, MID-BLOCK,
28 BETWEEN SUBDIVISIONS, OR LEADING FROM STREETS
29 TO PUBLIC AMENITIES, SUCH AS SCHOOLS OR PARKS.]
30

31 A. A WALKWAY SHALL HAVE A MINIMUM
32 UNOBSTRUCTED CLEAR WIDTH OF FIVE FEET,
33 EXCEPT WHERE OTHERWISE STATED IN THIS
34 TITLE. A WALKWAY THAT PROVIDES ACCESS TO
35 NO MORE THAN FOUR RESIDENTIAL DWELLING
36 UNITS MAY HAVE AN UNOBSTRUCTED CLEAR
37 WIDTH OF THREE FEET.
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39 B. WALKWAYS SHALL BE IMPROVED IN
40 ACCORDANCE WITH SUBSECTION 21.08.050H.]
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4. Enhanced on-site [PRIMARY PEDESTRIAN] walkway. An enhanced on-site [A PRIMARY PEDESTRIAN] walkway is intended to provide an option for applicants to receive credit for exceeding the minimum development standards for walkways and improving pedestrian convenience, comfort, and safety on the site. Enhanced on-site walkways provide additional width [UNOBSTRUCTED CLEAR WIDTH OF AT LEAST EIGHT FEET] for pedestrian movement [WITH ADDITIONAL SPACE INCORPORATING FEATURES ALONG THE WALKWAY SUCH AS STOREFRONT SIDEWALK SPACE, ROOM FOR RESIDENTIAL STOOPS OR BUILDING FOUNDATION PLANTINGS,] and peripheral space that accommodates landscaping, furniture, and utilities. [AS ESTABLISHED GENERALLY IN SUBSECTION F.1 AND F. 2 ABOVE, THE STANDARDS OF THIS SUBSECTION APPLY ONLY WHERE THE SPECIFIC TERM "PRIMARY PEDESTRIAN WALKWAY" IS LISTED AS A REQUIREMENT, MENU CHOICE, OR SPECIAL FEATURE THAT COUNTS TOWARD A BONUS. THIS SUBSECTION IS NOT A GENERALLY APPLICABLE REQUIREMENT FOR OTHER LARGE WALKWAYS.]

a. [A PRIMARY PEDESTRIAN WALKWAY SHALL BE DEVELOPED AS A CONTINUOUS PEDESTRIAN ROUTE EXTENDING FOR AT LEAST 50 FEET.]

[B.] An enhanced on-site [A PRIMARY PEDESTRIAN] walkway shall have a pedestrian movement zone with a continuous,[N] unobstructed walkway clear width of at least eight feet, or six feet where providing access only to four or fewer residential dwelling units. Where adjacent to a ground-floor building elevation it shall also have a [SIDEWALK STOREFRONT OR] building interface zone a minimum of two feet in width for building foundation landscaping or [THREE FEET IN WIDTH OF SIDEWALK] space for opening doors or seating and transition pedestrian spaces. In addition to the pedestrian movement zone and any building interface zone, the enhanced on-site walkway shall have a buffer space of at least two[FOUR] feet in width where abutting motor vehicle parking lots, circulation aisles, or driveways [SHALL BE INCORPORATED AS PART OF THE WALKWAY WHEN ABUTTING ANY STREET OR VEHICLE AREA,]. The buffer space shall [TO] accommodate [STREET TREES,]landscaping beds, fencing or bollards, light poles, utilities, benches, and other furnishings[OBJECTS TO BE KEPT CLEAR OF THE WALKWAY].

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b[C]. A minimum of two pedestrian features as defined by this title (21.15.040) shall be provided along the enhanced walkway, with at least one for every 50 feet of the walkway length [AT LEAST ONE PEDESTRIAN FEATURE AS DEFINED BY THIS TITLE SHALL BE PROVIDED FOR EVERY 50 FEET OF LENGTH ALONG A PRIMARY PEDESTRIAN WALKWAY].

[D. A PRIMARY PEDESTRIAN WALKWAY SHALL BE ILLUMINATED WITH PEDESTRIAN SCALE LIGHTING.]

c[E]. Enhanced on-site [A PRIMARY PEDESTRIAN] walkways shall provide continuous, direct[LY] connections from building primary entrance(s) to surrounding public streets and sidewalks[,] and be publicly accessible or available to all residents of the development [AT ALL TIMES].

*** *** ***

13. Separated walkway to the street. The development shall connect the building primary entrances to the street with a clear and direct walkway that is not routed through a parking facility or across vehicle driveways or circulation aisles. The minimum clear width portion of the walkway shall be separated from the parking facility by at least five feet.

14. Covered, visible residential entrance. A porch, stoop, or landing sheltered by a roof is intended to give visual emphasis to the building entrance as an aid in wayfinding, and help provide safe, convenient access to residential buildings from the street. The entrance shall meet the following standards:

a. The entrance shall incorporate a porch, stoop, or landing with an internal dimension of at least 16 square feet, and a permanent, sheltering roof covering at least 12 square feet.

b. The porch, stoop, or landing shall be distinguished from adjoining areas and vehicle parking by vertical separation or a change in surfacing material.

c. The building entrance shall also be visible (via an unobstructed line of sight) from a street or face a common private open space (21.07.030), a plaza or courtyard (21.07.060G.6.), a housing courtyard

(21.07.060G.7.), or a shared parking courtyard (21.07.060G.21.) that is visible from a street.

15. Enhanced primary entrance. An enhanced building entry is intended to provide a more prominent and inviting primary pedestrian entrance. To receive credit, the enhanced primary entrance shall incorporate at least three of the following features:

a. Outdoor sheltering roof feature projecting from the building façade such as an overhang, portico, canopy, marquee with an inside dimension of at least 16 square feet;

b. Recessed and/or projected entrance or other building wall modulation with projections or recesses in the building wall plane;

c. Changes in the building’s main roofline such as arches, peaked roof forms, or terracing parapets;

d. Changes in siding material or exterior finishes, or façade detail features such as tilework that emphasize the entrance;

e. Entrance plaza, patio, or similar common private open space;

f. Landscaping not otherwise required by this title, such as integrated planters, landscape accent lighting, or special paving treatments; or

g. One or more pedestrian features (21.15.040) such as pedestrian-scale lighting or seating.

16. Enhanced Façade Transparency. Increase the windows and/or primary entrances on street-facing building elevations which are subject to the window requirement in 21.07.060F., by an additional five (5) percent of the wall area.

17. Pedestrian Frontage Free of Parking in Front. In urban neighborhood development contexts (21.07.010E.), provide a street frontage without driveways, parking, or loading facilities in front of at least 75% of the ground-floor, street-facing building elevation. In other areas, provide such a frontage in front of at least 33% of the street-facing building elevation.

1 18. Site Entry Feature. Highlight and define a pedestrian entrance
2 to a development site using three or more of the following
3 elements:

4
5 a. Landscape treatment with seasonal color and trees,
6 which clearly distinguishes and highlights the site
7 entry.

8
9 b. Plaza or courtyard as described in subsection
10 21.07.060G.6.

11
12 c. Identifying building primary entrance form including a
13 covered entry, when the primary entrance is within 50
14 feet of the site entrance.

15
16 d. Special paving, pedestrian walkway area lighting,
17 ornamental lighting, and/or bollards.

18
19 e. Ornamental gate and/or fence.

20
21 19[16]. *Pedestrian-interactive building [USE].* A pedestrian-
22 interactive building [USE] is intended to provide interior
23 [GROUND-FLOOR] spaces that [STRONGLY] engage the
24 sidewalk with street-facing windows and entrances, and
25 [FEATURE] activities and services that support neighborhood
26 residents, and generally contribute to the activity level and
27 quality of the pedestrian[-ORIENTED] environment of the
28 neighborhood or district. [THE STANDARDS THAT FOLLOW
29 APPLY WHERE THE TERM “PEDESTRIAN-INTERACTIVE
30 USE” IS LISTED IN THIS TITLE AS A REQUIREMENT,
31 SPECIAL FEATURE FOR A BONUS, OR A MENU CHOICE.]

32
33 [A. A PEDESTRIAN-INTERACTIVE USE SHALL BE ANY
34 OF THE FOLLOWING USES THAT ARE PERMITTED
35 IN THE DISTRICT: RETAIL AND PET SERVICES;
36 FINANCIAL INSTITUTION PROVIDING BANKING
37 SERVICES OPEN TO THE PUBLIC WITH AT LEAST
38 ONE EMPLOYEE ON SITE; FOOD OR BEVERAGE
39 SERVICE; PERSONAL SERVICE; CULTURAL
40 FACILITY; OR THE FRONTAGE OF ENTRYWAYS
41 OR STAIRWAYS THROUGH WHICH SUCH USES
42 ARE PRINCIPALLY ACCESSED;

43
44 B. RETAIL SALES USES THAT ARE PERMITTED IN
45 THE DISTRICT SHALL BE CONSIDERED
46 PEDESTRIAN-INTERACTIVE USES, EXCEPT FOR
47 THE FOLLOWING TYPES OF RETAIL SALES USES:
48 FUELING STATION; BUILDING MATERIALS STORE.

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C. THE FOLLOWING USES SUPPORTING RESIDENTIAL NEIGHBORHOOD AND HOUSING DEVELOPMENT ARE ALSO CONSIDERED PEDESTRIAN-INTERACTIVE USES WHEN PERMITTED IN THE DISTRICT: RESIDENTIAL DWELLINGS WITH INDIVIDUAL FRONT ENTRIES ALONG THE STREET; ELEMENTARY SCHOOL; MIDDLE OR HIGH SCHOOL; HEALTH SERVICES; CHILD CARE CENTER.]

a[D]. A pedestrian-interactive building [USE] shall provide a primary entrance facing the street. Entrances at building corners facing a street may be used to satisfy this requirement.

b[E]. A pedestrian-interactive building [USE] shall contain habitable floor area at least 24 feet deep extending along a minimum of 50 percent of the [FULL] length of the ground-floor, street-facing building elevation in Urban Neighborhood Contexts, and 30 percent elsewhere [ALLOWING FOR]. The habitable floor area may include pedestrian [AND VEHICLE] entrances, entry lobbies or atriums, and stairwells.

c[F]. At least 50 percent of the street-facing building elevation width of a pedestrian-interactive building shall have no parking facilities in front [USE SHALL COMPLY WITH SUBSECTION 21.06.030C.5., MAXIMUM SETBACKS, BUT THE EXCEPTIONS OF SUBSECTION 21.06.030C.5.D. SHALL NOT BE AVAILABLE].

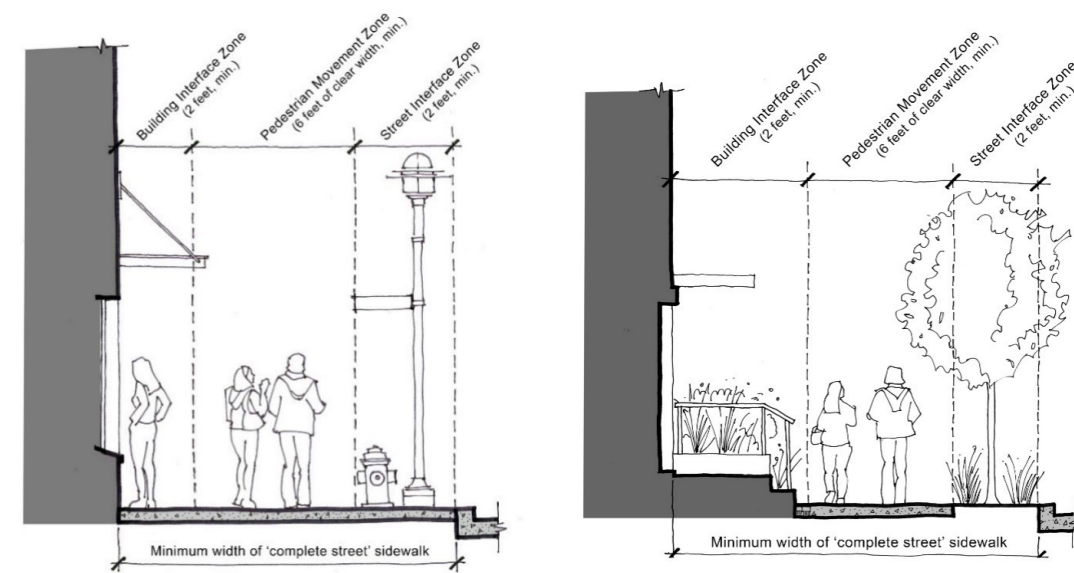
d[G]. Street-facing [GROUND-FLOOR] wall areas of a non-residential [PEDESTRIAN-INTERACTIVE] use shall be 50[67] percent visual access windows or primary entrances on the ground floor, and 20 percent windows above the ground floor. [, EXCEPT THAT SUCH]

e. Street-facing wall areas for residential uses [DWELLINGS] shall be at least 20 percent [VISUAL ACCESS] windows or primary entrances.

f[H]. Where a building has three or more street frontages, these criteria apply along only two of the frontages.

20[17]. Enhanced street sidewalk [OPTION]. An enhanced street sidewalk is intended to provide an option for applicants to receive credit for exceeding the minimum development standards for sidewalks. An enhanced street sidewalk promotes sidewalk widening and streetscape enhancements to support higher levels of pedestrian activity, comfort, and safety in the district [AND ACCESS IN MIXED-USE DEVELOPMENTS]. An enhanced street sidewalk [ENVIRONMENT] with “complete[MAIN] street” (21.15.040) style amenities may be provided in lieu of required site perimeter landscaping [WHERE IT IS LOGICAL TO SUPPORT A PEDESTRIAN ZONE] as determined through an administrative site plan review, and subject to the following:

- a. The enhanced street sidewalk cross-section [WIDTH] shall be at least 12 feet wide, and include a pedestrian movement zone, building interface zone, and street interface zone (21.15.040). The pedestrian movement zone shall have a clear width of at least six feet. The street interface zone shall be at least two feet wide from back-of-curb, and four feet wide along major arterials. Where a building adjoins the sidewalk, there shall be a building interface zone at least two feet wide [AT LEAST PART OF THE DEVELOPMENT’S FRONTAGE ALONG THE ENHANCED SIDEWALK SHALL FEATURE A PRINCIPAL BUILDING WITH A 20-FOOT MAXIMUM SETBACK IN COMPLIANCE WITH SUBSECTION 21.06.030C.5].



Enhanced Street Sidewalks in [A]Commercial and Residential Settings

(Note to code revisor: replace existing illustration with illustrations above.)

[I. A PUBLIC USE EASEMENT SHALL BE RECORDED FOR ANY PART OF THE DESIGNATED SIDEWALK TO BE LOCATED WITHIN THE SUBJECT PARCEL.

II. PHYSICAL OBSTRUCTIONS WITHIN THE SIDEWALK'S BUILDING INTERFACE ZONE, SUCH AS LANDSCAPING, ENTRY STOOPS, OR SEATING, SHALL EXTEND NO MORE THAN TWO FEET INTO THE MINIMUM REQUIRED 12 FOOT WIDTH, SO THAT AT LEAST TEN FEET REMAIN.]

b. The enhanced street sidewalk shall provide at least half[TWO-THIRDS] the number of trees and shrubs that would otherwise have been required for site perimeter landscaping. Shrubs are not required if perennials are substituted for shrubs on a three to one basis or if the tree planting bed is provided as a suspended pavement system with a minimum of 300 cubic yards of soil per tree.

c. The enhanced street sidewalk may be placed wholly or in part within a right-of-way, subject to approval of the traffic engineer and municipal engineer.

i. The enhanced street sidewalk shall be subject to the applicable requirements of title 24, including sections 24.30.020., *Permit to use Public Places*, and 24.90, *Encroachment Permit*.

[II. IMPROVEMENTS WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSISTENT WITH THE DCM AND MASS.]

ii[III]. Existing improvements that meet the standards of the enhanced street sidewalk may be counted towards the requirements of this section, subject to approval by the director.

iii[IV]. The owner shall maintain landscaping and amenities for the enhanced street sidewalk within the right-of-way[, AND COMPLY WITH THE PROVISIONS FOR REMOVAL OF SNOW AND ICE IN AMC 24.80.090, 100, AND 110].

1 iv[V]. Where the right-of-way is not adequate or
2 cannot be configured to accommodate the
3 enhanced street sidewalk, then the
4 development shall be set back from the street
5 frontage as necessary to accommodate part of
6 the improvements within the property. A public
7 use easement shall be recorded for any part of
8 the designated sidewalk width to be located
9 within the subject parcel.

10
11 21[18]. Shared p[P]arking courtyard.
12 *** *** ***

13 (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2015-82, 7-28-15;
14 AO 2015-100, 10-13-15; AO 2017-55, 4-11-17; AO 2020-38, 4-28-20)

15 *** *** ***

16
17 **21.07.090 Off-Street Parking and Loading**

18 *** *** ***

19 H. *Parking and loading facility design standards.*

20 *** *** ***

21 2. *Applicability.* These standards apply to all areas used for off-
22 street parking, and any parking facility or loading facility
23 [INCLUDING ALL PARKING SPACES] in a development,
24 except where stated otherwise. A temporary parking lot shall
25 comply with all applicable development requirements of this
26 title for surface parking lots and parking lot landscaping,
27 except when associated with another temporary use
28 permitted pursuant to section 21.05.080.

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30 *** *** ***

31 8. *Vehicular Access and Circulation – General.* Parking lots and
32 structures access, layout, and dimensions shall be designed
33 for a safe and orderly flow of traffic throughout the site, as
34 provided in [THE] subsections 9. through 12. that follow.

35
36 [A. *KEY ELEMENTS.* THE PARKING FACILITY LAYOUT,
37 CIRCULATION, AND DESIGN PLAN SHALL
38 ADDRESS THE FOLLOWING ELEMENTS AS THEY
39 RELATE TO PARKING LOTS, INCLUDING BUT NOT
40 LIMITED TO: FIRE LANES, EMERGENCY ACCESS,
41 DRIVE-THROUGHS, QUEUING SPACES,
42 PASSENGER LOADING ZONES, PEDESTRIAN
43 CIRCULATION, AND LOADING BERTHS.]

44
45 [B. CIRCULATION PATTERNS. INTERNAL] Vehicular
46 access and circulation patterns and the location and
47 traffic direction of all circulation aisles, driveways, and
48 queuing lanes shall be designed and maintained in

1 accordance with [THE MUNICIPAL DRIVEWAY
2 STANDARDS CURRENTLY ESTABLISHED BY THE
3 TRAFFIC ENGINEER, AND WITH] accepted principles
4 of traffic engineering and safety as determined by the
5 traffic engineer.], PER THE TRAFFIC ENGINEER'S
6 REVIEW BASED ON THE CURRENT MANUALS OF
7 THE INSTITUTE OF TRANSPORTATION
8 ENGINEERS AND THE URBAN LAND INSTITUTE,
9 AND THE MANUAL OF UNIFORM TRAFFIC
10 CONTROL DEVICES OR THE SUCCESSOR
11 DOCUMENTS. CIRCULATION PATTERNS WITHIN
12 PARKING FACILITIES SHALL BE WELL DEFINED
13 WITH PAVEMENT MARKING AND SIGNAGE,
14 CURBS, LANDSCAPING, LANDSCAPED ISLANDS,
15 AND/OR OTHER SIMILAR FEATURES. IN ORDER
16 TO DEFINE CIRCULATION AND PROVIDE BETTER
17 SIGHT DISTANCE, CURBED END ISLANDS SHALL
18 BE REQUIRED AT THE END OF EACH ROW OF
19 PARKING SPACES. WHERE LOADING FACILITIES
20 OR ON-SITE REFUSE COLLECTION ARE
21 PROVIDED, COMMERCIAL TRUCK CIRCULATION
22 SHALL BE CONSIDERED, AND TRUCK TURNING
23 RADII SHALL BE SHOWN ON THE PARKING
24 FACILITY LAYOUT, CIRCULATION, AND DESIGN
25 PLAN WHEN REQUIRED BY THE TRAFFIC
26 ENGINEER.]

27
28 [C. *PARKING SPACES ALONG MAJOR SITE*
29 *ENTRANCE DRIVES.* THE PROVISION, LOCATION,
30 DESIGN, AND DIMENSIONS OF PARKING SPACES
31 ON A MAJOR ACCESS DRIVEWAY THAT SERVES
32 AS AN ENTRY OR EXIT FOR A LARGE
33 ESTABLISHMENT WITH MULTIPLE LOTS, TRACTS,
34 OR BUSINESSES, SHALL CONFORM TO
35 MUNICIPAL STANDARDS FOR ON-STREET
36 PARKING AND BE SUBJECT TO REVIEW AND
37 APPROVAL BY THE TRAFFIC ENGINEER.]

38
39 9. Vehicular Access Location

40
41 a. *Street Access Location.* The number and spacing of
42 driveways, including minimum distance from street
43 intersections, shall be as provided in the *Municipal*
44 *Driveways Standards* and this section. Access to
45 streets owned by the state of Alaska requires
46 compliance with state driveway standards, department
47 of transportation and public facilities approval and
48 driveway permit.

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- b. *Alley Access Requirement.* Where a residential use is served by an alley in the Urban Neighborhood Development Context (21.07.010E.), vehicle access shall take place from the alley, except for direct vehicle access to the street that is limited to the following:
 - i. One driveway no wider than 12 feet at any point, except that lot frontages 100 feet or wider on a primary street frontage may have one driveway per 50 feet of lot frontage on that street.
 - ii. One additional driveway no wider than 12 feet providing access to the secondary street frontage on corner lots for multifamily, townhouse, or two-family developments.
 - iii. The director and traffic engineer may approve additional driveway access to the primary street frontage for multifamily or townhouse developments, provided the alley is unimproved and the traffic engineer determines that alley improvement and vehicle access are not feasible or would create a traffic impact or safety hazard. The additional driveway access shall be the minimum the traffic engineer determines is necessary to provide access for the development. The department shall keep record of the approved additional driveway access with written findings supporting the approval on file and available for public inspection.

- c. *Cross-Access and Shared Access with Adjacent Sites.* Parking facilities serving a site, whether located on that same lot or on an adjacent lot, may be connected by means of a common access driveway within or between the interior of such lots. Where a property receives its access to the street through a shared driveway with another lot, a shared access easement shall be provided on the plat, or a shared access agreement running with the land shall be recorded by the municipality, as approved and executed by the director, guaranteeing the continued availability of the shared access between the properties.

- d. *Paired Residential Driveways Allowed.* Driveways for two single-family, two-family, or townhouse units may be attached across a property line, provided the maximum combined width of the driveway is 24 feet.

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e. Residential Garage Setback. Street-facing garages accessed from the driveway of an individual dwelling shall be set back from the street by the length of a standard parking space (as defined in table 21.07-8, Parking Angle, Stall, and Aisle Dimensions). The traffic engineer may approve exceptions to relieve hardship on narrow lots.

10. Access to Parking Spaces.

a. Access to Parking Spaces. Each parking space shall open to a parking aisle or driveway of such width and design as provided in subsection 21.07.090H.12. to provide safe and efficient means of vehicular access with no more than a standard two-movement entrance or exit from the parking space and without having to move another vehicle. Stacked and tandem parking spaces are allowed exceptions as provided in 21.07.090H.12.

b. Backing Distances. Adequate ingress and egress to each parking space shall be provided without backing more than 25 feet.

c. Dead-End Parking Aisles. Any parking bay that does not provide two means of vehicle egress shall provide, at the closed end, an extension of the parking aisle at least nine feet in depth, designated and signed as a "No Parking" turn-around area. Dead-end parking bays longer than 100 feet shall be subject to discretionary review and approval by the traffic engineer.

d. Vehicle Maneuvering. Off-street parking facilities shall be designed so that all vehicle maneuvers occur on the development site and not in the public right-of-way, and vehicles enter and exit the right-of-way in a forward motion, except the following are exempt:

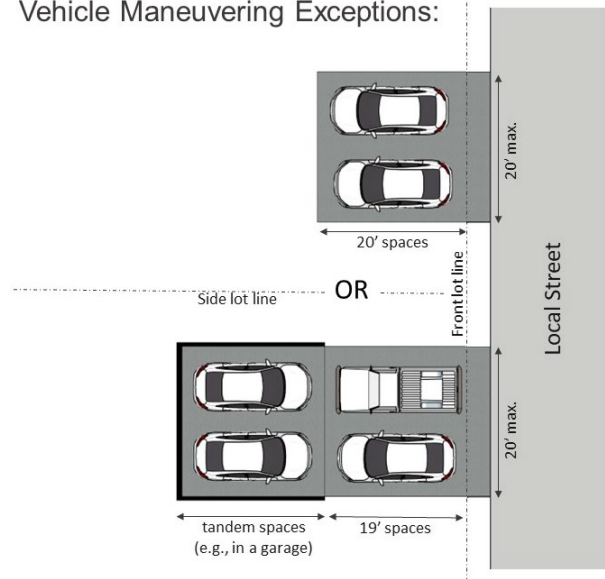
i. Parking for single-family, two-family, and mobile home dwellings on individual lots, accessing a local street.

ii. For other developments, parking comprising only one or two parking spaces whose only access is to a local street, provided that the vehicle area occupies no more than 20 feet of the lot frontage in the front setback. The number

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of spaces may be increased to four spaces if arranged in tandem for residential dwellings as provided in 21.07.090K.12.k. Additional spaces for multifamily developments with up to four dwelling units may be approved by the traffic engineer in appropriate circumstances such as lots on dead-end streets, cul-de-sacs, or other local streets with low traffic volumes.

Vehicle Maneuvering Exceptions:



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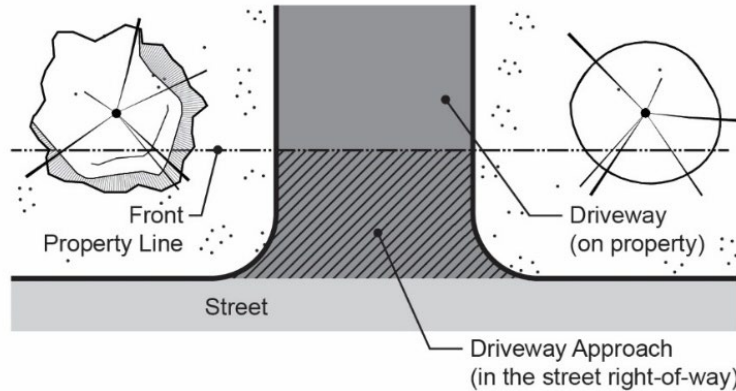
iii. The usable portion of an alley may be credited as circulation and/or parking aisle space for parking areas. The maneuvering area between the end of the parking space and the opposite side of the improved alley shall meet the parking aisle width in table 21.07-8, Parking Angle, Stall, and Aisle Dimensions.

11. Driveway Design and Dimensions

a[D]. Parking Lot Entries/Driveway Approaches. Entries and driveway approaches providing access from the street edge to the front property line [TO PARKING LOTS] shall conform to the municipal driveway standards [CURRENTLY ESTABLISHED BY THE TRAFFIC ENGINEER] and this subsection 21.07.090H.11. Access to streets owned by the state of Alaska requires compliance with state driveway standards, as provided in 21.07.090H.9. [DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES APPROVAL, AND A CURRENT VALID STATE OF

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ALASKA DRIVEWAY PERMIT. INGRESS AND EGRESS TO PARKING FACILITIES SHALL BE DESIGNED TO MAINTAIN ADEQUATE SIGHT DISTANCE AND SAFETY AND AS PRESCRIBED IN THE MUNICIPAL DRIVEWAY STANDARDS.]



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b. Curb Openings and Public Walkway Crossings.

- i. Curb cut and curb returns at driveway openings to the street shall be provided as prescribed in the municipal driveway standards.
- ii. Public walkways shall be maintained or restored to the maximum running slope and cross-slope prescribed by M.A.S.S. and A.D.A., except that in the urban neighborhood context areas (21.07.010E.), public walkways on local streets shall be restored to a level running grade to the extent reasonably feasible.

c. Driveway Approach (in ROW) as a Percentage of Lot Frontage Width.

I. RESIDENTIAL USES. RESIDENTIAL DRIVEWAY ENTRANCES SHALL COMPLY WITH SUBSECTION 21.07.110F.3., DRIVEWAY WIDTH.

II. NONRESIDENTIAL USES.]
The total width of the driveway approach [ENTRANCES TO A NONRESIDENTIAL LOT] from a street shall not exceed 40 percent of the frontage of the lot, or 33 percent of the frontage if the platting authority or traffic engineer finds that conditions warrant it, provided the following: [, UNLESS THE APPLICANT

1 PROVIDES FOR SNOW STORAGE IN A MANNER
2 APPROVED BY THE DECISION-MAKING BODY.]

3
4 i. The driveway approach is always allowed to
5 have the minimum driveway width provided by
6 subsection d. The traffic engineer may approve
7 more than the minimum driveway width,
8 provided the traffic engineer determines that
9 snow storage, on-street parking, traffic flow and
10 safety, and the neighborhood context are
11 addressed.

12
13 ii. The driveway approach shall not exceed the
14 maximum driveway width established in the
15 municipal driveway standards.

16
17 iii. Flag lots are exempt from the percentage
18 limitations but shall have a maximum driveway
19 approach width of 20 feet. Abutting flag lots may
20 share a driveway approach up to 24 feet wide
21 (12 feet per lot).

22
23 d. Minimum Driveway Width. The minimum required
24 width of driveways including the driveway approach
25 within the street ROW and the portion of the driveway
26 on the development property is as follows:

27
28 i. The minimum width of parking aisles shall be as
29 set forth in 21.07.090H.12.

30
31 ii. The minimum width of the driveway approach
32 shall be as set forth in the municipal driveway
33 standards, except as modified or clarified by
34 subsections iii. through vi.

35
36 iii. The minimum width of a driveway providing
37 access to a single-family, two-family,
38 townhouse, mobile home, or other individual
39 dwelling from a local street or alley is 12 feet.

40
41 iv. The minimum width of a driveway providing
42 access for up to 10 parking spaces serving a
43 townhouse or multifamily residential use from a
44 local street or alley is 12 feet, except that
45 straight (non-curving) driveway segments on the
46 development property (i.e., not in the driveway
47 approach) may be 10 feet.
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v. The minimum width of a driveway providing access for more than 10 parking spaces serving a townhouse or multifamily use from a local street or alley is 12 feet for one-way traffic and 20 feet for two-way traffic.

vi. The traffic engineer may reduce the driveway width to relieve hardship on small or narrow lots or increase the required width to meet vehicle turning and maneuvering needs, in accordance with accepted principles of traffic engineering and safety as determined by the traffic engineer.

e. Driveway Throat Length. The driveway throat shall be of sufficient length to enable the street and walkways in the ROW and the circulation, parking, and walkways in the development site to function without interfering with each other, as provided in the municipal driveway standards.

f. Sight Distance. Ingress and egress to parking facilities shall be designed as prescribed in the municipal driveway standards to maintain adequate sight distance and safety.

g. Circulation Definition. Circulation patterns within parking facilities shall be well defined with pavement marking and signage, curbs, landscaping, and/or other similar features. Curbed end islands shall be provided at the end of each row of parking spaces to define circulation and provide sight distance at internal intersections of parking aisles, driveways, and/or on-site roadways. Shared parking courtyards meeting 21.07.060G.21. may depart from this requirement.

[E. **PARKING AND MANEUVERING.** ALL CIRCULATION AISLES, DRIVEWAYS, AND VEHICLE MANEUVERING AREAS REQUIRED BY THIS SECTION SHALL BE LOCATED ENTIRELY OFF-STREET AND ON THE PROPERTY UNLESS SPECIFICALLY PROVIDED OTHERWISE BY THIS SECTION.

I. **ACCESS TO PARKING SPACES.** TO ENSURE SAFE AND EFFICIENT VEHICULAR ACCESS TO PARKING SPACES, EACH OFF-STREET PARKING SPACE SHALL OPEN DIRECTLY ON A PARKING AISLE OR DRIVEWAY OF

1 SUCH WIDTH AND DESIGN AS PROVIDED IN
2 TABLE 21.07-7 AND THE ILLUSTRATIONS
3 THAT FOLLOW THE TABLE. ADEQUATE
4 INGRESS AND EGRESS TO EACH PARKING
5 SPACE SHALL BE PROVIDED WITHOUT
6 BACKING MORE THAN 25 FEET.

7
8 II. *MANEUVERING AREA.* OFF-STREET
9 PARKING FACILITIES SHALL BE DESIGNED
10 WITH SUFFICIENT MANEUVERING ROOM
11 SO THAT ALL MANEUVERS ASSOCIATED
12 WITH THE PARKING SHALL OCCUR IN THE
13 OFF-STREET PARKING FACILITY, AND
14 THAT ALL VEHICLES ENTER THE ABUTTING
15 STREET IN A FORWARD MOTION.

16
17 III. *SOME DWELLINGS EXEMPTED.* SINGLE-
18 FAMILY, TWO-FAMILY, TOWNHOUSE, AND
19 MOBILE HOME DWELLINGS ON INDIVIDUAL
20 LOTS SHALL BE EXEMPTED FROM THIS
21 SUBSECTION. MULTIFAMILY DWELLINGS
22 WITH UP TO FOUR UNITS SHALL BE
23 EXEMPTED FROM THIS SUBSECTION IN
24 APPROPRIATE CIRCUMSTANCES IF
25 APPROVED BY THE TRAFFIC ENGINEER.
26 APPROPRIATE CIRCUMSTANCES MAY
27 INCLUDE LOTS WITH ALLEY ACCESS, LOTS
28 LOCATED ON LOW-VOLUME STREETS, AND
29 LOTS LOCATED ON DEAD-END STREETS
30 OR CUL-DE-SACS.

31
32 F. *DEAD-END PARKING AISLES.* DEAD-END
33 PARKING AISLES MAY BE ALLOWED ONLY WITH
34 THE APPROVAL OF THE TRAFFIC ENGINEER.

35
36 G. *ALLEYS.* SUBJECT TO SAFETY APPROVAL BY THE
37 TRAFFIC ENGINEER, THE USABLE PORTION OF
38 AN ALLEY MAY BE CREDITED AS CIRCULATION
39 AND/OR PARKING AISLE SPACE.

40
41 H. *CROSS ACCESS AND JOINT ACCESS WITH*
42 *ADJACENT SITES.* THE PLAN SHALL SHOW
43 EXISTING PARKING AND CIRCULATION
44 PATTERNS ON ADJACENT PROPERTIES AND
45 POTENTIAL CONNECTIONS. REQUIRED PARKING
46 LOTS SERVING A SITE, WHETHER LOCATED ON
47 THAT SAME LOT OR ON AN ADJACENT LOT, MAY
48 BE CONNECTED BY MEANS OF A COMMON

ACCESS DRIVEWAY WITHIN OR BETWEEN THE INTERIOR OF SUCH LOTS. APPLICANTS ARE ENCOURAGED TO PROVIDE SHARED VEHICLE AND PEDESTRIAN ACCESS TO ADJACENT PROPERTIES FOR CONVENIENCE, SAFETY, AND EFFICIENT CIRCULATION. AN ACCESS EASEMENT SHALL BE PROVIDED ON THE PLAT, OR A SHARED ACCESS AGREEMENT RUNNING WITH THE LAND SHALL BE RECORDED BY THE MUNICIPALITY, AS APPROVED AND EXECUTED BY THE DIRECTOR, GUARANTEEING THE CONTINUED AVAILABILITY OF THE SHARED ACCESS BETWEEN THE PROPERTIES.]

12[9]. Dimensions of Parking Spaces and Aisles.

- a. *Minimum Dimensions for Required Parking.* Parking shall meet or exceed the STANDARD parking space and aisle dimensions set forth in table 21.07-8[6], except as follows:
 - i. Parking facilities in the Downtown (DT) zoning districts, and the Urban Neighborhood Contexts delineated in subsection 21.07.010E. [TRADITIONAL NEIGHBORHOOD DESIGN AREAS DESIGNATED ON MAP 2-1: ANCHORAGE 2040 LAND USE PLAN MAP IN THE ANCHORAGE 2040 LAND USE PLAN,] may meet the SMALL parking space and aisle dimensions in table 21.07-8[6]. Up to 30 percent of parking in the Downtown (DT) zoning districts may meet the COMPACT parking dimensions, if signed for compact vehicles only.

TABLE 21.07-8[6] PARKING ANGLE, STALL, AND AISLE DIMENSIONS									
Parking Angle (Degrees)	Parking Space Size	Space Width	Curb Length (Width Projection)	Space Depth (Vehicle Projection)	Aisle Width 1-way	Aisle Width 2-way	Typical Parking Bay Width (Module)	Interlock Reduction	Overhang Allowance
***	***	***							
¹ Developments in the Urban Neighborhood Development Context Areas delineated in 21.07.010E.[TRADITIONAL NEIGHBORHOOD DESIGN AREAS DESIGNATED ON MAP 2-1: ANCHORAGE 2040 LAND USE PLAN MAP IN THE ANCHORAGE 2040 LAND USE PLAN] may use the following alternative STANDARD parking space depth (Vehicle Projection): 19' 0" for 90-degree parking angle; 20' 8" for 75-degree parking angle; 20' 11" for 60-degree parking angle, and 19' 6" for 45-degree parking angle. Parking bay width may decrease as a result.									

*** *** ***

(AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2014-58, 5-20-14;
AO 2015-82, 7-28-15; AO 2015-100, 10-13-15; AO 2015-131, 1-12-16; AO
2016-3(S), 2-23-16; AO 2017-55, 4-11-17; AO 2017-176, 1-9-18; AO 2019-
132, 12-3-19; AO 2020-38, 4-28-20)

*** **

21.07.110 Residential Design Standards.

A. Purpose.

*** **

[4. ENHANCE PUBLIC SAFETY BY PROMOTING LINES OF SIGHT TO RESIDENTIAL ENTRIES, CLEARLY DEFINING TRANSITIONS FROM PUBLIC TO MORE PRIVATE RESIDENTIAL OUTDOOR SPACES, AND DESIGNING ENTRANCES TO BE PROMINENT.

5. LOCATE ACTIVE LIVING SPACES, ENTRANCES, AND WINDOWS TO IMPROVE THE PHYSICAL AND VISUAL CONNECTION FROM RESIDENCES TO THE STREET, AND FOSTER OPPORTUNITIES FOR CASUAL SURVEILLANCE OF THE STREET AND OUTWARDLY EXPRESSED PROPRIETORSHIP OF THE NEIGHBORHOOD.]

(Note to code revisor: Re-number subsequent subsections.)

*** **

C. Standards for Multifamily and Townhouse Residential.

1. Purpose.

*** **

[G. PROMOTE A SAFE LIVING ENVIRONMENT THROUGH “EYES ON THE STREET” DESIGN ELEMENTS, SUCH AS PLACEMENT OF WINDOWS, AND TRANSITION SPACES BETWEEN THE PUBLIC REALM AND THE MORE PRIVATE AREAS OF THE DEVELOPMENT.]

(Note to code revisor: Re-number remaining subsections.)

*** **

3. Pedestrian-Oriented Street Frontage Standards. Provisions for site planning and building orientation in relationship to street frontages and pedestrian access are provided in subsection 21.07.060F., Pedestrian Frontage Standards. [WINDOWS FACING THE STREET.

A. WINDOWS. WINDOWS OR PRIMARY ENTRANCE DOORS SHALL BE PROVIDED ON EACH BUILDING

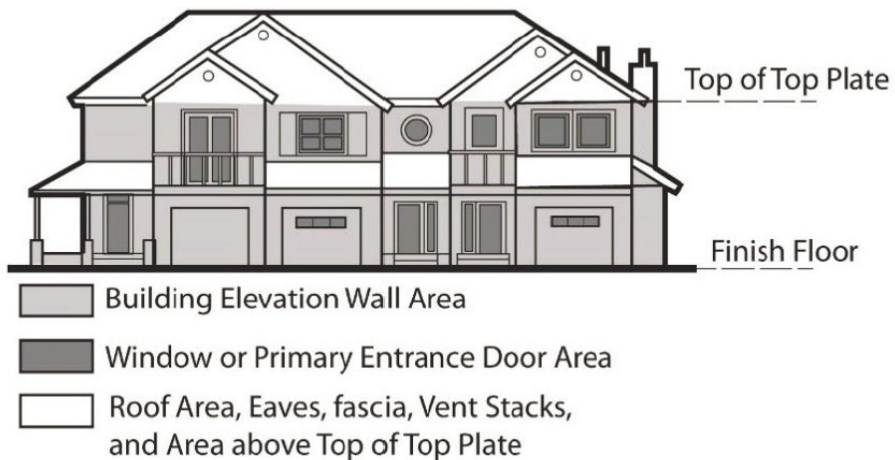
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ELEVATION FACING A STREET OR THAT HAS PRIMARY ENTRANCES TO DWELLINGS (UP TO A MAXIMUM OF TWO ELEVATIONS). AT LEAST 10 PERCENT OF THE WALL AREA OF THE BUILDING ELEVATION SHALL BE WINDOWS OR PRIMARY ENTRANCE DOORS. THE DIRECTOR MAY ELIMINATE OR REDUCE THE REQUIREMENT ON ONE ELEVATION (IN INSTANCE WHERE UP TO TWO APPLY) BASED ON SITE SPECIFIC CIRCUMSTANCES (SUCH AS INFILL PROJECTS WHERE AN ENTRY FACES AN ADJACENT PROPERTY RATHER THAN THE STREET).

B. *WALL AREA CALCULATION.* THE AREA OF VERTICAL WALL SURFACE MEASURED FROM FINISHED FLOOR OF EACH LEVEL, INCLUDING GARAGES TO TOP OF TOP PLATE. IN THE CASE OF A BASEMENT WALL, CALCULATE THE AREA OF VERTICAL WALL SURFACE MEASURED FROM FINISHED GRADE TO THE TOP OF THE FINISHED FLOOR ABOVE OR TOP OF TOP PLATE, WHICHEVER IS APPLICABLE. ANY WALL AREA ABOVE THE TOP PLATE SHALL NOT APPLY, INCLUDING ANY GABLE ENDS. THE FAÇADE WALL AREA OF STORIES BELOW GRADE PLANE, SUCH AS BELOW GRADE PARKING, ARE EXEMPT FROM THE MEASUREMENT OF WALL AREA. THE ROOF, INCLUDING EAVES, FASCIA, AND VENT STACKS, IS ALSO EXEMPT, AS ILLUSTRATED BELOW.]

*** *** ***

(Note to code revisor: Delete the following illustration.)



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[C. *ENERGY EFFICIENCY EXCEPTION.* A REDUCTION IN REQUIRED WINDOW AREA IS PERMITTED IF DEMONSTRATED BY CALCULATION BY AN ENERGY RATER CERTIFIED BY THE STATE OF ALASKA THAT INSTALLING THE REQUIRED WINDOW AREA WILL REDUCE THE ENERGY RATING BELOW A 5-STAR ENERGY RATING.

D. *ADDITIONS AND RENOVATIONS.* ONLY THE WALL AREA AFFECTED BY EITHER AN ADDITION OR A RENOVATION SHALL BE REQUIRED TO COMPLY WITH THE OPENING REQUIREMENTS. UNAFFECTED WALL AREAS NEED NOT COMPLY. GARAGE ADDITIONS ARE EXEMPT FROM OPENING REQUIREMENTS. THE DIRECTOR MAY ELIMINATE OR REDUCE THE REQUIREMENT FOR WINDOW OPENINGS ON ADDITIONS/RENOVATIONS BASED ON PROJECT SPECIFIC CIRCUMSTANCES (SUCH AS THE NATURE OF THE SPACES BEING ADDED, BUILDING ORIENTATION, OR STRUCTURAL REQUIREMENTS).]

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[5. *PEDESTRIAN ACCESS.* WALKWAY CONNECTIONS FROM PRIMARY FRONT ENTRANCES TO THE STREET ARE REQUIRED IN ACCORDANCE WITH SUBSECTION 21.07.060E.4., EXCEPT THAT DEVELOPMENTS MAY PROVIDE ONE OF THE FOLLOWING ALTERNATIVES INSTEAD:

A. PRIMARY FRONT ENTRANCES FOR INDIVIDUAL DWELLINGS MAY CONNECT TO THE STREET BY THE DWELLING UNIT'S INDIVIDUAL DRIVEWAY IF SUCH IS PROVIDED;

B. A PARKING COURTYARD MAY BE PROVIDED IN CONFORMANCE WITH SUBSECTION 21.07.060F.18., OR

C. OTHER METHODS, AS APPROVED BY THE DIRECTOR, THAT PROVIDE SAFE, CONVENIENT, AND ADEQUATE PEDESTRIAN ACCESS.

6. *BUILDING AND SITE ORIENTATION MENU.* BUILDINGS SHALL BE ORIENTED TO SURROUNDING STREETS, SIDEWALKS, COMMON PRIVATE OPEN SPACES, AND THE NEIGHBORHOOD PUBLIC REALM THROUGH AT LEAST THREE OF THE FOLLOWING MENU CHOICES.

1 THE DIRECTOR MAY REDUCE THE REQUIREMENT TO
2 TWO MENU CHOICES IF HE OR SHE DETERMINES THAT
3 THE PRIMARY OBJECTIVE OF THE MENU OPTION IS
4 ACHIEVED OR THAT THE SPECIFIC SITE LIMITS
5 AVAILABILITY OF A MAJORITY OF THE OPTIONS.
6

7 A. *COURTYARD HOUSING.* ARRANGE OR
8 CONFIGURE THE BUILDING(S) TO ENCLOSE AND
9 FRAME A HOUSING COURTYARD AS DESCRIBED
10 IN SUBSECTION 21.07.060F.7.
11

12 B. *ORIENTATION OF LIVING SPACES AND*
13 *WINDOWS—GROUND FLOOR.* PROVIDE
14 WINDOWS AND/OR ENTRANCES TO HABITABLE
15 LIVING SPACES COMPRISING, AT LEAST 10
16 PERCENT OF THE GROUND-FLOOR WALL AREA
17 OF UP TO TWO BUILDING ELEVATIONS FACING A
18 STREET OR HAVING A PRIMARY FRONT
19 ENTRANCE. WINDOWS IN A GARAGE DOOR DO
20 NOT COUNT TOWARDS THE MINIMUM AREA IN
21 THIS SECTION.
22

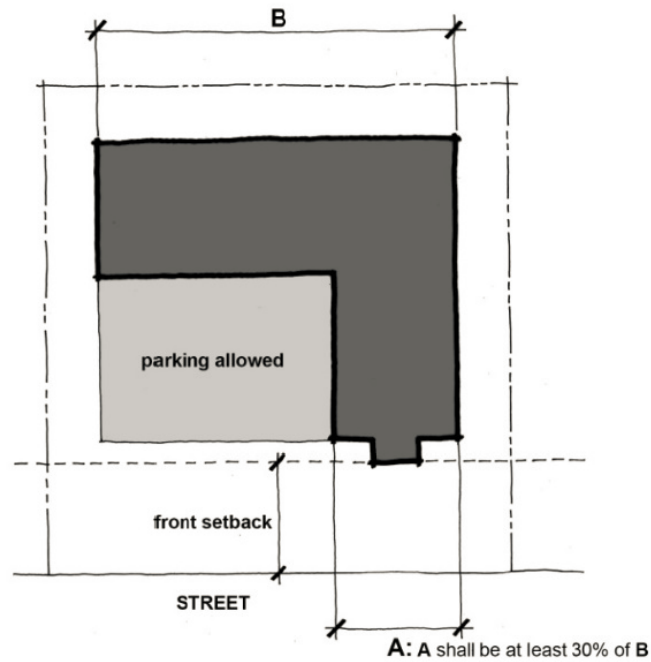
23 C. *ORIENTATION OF LIVING SPACES AND*
24 *WINDOWS—OVERALL DEVELOPMENT.* PROVIDE
25 WINDOWS AND/OR ENTRANCES TO HABITABLE
26 LIVING SPACES, COMPRISING AT LEAST 10
27 PERCENT OF THE TOTAL WALL AREA OF UP TO
28 TWO BUILDING ELEVATIONS FACING A STREET
29 OR HAVING A PRIMARY FRONT ENTRANCE.
30 WINDOWS IN A GARAGE DOOR DO NOT COUNT
31 TOWARDS THE MINIMUM AREA OF THIS SECTION.
32

33 D. *ORIENTATION OF LIVING SPACES AND*
34 *WINDOWS—ADDITIONAL TRANSPARENCY.*
35 INCREASE THE PERCENTAGE OF THE WALL
36 AREA COMPRISED OF WINDOWS AND/OR
37 PEDESTRIAN ENTRANCES TO 20 PERCENT IN
38 SUBSECTIONS 6.B. AND/OR 6.C. ABOVE.
39

40 E. *STREET FRONTAGE – PARKING BESIDE OR*
41 *BEHIND THE BUILDING.* NO MORE THAN 70
42 PERCENT OF A STREET-FACING BUILDING
43 ELEVATION SHALL HAVE PARKING FACILITIES
44 (INCLUDING GARAGES) BETWEEN IT AND THE
45 STREET.]
46

47 *(Note to code revisor: Delete the following illustration.)*

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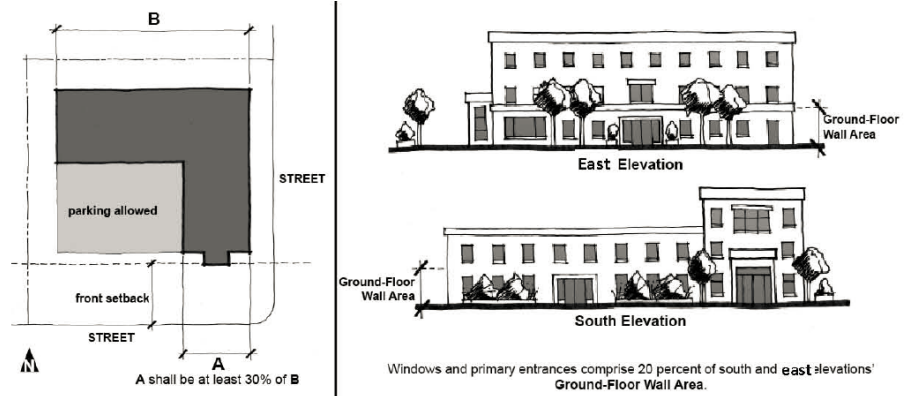


- [F. *STREET FRONTAGE – LIMITED PARKING WIDTH.* AS AN ALTERNATIVE TO 6.E. ABOVE, LIMIT DRIVEWAYS AND PARKING FACILITIES TO NO MORE THAN 50 PERCENT OF THE TOTAL SITE AREA BETWEEN THE BUILDING AND A STREET, AND GARAGES TO NO MORE THAN 50 PERCENT OF THE STREET FACING BUILDING ELEVATION.

- G. *STREET CORNER BUILDING.* ON A CORNER LOT, PROVIDE CHOICES 6.E. AND 6.F. ON BOTH STREET FRONTAGES, SUCH THAT THE BUILDING (INCLUDING ITS HABITABLE FLOOR AREA WITH WINDOWS) IS PLACED NEAREST THE CORNER, AND ANY PARKING FACILITIES ARE LOCATED BESIDE OR BEHIND THE BUILDING AWAY FROM THE STREET CORNER. COMPLIANCE WITH THIS FEATURE COUNTS FOR MEETING TWO.]

(Note to code revisor: Delete the following illustration.)

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- [H. *LIMITED FRONT-FACING GARAGE WIDTH FOR TOWNHOUSES.* WHERE GARAGES FOR INDIVIDUAL TOWNHOUSE-STYLE DWELLING UNITS FACE THE STREET OR ARE ON THE SAME BUILDING ELEVATION AS THE PRIMARY FRONT ENTRY TO THE DWELLINGS, LIMIT THE GARAGE DOOR WIDTH TO NO MORE THAN 50 PERCENT OF THE WIDTH OF EACH DWELLING, OR UP TO 67 PERCENT PROVIDED THE BUILDING FAÇADE ACHIEVES ONE MORE FEATURE THAN REQUIRED IN SUBSECTION C.7., BUILDING ARTICULATION MENU.
- I. *ENHANCED SIDEWALK.* PROVIDE AN ENHANCED SIDEWALK CONFORMING TO SUBSECTION 21.07.060F.17. ON AT LEAST ONE STREET FRONTAGE THAT IS NOT LESS THAN 100 LINEAR FEET.
- J. *SEPARATED WALKWAY TO THE STREET.* CONNECT ALL PRIMARY ENTRANCES TO THE STREET BY A CLEAR AND DIRECT WALKWAY SEPARATED FROM AND NOT ROUTED THROUGH A PARKING FACILITY.
- K. *VISIBLE FRONT ENTRIES.*
 - I. PLACE THE PRIMARY FRONT ENTRANCE(S) (AT LEAST ONE SHARED PRIMARY FRONT ENTRANCE FOR A MULTIFAMILY STRUCTURE OR AT LEAST 50 PERCENT OF INDIVIDUAL UNIT ENTRANCES) ON A STREET-FACING BUILDING ELEVATION, OR FACING A COMMON PRIVATE OPEN SPACE THAT IS VISIBLE FROM AND HAS DIRECT ACCESS TO THE STREET.

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II. AS AN ALTERNATIVE, PLACE THE PRIMARY FRONT ENTRANCE(S) ON A FAÇADE AT AN ANGLE OF UP TO 90 DEGREES FROM THE STREET, WHERE THERE IS AN UNOBSTRUCTED LINE OF SIGHT FROM THE STREET OR SIDEWALK EDGE (ABUTTING THE SITE) TO THE ENTRY DOOR, AND A SHELTERING ROOF STRUCTURE NO MORE THAN 12 FEET ABOVE THE FLOOR OF THE ENTRY AS A PERMANENT ARCHITECTURAL FEATURE PROJECTING FROM THE FAÇADE.

L. *ENHANCED FRONT YARD LANDSCAPING.* PROVIDE ONE LEVEL HIGHER OF SITE OR PARKING LOT PERIMETER LANDSCAPING ALONG THE STREET FRONTAGES (UP TO A MAXIMUM OF TWO FRONTAGES) THAN OTHERWISE REQUIRED BY THIS TITLE. FOR EXAMPLE, IF L1 LANDSCAPING IS REQUIRED, PROVIDE L2 LANDSCAPING.

M. *SITE ENTRY FEATURE.* HIGHLIGHT AND DEFINE A PEDESTRIAN AND/OR VEHICLE ENTRANCE TO A DEVELOPMENT SITE USING THREE OR MORE OF THE FOLLOWING ELEMENTS:

I. LANDSCAPE TREATMENT WITH SEASONAL COLOR AND TREES, WHICH CLEARLY DISTINGUISHES AND HIGHLIGHTS THE SITE ENTRY.

II. PLAZA OR COURTYARD AS DESCRIBED IN SUBSECTION 21.07.060F6. OR 7.

III. IDENTIFYING BUILDING PRIMARY ENTRANCE FORM INCLUDING A COVERED ENTRY, WHEN THE PRIMARY ENTRANCE IS WITHIN 40 FEET OF THE SITE ENTRANCE.

IV. SPECIAL PAVING, PEDESTRIAN SCALE LIGHTING, AND/OR BOLLARDS.

V. ORNAMENTAL GATE AND/OR FENCE.

N. *DIRECTOR APPROVAL.* OTHER METHODS, AS APPROVED BY THE DIRECTOR, THAT PROVIDE

1 APPROPRIATE BUILDING AND SITE LAYOUTS
2 RELATIVE TO THE SURROUNDING
3 NEIGHBORHOOD AND STREETS.]

4 *** **

5 4[7]. *Building Articulation Menu.*

6 *** **

7 5[8]. *Northern Climate Weather Protection and Sunlight Menu.*

8 *** **

9 [9. *ENTRYWAY TREATMENT.* PRIMARY FRONT
10 ENTRANCES SHALL BE GIVEN EMPHASIS AND
11 PHYSICAL ACCESS AS FOLLOWS. A PORCH, LANDING,
12 PATIO, OR OTHER SEMI-PRIVATE OUTDOOR
13 ENTRYWAY SPACE WITH A MINIMUM DIMENSION OF
14 FOUR FEET SHALL BE PROVIDED THAT IS
15 DISTINGUISHED FROM ADJACENT AREAS AND
16 VEHICULAR PARKING BY A VERTICAL SEPARATION OR
17 CHANGE IN SURFACE MATERIAL. EXAMPLES OF
18 FEATURES THAT MEET THE INTENT OF THE SECTION
19 INCLUDE THE FOLLOWING:

20
21 A. OUTDOOR SHELTERING ROOF STRUCTURE
22 SUCH AS AN OVERHANG, RECESS, PORTICO, OR
23 OTHER PERMANENT ARCHITECTURAL FEATURE
24 COVERING AT LEAST 12 SQUARE FEET.

25
26 B. FAÇADE VARIATION THROUGH WALL
27 MODULATION (CHANGES IN WALL PLANE) OR
28 CHANGES IN EXTERIOR FINISHES (COLOR OR
29 MATERIALS) THAT RELATE TO AND EMPHASIZE
30 THE ENTRY.

31
32 C. ENTRY DETAIL ELEMENTS SUCH AS DOUBLE
33 DOORS, ENTRY SIDE LIGHT OR TRANSOM
34 WINDOWS, OR PLANTER BOXES.

35
36 D. FRONT STEPS CREATING A VERTICAL
37 SEPARATION, AND/OR VERTICAL VEGETATION
38 OR LOW FRONT FENCING, TO DEFINE THE
39 TRANSITION FROM PUBLIC TO MORE PRIVATE
40 OUTDOOR ENTRYWAY SPACE.

41
42 E. LANDSCAPING AND PEDESTRIAN FEATURES,
43 NOT OTHERWISE REQUIRED BY THIS TITLE,
44 SUCH AS INTEGRATED PLANTERS, PEDESTRIAN-
45 SCALE LIGHTING OR ACCENT LIGHTING, OR
46 SPECIAL PAVING TREATMENTS.

1
2 F. OTHER METHODS AS APPROVED BY THE
3 DIRECTOR THAT EMPHASIZE PRIMARY FRONT
4 ENTRANCES.]
5

6 (Note to code revisor: Re-number remaining subsections.)
7 *** **

8 D. Standards for some single-family and two-family residential
9 structures.
10 *** **

11 (Note to code revisor: Re-number table 21.07-10 to become table
12 21.07-16.)
13 *** **

14 3. Pedestrian-Oriented Street Frontage Standards. Provisions
15 for site planning and building orientation in relationship to
16 street frontages and pedestrian access are provided in
17 subsection 21.07.060F., Pedestrian Frontage Standards.
18 [PRIMARY ENTRANCE.
19

20 A. A PORCH OR LANDING WITH AT LEAST 16
21 SQUARE FEET SHALL BE PROVIDED AT THE
22 PRIMARY ENTRANCE. THE PORCH OR LANDING
23 SHALL BE COVERED BY A ROOF OF AT LEAST 16
24 SQUARE FEET.
25

26 B. THE PRIMARY ENTRANCE OF EACH RESIDENCE
27 AND THE WALKWAY TO THAT ENTRANCE SHALL
28 BE CLEARLY VISIBLE FROM THE STREET.
29 PRIMARY ENTRANCES SHALL NOT BE LOCATED
30 ON THE REAR OF THE STRUCTURE.
31

32 C. A HARD-SURFACED PEDESTRIAN WALKWAY
33 SHALL BE PROVIDED FROM THE STREET,
34 SIDEWALK, OR DRIVEWAY TO THE PRIMARY
35 ENTRANCE. ROOF DRAINAGE SHALL NOT FALL
36 UPON THE WALKWAY.
37

38 4. GARAGES.
39

40 A. WHERE A GARAGE (WITH NO HABITABLE FLOOR
41 AREA ABOVE) EXTENDS FROM THE REST OF THE
42 STRUCTURE TOWARDS THE STREET, THE WIDTH
43 OF THE NON-GARAGE PORTION OF THE FRONT
44 BUILDING ELEVATION SHALL BE NO LESS THAN
45 THE LENGTH THAT THE GARAGE EXTENDS FROM
46 THE REST OF THE STRUCTURE.
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B. GARAGE DOORS FACING THE STREET SHALL COMPRISE NO MORE THAN 67 PERCENT OF THE TOTAL WIDTH OF A DWELLING'S BUILDING ELEVATION.

C. DWELLING UNITS WITH A STREET-FACING BUILDING ELEVATION THAT IS 40 FEET WIDE OR NARROWER AND WITH GARAGE DOORS THAT FACE THE STREET SHALL FEATURE AT LEAST ONE DESIGN ELEMENT FROM EACH OF THE THREE LISTS BELOW.

I. LIST A:

(A) AT LEAST ONE DORMER THAT IS ORIENTED TOWARD THE STREET.

(B) THE FRONT BUILDING ELEVATION HAS TWO OR MORE FACADES THAT ARE OFFSET BY AT LEAST 16 INCHES. EACH FAÇADE OR A COMBINATION OF OFFSET FACADES SHALL BE AT LEAST ONE THIRD OF THE AREA OF THE BUILDING ELEVATION.

(C) FRONT-FACING BALCONY, ACCESSIBLE FROM A HABITABLE ROOM, AT LEAST SIX FEET WIDE, THAT PROJECTS FROM A FAÇADE AT LEAST TWO FEET AND IS ENCLOSED BY AN OPEN RAILING.

II. LIST B:

(A) A PRIMARY ENTRANCE AREA WITH A COVERED PORCH OR LANDING AT LEAST EIGHT FEET WIDE, INCORPORATING VISUAL ENHANCEMENTS SUCH AS GABLED ROOF FORMS, ROOF BRACKETS, FASCIA BOARDS, SIDE LIGHTS, AND/OR ORNAMENTAL COLUMNS DIVIDED VISUALLY INTO TOP, MIDDLE, AND BOTTOM.

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(B) A BAY WINDOW ON THE FRONT ELEVATION AT LEAST SIX FEET WIDE THAT EXTENDS A MINIMUM OF 12 INCHES OUTWARD FROM A FAÇADE, FORMING A BAY OR ALCOVE IN THE ROOM WITHIN.

(C) IF THE GARAGE IS MORE THAN ONE CAR WIDE, MULTIPLE GARAGE DOORS ARE USED.

III. LIST C:

(A) WINDOWS AND PRIMARY ENTRANCE DOOR(S) THAT OCCUPY A MINIMUM OF 25 PERCENT OF THE WALL AREA OF THE FRONT ELEVATION. WINDOWS IN THE GARAGE DOOR DO NOT COUNT TOWARDS THE 25 PERCENT.

(B) TRIM (MINIMUM THREE AND ONE HALF INCHES WIDE) OF A DIFFERENT COLOR FROM THE PRIMARY SIDING COLOR, SHALL OUTLINE ALL WINDOWS, DOORS, AND ROOF EDGES ON THE FRONT BUILDING ELEVATION, AND MAY OUTLINE CORNERS AND PROJECTIONS/RECESSES ON THE FRONT BUILDING ELEVATION.

(C) A MINIMUM OF TWO DIFFERENT SIDING MATERIALS AND/OR PATTERNS ARE USED ON THE FRONT BUILDING ELEVATION. DOORS AND TRIM DO NOT QUALIFY AS A TYPE OF SIDING MATERIAL.

D. THE HOUSE MAY ENCROACH INTO THE PRIMARY FRONT SETBACK BY UP TO FIVE FEET WHEN THERE IS NO GARAGE, OR WHERE THERE IS A GARAGE (ATTACHED OR DETACHED) WHERE THE FRONT WALL OF THE GARAGE IS LOCATED AT LEAST 8 FEET BEHIND THE FRONT FAÇADE OF THE HOUSE.

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5. *WINDOWS.*

- A. *MINIMUM WINDOW AREA REQUIREMENT.* WINDOWS AND PRIMARY ENTRANCE DOOR(S) SHALL OCCUPY A MINIMUM OF 10 PERCENT OF THE WALL AREA OF A BUILDING ELEVATION FACING A STREET OR REQUIRED PRIVATE COMMON OPEN SPACE (UP TO A MAXIMUM OF TWO ELEVATIONS). THE DIRECTOR MAY ELIMINATE OR REDUCE THE REQUIREMENT ON ONE ELEVATION (IN INSTANCES WHERE UP TO TWO APPLY) BASED ON SITE SPECIFIC CIRCUMSTANCES, STRUCTURAL REQUIREMENTS FOR SHEAR, AND ORGANIZATION OF SPACES IN THE HOME (WINDOWS ARE NOT REQUIRED IN ROOMS NOT NORMALLY INHABITED OR IN GARAGES).
- B. *WALL AREA CALCULATION.* WALL AREA CALCULATION SHALL BE IN ACCORDANCE WITH SUBSECTION 21.07.110C.3.B.
- C. AN OVERALL REDUCTION IN REQUIRED WINDOW AREA MAY BE APPROVED IF DEMONSTRATED BY CALCULATION BY AN ENERGY RATER CERTIFIED BY THE STATE OF ALASKA THAT INSTALLING THE REQUIRED WINDOW AREA WILL REDUCE THE ENERGY RATING BELOW A 5-STAR ENERGY RATING.
- D. *NARROW LOT/SMALL HOME REDUCTION.* AN OVERALL REDUCTION IN REQUIRED WINDOW AND PRIMARY ENTRANCE DOOR AREA TO 7% MAY BE APPROVED WHEN THE CALCULATED WALL AREA IS UNDER 500 SQUARE FEET. UNITS USING THIS REDUCTION SHALL COMPLY WITH SUBSECTION 21.07.110D.4.C.
- E. *ADDITIONS AND RENOVATIONS.* ON EXISTING STRUCTURES, ONLY THE WALL AREA AFFECTED BY EITHER AN ADDITION OR A RENOVATION SHALL BE REQUIRED TO COMPLY WITH THE OPENING REQUIREMENTS. UNAFFECTED WALL AREAS NEED NOT COMPLY. GARAGE ADDITIONS ARE EXEMPT FROM OPENING REQUIREMENTS. THE DIRECTOR MAY ELIMINATE OR REDUCE THE REQUIREMENT FOR WINDOW OPENINGS ON ADDITIONS/RENOVATIONS BASED ON PROJECT-

SPECIFIC CIRCUMSTANCES (SUCH AS THE NATURE OF THE SPACES BEING ADDED, BUILDING ORIENTATION, OR STRUCTURAL REQUIREMENTS).]

[E. *PROHIBITED STRUCTURES.* [RESERVED]]

E[F]. *Site Design.*
*** *** ***

[3. *DRIVEWAY WIDTH.*

A. PURPOSE. THIS SECTION LIMITS THE WIDTH OF A DRIVEWAY AT THE PROPERTY LINE AND AT THE STREET CURB. THE INTENT OF THESE LIMITATIONS IS TO PROVIDE ADEQUATE SPACE FOR SNOW STORAGE WITHIN THE RIGHT-OF-WAY, TO HAVE SPACE FOR ON-STREET PARKING WHERE APPROPRIATE, AND TO DISCOURAGE THE MAJORITY OF THE FRONT AREA OF A LOT FROM BEING PAVED AND/OR USED FOR VEHICLE PARKING.

B. *APPLICABILITY.*

I. THIS SECTION APPLIES TO DRIVEWAY THROAT WIDTH AT THE PROPERTY LINE AND STREET CURB.

II. RESIDENTIAL DRIVEWAYS ARE ALSO SUBJECT TO THE MUNICIPAL DRIVEWAY STANDARDS CURRENTLY ESTABLISHED BY THE TRAFFIC ENGINEER. WHERE THERE IS A CONFLICT, THIS SECTION SHALL GOVERN. ACCESS TO STREETS OWNED BY THE STATE OF ALASKA REQUIRES COMPLIANCE WITH STATE DRIVEWAY STANDARDS, AS PROVIDED IN SUBSECTION 21.07.090H.8.D.

III. WHEN A DRIVEWAY SERVES BOTH RESIDENTIAL AND NONRESIDENTIAL PRINCIPAL USES, THE DRIVEWAY DIMENSIONS SHALL BE AS REQUIRED FOR THE NONRESIDENTIAL USE, UNLESS APPROVED OTHERWISE BY THE TRAFFIC ENGINEER.

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- C. *PERCENT OF LOT FRONTAGE.* THE TOTAL WIDTH OF DRIVEWAY ENTRANCES TO A RESIDENTIAL LOT FROM A STREET SHALL NOT EXCEED 40 PERCENT OF THE FRONTAGE OF THE LOT, OR 33 PERCENT OF THE FRONTAGE IF THE PLATTING AUTHORITY OR TRAFFIC ENGINEER FINDS THAT CONDITIONS WARRANT IT.
 - I. A DRIVEWAY FOR MULTIFAMILY DWELLINGS, MIXED-USE DWELLINGS, OR A GROUP LIVING USE MAY ALWAYS BE AT LEAST 14 FEET WIDE.
 - II. A DRIVEWAY FOR A SINGLE-FAMILY, TWO-FAMILY, OR TOWNHOUSE DWELLING MAY ALWAYS BE AT LEAST TEN FEET WIDE, PROVIDED THE TRAFFIC ENGINEER DETERMINES SNOW STORAGE, TRAFFIC FLOW AND SAFETY, AND THE URBAN CONTEXT ARE ADDRESSED, AND PROVIDED TOWNHOUSE DRIVEWAYS ARE ATTACHED IN PAIRS TO THE MAXIMUM EXTENT FEASIBLE.
 - III. FLAG LOTS ARE EXEMPT FROM THE PERCENTAGE LIMITATIONS, BUT SHALL HAVE A MAXIMUM DRIVEWAY WIDTH OF 20 FEET. ABUTTING FLAG LOTS MAY SHARE A DRIVEWAY UP TO 24 FEET WIDE (12 FEET PER LOT).

- D. *EXCEPTIONS.* THE TRAFFIC ENGINEER AND THE PLANNING DIRECTOR MAY APPROVE A DEPARTURE FROM THE STANDARDS OF THIS SECTION, SUCH AS A NARROWER DRIVEWAY, IF DOCUMENTATION PREPARED BY A LICENSED PROFESSIONAL IN THE STATE OF ALASKA REGISTERED WITH THE ALASKA STATE BOARD OF REGISTRATION FOR ARCHITECTS, ENGINEERS AND LAND SURVEYORS DEMONSTRATES THE DRIVEWAY STILL MEETS THIS CHAPTER'S STANDARDS AND THE MUNICIPAL DRIVEWAY STANDARDS MEMO ISSUED BY THE MUNICIPAL TRAFFIC ENGINEER, AND THAT CHANGE IS APPROPRIATE. APPROVAL SHALL BE CONTINGENT ON FACTORS SUCH AS STREET CLASSIFICATION, STREET TYPOLOGY, URBAN CONTEXT, TRAFFIC VOLUME AND SPEED,

1 CURB RETURN RADII, STREET TRAVEL LANE
2 OFFSET FROM FACE OF CURB, PEDESTRIAN AND
3 BICYCLE FACILITIES, SNOW STORAGE,
4 DRIVEWAY CONFIGURATION AND LENGTH, SITE
5 AND PROJECT CHARACTERISTICS, NUMBER OF
6 VEHICLES EXPECTED TO USE THE DRIVEWAY,
7 AND COMPREHENSIVE PLAN POLICIES. THE
8 TRAFFIC ENGINEER AND PLANNING DIRECTOR
9 MAY ALSO BE MORE RESTRICTIVE THAN THE
10 STANDARDS OF THIS SECTION, PROVIDED THEY
11 DOCUMENT THE RATIONALE.
12

13 4. ALLEYS.

14 A. ACCESS TO PARKING FOR RESIDENTIAL USES
15 SHALL BE FROM THE ALLEY WHEN THE SITE
16 ABUTS AN ALLEY, EXCEPT THAT STREET
17 ACCESS IS PERMITTED IN ANY OF THE
18 FOLLOWING SITUATIONS:
19

20 I. ACCESS TO A TOWNHOUSE DWELLING ON
21 A CORNER LOT MAY BE FROM THE STREET
22 FRONTAGE HAVING THE SECONDARY
23 FRONT SETBACK OR THE ALLEY.
24

25 II. DUE TO THE RELATIONSHIP OF THE ALLEY
26 TO THE STREET SYSTEM AND/OR THE
27 PROPOSED HOUSING DENSITY OF THE
28 DEVELOPMENT, THE TRAFFIC ENGINEER
29 DETERMINES THAT USE OF THE ALLEY
30 FOR PARKING ACCESS WOULD BE A
31 SIGNIFICANT TRAFFIC IMPACT OR SAFETY
32 HAZARD.
33

34 III. THE TRAFFIC ENGINEER DETERMINES
35 THAT TOPOGRAPHY OR OTHER NATURAL
36 FEATURE OR PHYSICAL BARRIER MAKES
37 ALLEY ACCESS INFEASIBLE.
38

39 IV. THE ALLEY IS NOT IMPROVED AND
40 TRAFFIC ENGINEER DETERMINES THAT
41 IMPROVEMENT IS NOT FEASIBLE.
42

43 V. A SINGLE-FAMILY DWELLING, TWO-FAMILY
44 DWELLING, OR TOWNHOUSE DWELLING
45 WITH TWO UNITS, WITH ALLEY ACCESS
46 MAY HAVE A GARAGE OR DRIVEWAY THAT
47 FACES THE STREET IF THE GARAGE DOOR
48 IS NO WIDER THAN 10 FEET AND THE

1 DRIVEWAY NO WIDER THAN 12 FEET AT
2 ANY POINT.

3
4 B. IN SITUATIONS WHERE A GROUP OF LOTS FRONT
5 AN ENTIRE BLOCK ON ONE SIDE OF A STREET
6 BETWEEN TWO INTERSECTIONS, ABUT A MID-
7 BLOCK ALLEY, AND ARE BEING DEVELOPED
8 TOGETHER, THEN PARKING ACCESS TO THE
9 STRUCTURES SHALL BE FROM THE ALLEY, AND
10 BUILDING(S) MAY ENCROACH INTO THE FRONT
11 SETBACK BY UP TO FIVE FEET.

12
13 C. IF A NEW DEVELOPMENT INCLUDES ALLEYS, THE
14 LOT DEPTH REQUIREMENT IS REDUCED BY HALF
15 THE WIDTH OF THE ALLEY AND THE LOT AREA
16 REQUIREMENT IS REDUCED BY 12 PERCENT FOR
17 THOSE LOTS THAT ABUT AN ALLEY. VEHICULAR
18 ACCESS TO ALL DWELLING UNITS ON LOTS
19 ABUTTING ALLEYS SHALL BE FROM THE ALLEY,
20 AND VEHICULAR ACCESS TO SUCH UNITS FROM
21 THE STREET IS PROHIBITED.]

22 *** **

23 *(Note to code revisor: Re-number subsequent subsections in section*
24 *21.07.110, and re-number tables 21.07-11 and 21.07-12. Re-*
25 *number table 21.07-13 in chapter 21.07 section 21.07.115.)*

26
27 (AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2014-132, 11-5-
28 14; AO 2015-36, 5-14-15; AO 2015-100, 10-13-15; AO 2016-34(S), 4-12-
29 16; AO 2016-136, 11-15-16; AO 2017-160, 12-19-17; AO 2018-59. 7-31-
30 2018; AO 2019-132, 12-3-19; AO 2020-38, 4-28-20)

31
32 **Section 3.** Anchorage Municipal Code Chapter 21.15, Rules of Construction
33 and Definitions, is hereby amended to read as follows *(the remainder of the*
34 *chapter is not affected and therefore not set out):*

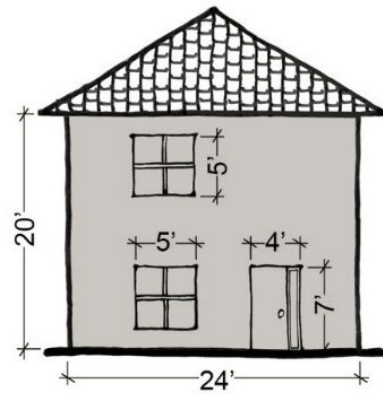
35
36 **21.15.020 Rules of Construction and Interpretation.**

37 *** **

38 P. Measurement of Windows and Entrances as a Percentage of
39 Building Wall Area. The percentage of building elevation wall area
40 that is window and entrance openings is determined by dividing the
41 total square footage of the windows and primary entrances on the
42 building elevation by the gross square footage of the building
43 elevation wall area (except for parts of the building exempted in 3.),
44 and then multiplying the resulting quotient by 100.

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1. Illustrated Example. In the illustration, the building elevation wall area is 20 feet of height x 24 feet of length = 480 square feet. The window and primary entrance area is the area of the two windows (5 x 5 feet x 2) plus the area of the primary entrance and its sidelight (4 x 7 feet) = 78 square feet. The percentage of the building elevation wall area is found by dividing 78 by 480 then multiplying by 100, to yield 16%.

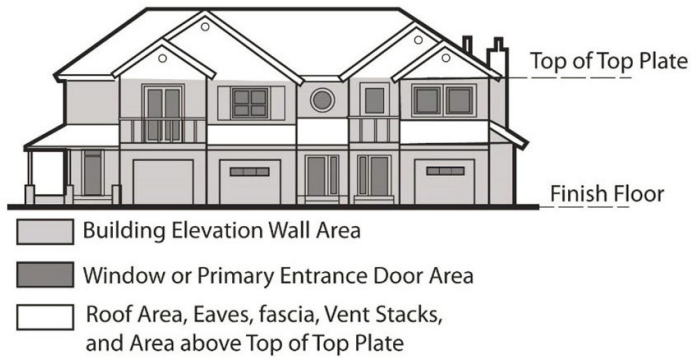


2. Measuring Window and Primary Entrance Area. “Window area” is defined in section 21.15.040, Definitions. Primary entrance area includes the entrance opening and door frame but excludes trim, molding, and other features beyond the door frame. “Primary entrance” is defined in 21.15.040.

3. Exempt Parts of the Building Elevation Wall Area. The building elevation wall area is measured as the exterior wall plane surface area from finished grade to the top of the wall on the building except that, for the purposes of measuring windows and entrances as a percentage of building wall area, the following parts of the building are not included in the wall area calculation:

- a. Wall area above the topmost story’s top plate, such as gable ends of a roof enclosing a cold attic, or parapet walls.
- b. Roof surfaces, eaves, fascia, vent stacks, mechanical equipment and rooms, rooftop access enclosures, and other roof appurtenances.
- c. Wall area of stories below grade plane, such as below grade parking, up to the finished floor of the story above grade plane.
- d. Parts of a street-facing building elevation below the grade plane of the abutting sidewalk (or edge of street pavement where there is no sidewalk).

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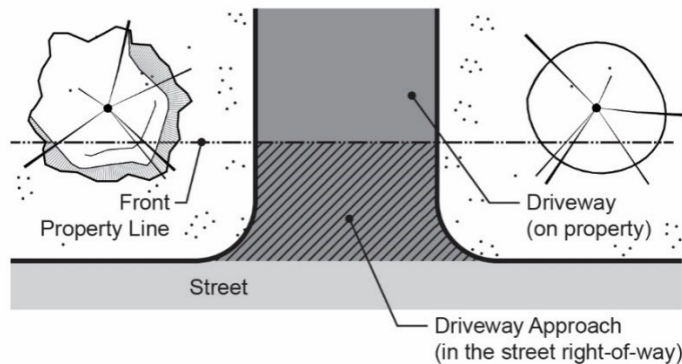
*** *** ***
(AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13)
*** *** ***

21.15.040 Definitions.

*** *** ***
Complete Street
See Street, Complete.
*** *** ***

Driveway Approach

The portion of a driveway providing direct vehicle ingress and egress over public right-of-way to a property. The driveway approach extends from the street edge to the front property line.



*** *** ***

Pedestrian Feature

A permanent object that provides pedestrians with increased convenience, comfort, and utility, and which is publicly accessible and not limited to a tenant or establishment such as seating for a restaurant. Pedestrian features include:

- Seating such as benches accommodating several people;
- Secondary/informal seating opportunities such as steps, pedestals, low walls, or edges of fountains, accommodating several people;
- A space for standing with objects to lean against, such as bollards, short fences, or irregular building facades, accommodating several people;
- A tree or raised planter;

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- A work of art such as a water feature, sculpture, cultural exhibit, or clock feature;
- Pedestrian-scale area lighting designed to light outdoor walkways or pedestrian spaces;
- Ornamental lighting designed to illuminate architecture and/or landscaping for decorative and aesthetic effect;
- A cross-country ski rack that can accommodate a ski lock and is located at building entrances within 500 feet of a ski trailhead;
- A winter city urban design feature such as a wind screen, or outdoor stove or space heater; or
- Other object supporting pedestrian utility, such as a gazebo or kiosk.

Pedestrian Movement Zone

The middle portion of an enhanced street sidewalk (21.07.060G.20.), comprising its walkway clear width located between the sidewalk's street interface and building interface zones. The pedestrian movement zone provides for the primary function of sidewalks[,] and is kept clear of any obstructions to pedestrian movement.

*** *** ***

Primary Entrance

See Entrance, Primary.

*** *** ***

Street, Complete

A street that is designed, used, and operated to enable safe access for all users, including pedestrians, bicyclists, motorists, and public transportation users of all ages and abilities, and for all users to safely move through the transportation network. For example, Anchorage Metropolitan Area Transportation Solutions (AMATS) adopted a complete streets policy in 2018 for its urbanized planning area. The AMATS Complete Streets Policy is available on the municipal website.

*** *** ***

Walkway Clear Width

That portion of the total width of a walkway, trail, pathway, or sidewalk cross-section which is unobstructed, continuous (i.e., without interruption or being split by obstructions), and kept clear for pedestrian movement.

*** *** ***

[WALKWAY, PRIMARY PEDESTRIAN

SEE PRIMARY PEDESTRIAN WALKWAY.]

*** *** ***

(AO 2012-124(S), 2-26-13; AO 2013-117, 12-3-13; AO 2014-132, 11-5-14; AO 2015-82, 7-28-15; AO 2015-100, 10-13-15; AO 2015-138, 1-12-16; AO 2015-133(S), 2-23-16; AO 2015-142(S-1), 6-21-16; AO 2016-3(S), 2-23-16; AO 2016-144(S), 12-20-16; AO 2017-55, 4-11-17; AO 2017-75, 5-9-2017; AO 2018-12, 2-27-18; AO 2018-67(S-1), 10-9-18; AO 2018-92, 10-23-18; AO 2019-132, 12-2-19; AO 2020-38, 4-28-20)

1 **Section 4.** This ordinance shall become effective on January 1, 2024.
2

3 PASSED AND APPROVED by the Anchorage Assembly this _____ day of
4 _____, 2023.
5

6
7
8 _____
9 Chair

10 ATTEST:
11

12
13 _____
14 Municipal Clerk

15
16 (Planning and Zoning Commission Case No. 2023-0011)
17