

**Annotated Version
Draft Public Hearing Ordinance Language**

Title 21 Text Amendment to Site Access Regulations

PZC Case No. 2023-0011

This page intentionally left blank.

Annotated (Narrated) Zoning Code Amendments

This *Annotated (Narrated) Version of the Public Hearing Draft Site Access Ordinance Language* presents the public hearing draft Title 21 *Site Access* text amendment language, accompanied by supporting information, explanation, and references. The text amendments in this document are the same code text amendments as provided in the Public Hearing Draft Assembly Ordinance, except they are shaded in light brown to be easier to see.

All text amendments are arranged in the order they appear in Anchorage’s *Title 21 Zoning Ordinance*. Only the sections of Title 21 that are being amended are included. Proposed new sections are marked “*new section*” below:

Zoning Code Section Affected by Proposed Changes	Page
21.03.240J. Review and Approval Procedures – Administrative Variances	1
21.04. Zoning Districts (<i>district-specific standards</i>)	2
21.07.010D. Alternative Equivalent Compliance (<i>applicability</i>)	4
21.07.010E. Urban Neighborhood Development Context Area (<i>new subsection E.</i>)	4
21.07.060E. Standards for Pedestrian Facilities (<i>sidewalks and walkways</i>)	7
21.07.060F. Pedestrian Frontage Standard (<i>new subsection F.</i>)	9
21.07.060G. Pedestrian Amenities Menu (<i>renumbered from F to G; various amendments</i>)	12
21.07.090A. Parking and Loading Facilities – Purpose	19
21.07.090H.2. Parking and Loading Facility Design Standards – Applicability	19
21.07.090H.8. - 11. Vehicular Access and Circulation (<i>renumbered to H.8. through H.11.</i>)	20
21.07.090H.12. Dimensions of Parking Spaces and Aisles (<i>renumbered from H.9. to H.12.</i>)	28
21.07.110C. Residential Design Standards – Multifamily and Townhouse	29
21.07.110D. Residential Design Standards – Single-family and Two-family	35
21.07.110F. Residential Design Standards – Site Design (<i>driveway width; alley access</i>)	39
21.15.020P. Rules of Construction and Interpretation (<i>new subsection P.</i>)	42
21.15.040. Definitions	43

How to Read this Document

Amended code language appears on the right-hand pages (numbered pages) and annotation on left-hand pages. The annotation explains how to interpret or apply the draft amendment language. Sometimes it describes the issue being addressed or the purpose of the proposed code change.

Proposed changes to zoning code language (called “amendments”) appear as “tracked changes” text with a shaded background, as follows:

- Added code language is underlined.
- Deleted language is in [ALL-CAPITALIZED TEXT IN BRACKETS].
- Text without tracked-changes/shaded background is current code language not being changed.

Each page of tracked-changes code language begins with a header showing the chapter and major section that contains the proposed changes. To show only those subsections being changed, a row with three sets of asterisks (***) indicates that all intervening parts of the chapter are unchanged between the section header or text above the asterisks and the language shown below the asterisks.

The following 4 pages provide a cross-reference to the main recommendations of this ordinance.

Cross-Reference to Main Proposals

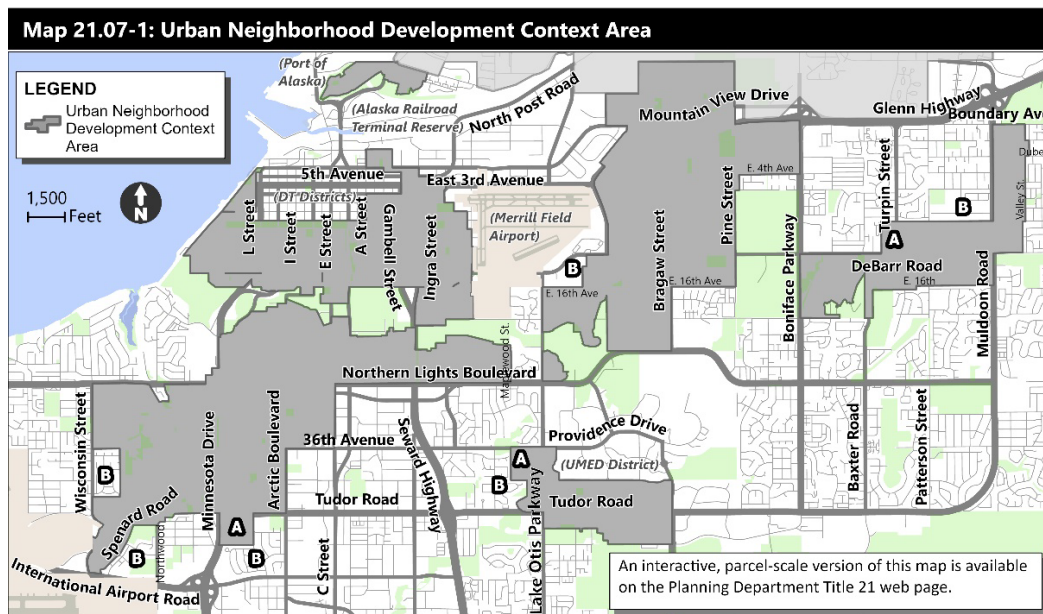
The table below and on the following three pages is the list of main recommendations of this ordinance from Exhibit C, with added *cross-references (in red)* to where those code changes appear in this Exhibit E.

1. Create an Urban Neighborhood Context Area

(Cross-reference: Section 21.07.010E.; Pages 4-6 in this Exhibit E)

- a. Establishes an **Urban Neighborhood Development Context Area** based on patterns recognized in the *Anchorage 2040 Land Use Plan* and neighborhood and district plans. The Urban Neighborhood Development Context Area is shown in the map below.

No proposed changes to Title 21 development standards in this section.



Current Title 21 refers to the *Anchorage 2040 Land Use Plan Map* “traditional neighborhood design” areas for several regulations specific to urban neighborhoods. Placing the map above in Title 21 provides easier reference to simplify the review process. It also creates flexibility in the code to tailor other site development standards for urban contexts, recognizing these areas are unique. This map includes adjustments from the *Anchorage 2040* map that more accurately reflect where urban neighborhoods are in the Anchorage Bowl.

As discussed in the PZC Minutes on pages 242-243 of Exhibit C, Anchorage’s current Zoning Map does not provide the ability to identify areas such as urban contexts where the community would like to place the highest pedestrian oriented standards. For instance, the R-1 and R-2M residential and B-3 commercial zoning districts each include urban neighborhoods and suburban neighborhoods. The proposed urban context map provides a rational nexus of areas appropriate for requiring an urban level of pedestrian design standards. These areas have a more connected grid of streets, smaller city block and lot sizes, proximity to mixed-use employment centers, and greater access to pedestrian facilities and public transit options. Some urban neighborhoods have a rigid street grid pattern, others a more relaxed grid, as shown in the aerial images on pages 4 and 5 of this Exhibit E.

2. Improve Site Access for Pedestrians and Alternative Transportation

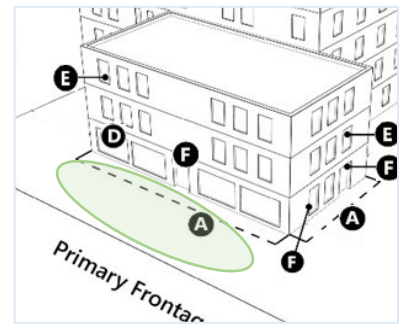
(Cross-reference: Section 21.07.010F., Pages 9-11 in this Exhibit E)

a. **Pedestrian Frontage Standard: Parking Placement.**

Amends requirements to place parking lot facilities mostly beside or behind buildings to engage the street without parking in front: Replaces an existing maximum required setback for mixed-uses and a menu parking placement standard for multifamily. Newly applies to commercial uses.

Urban Neighborhoods: At least half of the street-facing building elevation of residential and commercial uses to have no parking in front, except duplexes in R-2 zones.

Suburban Neighborhoods: A smaller part of the building to have no parking in front, exempting commercial buildings located more than 100 feet from the street.

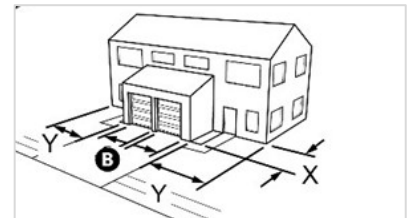


b. **Residential Garage Widths and Projections Reduced in Urban Neighborhoods.**

Replaces an existing menu standard for multi-family and townhouses with the same garage limitations as homes and duplexes have. Removes a requirement to articulate narrow homes/duplexes that have garages.

Urban Neighborhoods: Reduces the allowed garage door width and garage projection on front façades.

Suburban Neighborhoods: No change to existing allowed garage width and projection.



c. **Street-Facing Windows Increased in Urban Neighborhoods and for Commercial Uses.**

Revises existing requirements for residential and mixed-use buildings to have street-facing windows. Applies the mixed-use window requirements to commercial uses.

Urban Neighborhoods: Increases required window area from 10% to 15% of residential and upper floor façade walls; and from 15% to 25% of non-residential ground-floor facades.

Suburban Neighborhoods: No change to existing 10% residential and 15% mixed-use requirements, except to relax by 5% on secondary frontages, and apply to commercial uses.

d. **Visible, Sheltered Building Entrances.** Consolidates and simplifies the requirement for visible, covered primary entrances on multifamily dwellings with single- and two-family dwellings. No change to the single- and two-family requirement except to reduce the min. entrance shelter size and distinguish the entry space from adjacent parking. Relaxes and simplifies the requirement for visible primary entrances on mixed-uses, and applies that simplified mixed-use standard to commercial uses for consistency.

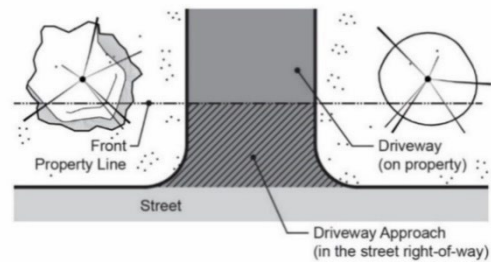
e. **Clarifies Pedestrian Amenity Choices in a Consolidated, Generally Applicable Menu.**

Multifamily, mixed use, and commercial developments must select 2 amenities in urban neighborhoods and 1 amenity in other areas from the Title 21 Pedestrian Amenities menu. Five menu options added from similar menus being deleted that previously applied to multifamily and mixed use. Existing menu choices becoming more flexible. (Cross-reference to the amended menu of pedestrian amenity options: Section 21.07.060G., Pages 12-18 in this Exhibit E)

3. Right-Size Driveway, Alley Access, and Circulation Standards

(Cross-reference: Sections 21.07.060H.8. - H.11., Pages 20-27 in this Exhibit E)

- a. **Municipal Driveway Standards Clarification.** Clarify that the *Municipal Driveway Standards* applies to the minimum and maximum width of the driveway approach within the right-of-way and the minimum driveway throat length.



- b. **More Consistent and Flexible Alley Access Requirement for Urban Neighborhoods.**
- **Refers to the Urban Neighborhood Context Areas map** (see Section 1) instead of the *Anchorage 2040 Plan* for where the alley access requirement applies.
 - **Allows a front driveway for multifamily housing.** Allows one 12-foot-wide front driveway per 50 ft of street frontage on lots with alley access, for all residential uses—including multifamily (i.e., 3 or more dwelling units on a lot).
 - **Reduces frequency of allowed front driveways for duplexes and townhouses.** Standard applies to amount of street frontage (one driveway per 50 feet) instead of the number of units, for consistent treatment of different housing types and retention of public street curb space for snow storage, on-street parking, and pedestrian facilities.
 - **Allows corner lots with multiple units to have a second driveway.** Duplex, townhouse, and multifamily uses on corner lots may have a 12-foot driveway from the side street.
 - **Tightens administrative exception to grant wider front driveways.** Limits eligibility to multifamily. Requires public documentation of the approved exception.
(Cross-reference: Section 21.03.240J.7., Page 1 in this Exhibit E.)
- c. **Allows Parking Spaces to Back into Alleys, By-right.** Allows parking spaces along alleys to use the alley as the parking aisle without need for on-site turnarounds.
- d. **Traffic Engineer Review of Garage Doors Set Close to Public ROW.** Where the garage bay door setback will not be enough to accommodate a parked car in the driveway without hanging across the sidewalk or into the street, Traffic Engineer must review and approve.
- e. **Restores Level Sidewalks.** Where driveways are constructed across sidewalks in the Urban Neighborhoods, requires the sidewalk to be restored to a level running grade.
- f. **Exempts Multifamily from On-site Turnaround Requirements (up to 4 spaces).** Exempts up to 2 multifamily parking spaces (or 4 tandem spaces) from on-site maneuvering aisle requirements for in driveways along local streets, freeing up a lot of space on the site.
- g. **Reduces Minimum Width of Multifamily Driveways.** Allows 2-way driveways providing access for up to 10 residential parking spaces to be 1-lane wide. Also reduces the minimum by-right width of multifamily driveways to 10 feet for 1-lane and 20 feet for 2-lane internal access driveways (not including parking aisles).

4. Consolidate and Simplify Regulations

- a. **Easier-to-use Code for Sidewalks.** Clarifies existing Title 21 requirements and design standards for street sidewalks.

No substantive change. *(Cross-reference: Section 21.07.060E.2.; Page 7 in this Exhibit E.)*

- b. **Easier-to-use Code for On-site Walkways.** Merges four Title 21 sections into one generally applicable section for on-site pedestrian connections, to make it easier to use.

No substantive change. *(Cross-reference: Section 21.07.060E.4.; Pages 8 and 12 in this Exhibit E.)*

- c. **Clarified Pedestrian Facility Definitions and Easier Window Area Measurement.** Five new/clarified definitions in Title 21 glossary. Consolidates and clarifies existing Title 21 “rules of interpretation” for measuring windows and primary entrances as a percentage of street-facing façade wall. *(Chapter 21.15, pp. 42-44)*

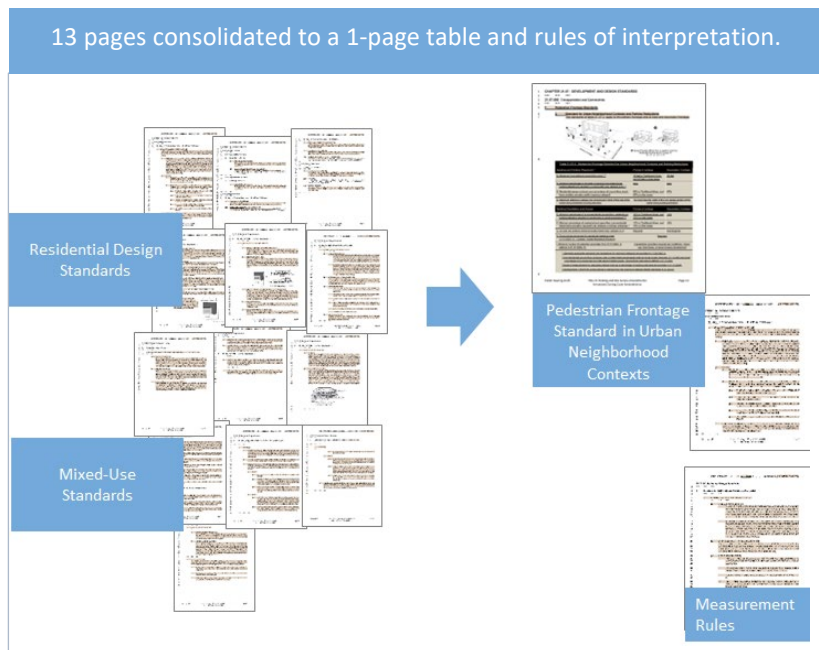


- d. **Easier-to-use Code for Driveways, Alley Access, and On-site Vehicle Circulation.** Consolidates and clarifies the Title 21 driveway, on-site vehicle circulation, and urban alley access standards, by merging 3 sections into one intuitive sequence of driveway standards. *(21.07.110; pp. 39-41)*
See previous page for substantive amendments to specific standards.

- e. **Easier-to-use Code for Pedestrian Frontage Standards.** Merges the pedestrian standards from different sections of Title 21 into one section with an illustrated table.
No substantive change for the reorganization of the information into the table.
See the page before last for amendments made to specific pedestrian standards.

(Cross-references:

- *Ch. 21.04; Pages 2-3.*
- *Section 21.07.110; Pages 29-38.*
- *Section 21.07.060F.; Pages 9-11.)*



ANNOTATION FOR PAGE 1

Chapter 21.03., Review and Approval Procedures – Administrative Variances

Current Title 21 section 21.03.240J. provides for *administrative variances*. Administrative variances are a form of administrative relief currently allowed for signs, large domestic animal facilities, refuse receptacle location and screening, and height encroachments into step-backs. The amendment on page 1 adds an administrative variance for front driveways on multifamily lots with alleys. It replaces administrative exceptions to be deleted from Chapter 21.07 (page 41). See explanation and details below.

Line (s) #	Comment on Change
7-27	<p>Create an administrative variance from the Chapter 21.07 alley access requirements that will replace a set of existing administrative exceptions from the alley requirements. See also related amendments on page 21, in section 21.07.090H.9.b.iii., <i>Alley Access Requirement</i>. In current code, the Planning Director and Traffic Engineer may approve an administrative exception from the front driveway limitations of the alley access requirements, as part of the land use permit approval. The current provisions are proposed to be deleted on page 41, lines 18-26, and replaced by the new administrative variance.</p> <p>The change to an administrative variance tightens the review and approval process to obtain the administrative exception from the alley access requirements. Administrative variances require written (mailed), published, posted, and community council notice. Mailed notice goes to properties within 500 feet of the subject site. The fee for an administrative variance is \$710. Title 21 requires a determination by the Planning Director within 45 days of the submittal of a complete application. An administrative variance is a separate land use permit with its own case file in which all the information about the administrative variance is documented. There is an application form, staff report, and a link to submit public comments posted to the Municipality’s CityView Portal (muni.org/CityViewPortal/Planning/Locator). An administrative variance may be appealed by following the process in 21.03.240J.1.d.</p> <p>The proposed language of the administrative variance also tightens the substantive criteria for obtaining the exception. It limits eligibility to multifamily and townhouse projects with 3 more dwellings. To obtain the administrative variance, a petitioner would have to meet conditions a. through d. showing there are physical circumstances of the property, strict application of the alley access regulation would create undue hardship that was not self-imposed by the property owner, and the administrative variance is the minimum necessary. The language clarifies that the Traffic Engineer must determine improvement of the alley and vehicle access are not feasible or would create a traffic hazard.</p> <p><i>Discussion: An administrative variance is less expensive with a quicker review time than a public hearing variance. The public notice is the same for both, but the decision for a variance is made through a public hearing before the Zoning Board of Examiners and Appeals or the Urban Design Commission. Cases are typically scheduled for a public hearing approximately 2 months out from the receipt of a complete application. The fee for a variance ranges from \$710 to \$3,965 depending on the property and type of variance. A variance is still an available option if a property owner wants to deviate from alley requirements further than what is allowed by an administrative variance or a minor modification for driveway width.</i></p>

1 **CHAPTER 21.03: REVIEW AND APPROVAL PROCEDURES**

2 *** **

3 **21.03.240 Variances**

4 *** **

5 **J. Administrative Variances**

6 *** **

7 **7. For Front Driveways on Multifamily and Townhouse Lots with Alleys**

8 Where a multifamily or townhouse development site is served by an alley in the Urban
9 Neighborhood Development Context (21.07.010E.), the director and traffic engineer may grant an
10 administrative variance from subsection 21.07.090H.9.b., *Alley Access Requirement*, provided:

11 **a.** There exist physical circumstances of the subject property such as topography, absence
12 of alley improvements, exceptional lot configuration not shared by landowners in general,
13 or adjoining street traffic patterns, and the traffic engineer determines that alley
14 improvement and/or vehicle access are not feasible or would create a traffic impact or
15 safety hazard;

16
17 **b.** Because of these physical circumstances, the strict application of the alley access
18 requirement in 21.07.090H.9.b. would create an exceptional or undue hardship upon the
19 property owner, and would deprive the owner of rights commonly enjoyed by other
20 properties in the same zoning district;

21
22 **c.** The hardship is not self-imposed, the conditions and circumstances do not result from the
23 actions of the applicant, and such conditions and circumstances do not merely constitute
24 inconvenience; and

25
26 **d.** The administrative variance granted for additional driveway access shall be the minimum
27 the traffic engineer determines is necessary to provide access for the development.
28

29 *** **

ANNOTATION FOR PAGE 2

Chapter 21.04., District-specific Standards

Current Title 21 Section 21.04.030G. establishes development standards specific to mixed-use projects in the B-1A and B-1B zoning districts. These also apply to mixed-use developments in the B-3 district via subsection 21.04.030H. These current standards apply only to mixed-use developments—not to stand-alone residential or commercial-only developments.

The changes on page 2 delete the mixed-use development standards for building frontages from the mixed-use development standards chapter 21.04, including maximum setbacks from the street, and street-facing ground-floor uses with prominent entrances and windows facing the sidewalk. These are proposed to be replaced by pedestrian frontage standards in Section 21.07.060F. Tables 21.07-2 and 21.07-3 (see pages 9-11 below).

<i>Line (s) #</i>	<i>Comment on Change</i>
9-11; 17-19	Clarify the name of a referenced pedestrian amenity from “enhanced sidewalk” to “enhanced <u>street</u> sidewalk.” This change reflects edits and clarifications in 21.07.060G.20. (page 17) to the name of one of the pedestrian amenity menu choices that earns credit toward several pedestrian-related development standards in Title 21.
20-44	Delete current Title 21 Code standards for pedestrian-oriented mixed-use developments. Subsections [A] and [C] are replaced by more flexible, generally applicable frontage requirements in 21.07.060F., Pedestrian Frontage Standard (pages 8-10). In that new subsection, the standards are modified to be stronger in designated urban neighborhood development context areas than elsewhere in the Municipality. Subsection [A] on lines 25-34 on the next page, regarding street-facing windows, is incorporated in revised form in rows D and E in Tables 21.07-2 and 21.-07-3 on pages 9 and 10. Subsection [B] starting on line 35 on the next page, regarding prominent entries, incorporated in revised form rows F and G in Tables 21.07-2 and 21.-07-3 on pages 9 and 10, which refer to new pedestrian amenity menu options in subsections 21.07.060G.14. and G.15 (page 13, lines 12-39).

1 **CHAPTER 21.04: ZONING DISTRICTS**

2 *** **

3 **21.04.020 Residential Districts**

4 *** **

5 **H. R-3A: Residential Mixed-Use District**

6 *** **

7 **2. District-Specific Standards**

8 *** **

9 **e. Enhanced Street Sidewalk [OPTION]**

10 An enhanced street sidewalk [ENVIRONMENT]may be provided in lieu of required
11 sidewalks and site perimeter landscaping, as defined[PROVIDED] in 21.07.060G.20[F.17].

12 *** **

13 **21.04.030 Commercial Districts**

14 *** **

15 **G. Standards for Mixed-Use Development in the B-1A and B-1B Districts**

16 *** **

17 **5. Enhanced Street Sidewalk [OPTION]**

18 An enhanced street sidewalk [ENVIRONMENT]may be provided in lieu of required sidewalks and
19 site perimeter landscaping, as defined[PROVIDED] in 21.07.060G.20[F.17].

20 **[6. BUILDING PLACEMENT AND ORIENTATION]**

21 [BUILDINGS SHOULD BE PLACED AND ORIENTED TO THE STREET, LINING SIDEWALKS
22 AND PUBLIC SPACES WITH FREQUENT SHOPS, ENTRANCES, WINDOWS WITH INTERIOR
23 VIEWS, AND ARTICULATED GROUND-LEVEL FACADES. THE FOLLOWING STANDARDS
24 APPLY:]

25 [A. BUILDINGS SHALL HAVE VISUAL ACCESS WINDOWS AND/OR PRIMARY
26 ENTRANCES ON STREETFACING BUILDING ELEVATIONS (UP TO A MAXIMUM OF
27 TWO ELEVATIONS) FOR AT LEAST 15 PERCENT OF THE NON-RESIDENTIAL
28 GROUND FLOOR WALL AREA. QUALIFYING WINDOWS SHALL BE NO MORE THAN
29 FOUR FEET ABOVE FINISHED GRADE. WINDOWS SHALL COMPRISE AT LEAST 10
30 PERCENT OF THE WALL AREA OF THE UPPER FLOOR BUILDING ELEVATION
31 (ABOVE THE GROUND FLOOR). EXTERIOR WALL AREAS OF BUILDING
32 MECHANICAL ROOMS ARE EXEMPT. AN ELEVATION THAT IS MORE THAN 150 FEET
33 AWAY FROM THE FACING STREET SHALL BE EXEMPT, UNLESS IT IS THE ONLY
34 APPLICABLE ELEVATION.]

35 [B. EITHER:]

36 [I. PROVIDE AT LEAST ONE PRIMARY ENTRANCE WITHIN 60 FEET OF A
37 STREET SIDEWALK, OR 90 FEET FOR BUILDINGS OVER 25,000 SQUARE
38 FEET OF GROSS FLOOR AREA, AND CONNECTED TO THE STREET BY A
39 CLEAR AND DIRECT WALKWAY; OR]

40 [II. PROVIDE A PROMINENT AND INVITING PRIMARY ENTRANCE THAT IS
41 VISIBLE FROM THE STREET, CONNECTED BY A DIRECT WALKWAY TO THE
42 STREET, AND HIGHLIGHTED BY TWO OF THE FOLLOWING:]

43 [(A) PORTICO, OVERHANG, CANOPY, OR SIMILAR PERMANENT
44 FEATURE PROJECTING FROM THE WALL;]

ANNOTATION FOR PAGE 3

Chapter 21.04, District-specific Standards, (cont'd)

The changes on page 2 are a continuation of the amendments from page 1 to the district-specific mixed-use development standards.

<i>Line (s) #</i>	<i>Comment on Change</i>
15-25	<p>Delete maximum allowed setback requirements for mixed-use developments in the commercial zoning districts.</p> <p>Subsections 21.04.030G.7.C. on lines 15-18 and 21.04.030H.3. on lines 22-25, regarding maximum setback requirements, are deleted from chapter 21.04 and not carried forward as a frontage standard in 21.07.060F.</p> <p>Maximum setback requirements are replaced in 21.07.060F. by a more flexible, generally applicable standard requiring a portion of the front building façade to not have parking facilities in between it and the street frontage (Row A in Tables 21.07-2 and 21.07-3, on pages 10 and 11).</p>

1 **CHAPTER 21.04: ZONING DISTRICTS**

2 *** **

3 **21.04.030 Commercial Districts**

4 *** **

5 **G. Standards for Mixed-Use Development in the B-1A and B-1B Districts**

6 *** **

7 [(B) RECESSED AND/OR PROJECTED ENTRANCE THAT COVERS AT
8 LEAST 80 SQUARE FEET;]

9 [(C) ARCHES, PEAKED ROOF FORMS, TERRACING PARAPETS, OR
10 OTHER CHANGE OF BUILDING ROOFLINE;]

11 [(D) CHANGES IN SIDING MATERIAL, OR DETAIL FEATURES SUCH AS
12 TILEWORK, TO SIGNIFY THE ENTRANCE; OR]

13 [(E) ENTRANCE PLAZA, PATIO, OR SIMILAR COMMON PRIVATE OPEN
14 SPACE.]

15 [C. BUILDINGS SHALL COMPLY WITH THE MAXIMUM SETBACKS ESTABLISHED IN
16 SECTION 21.06.020, TABLES OF DIMENSIONAL STANDARDS, AND SUBSECTION
17 21.06.030C.5. THE MAXIMUM SETBACK MAY BE REDUCED OR ELIMINATED WITH
18 THE CONCURRENCE OF THE DIRECTOR.]

19 *** **

20 **H. Standards for Mixed-Use Development in the B-3 District**

21 *** **

22 [3. BUILDING PLACEMENT AND ORIENTATION]
23 [BUILDINGS ARE SUBJECT TO THE BUILDING PLACEMENT AND ORIENTATION
24 STANDARDS FOR MIXED-USE DEVELOPMENT IN THE B-1A/B-1B DISTRICTS IN SECTION
25 21.04.030G.7. ABOVE.]

26
27 **3[4]. Enhanced Street Sidewalk** [OPTION]
28 An enhanced street sidewalk [ENVIRONMENT] may be provided in lieu of required sidewalks and
29 site perimeter landscaping, as defined [PROVIDED] in 21.07.060G.20[F.17].

30 *** **

31 **21.04.070 Overlay Zoning Districts**

32 *** **

33 **C. Commercial Center Overlay**

34 *** **

35 **5. Dimensional Standards**

36 [A. THE MINIMUM FRONT SETBACK IS REDUCED TO FIVE FEET FOR BUILDING
37 ELEVATIONS THAT COMPLY WITH 21.04.030G.7., BUILDING PLACEMENT AND
38 ORIENTATION.]

39
40 [B.]Uses in the B-1B district are exempt from the gross floor area limitations of subsection
41 21.04.030C.2.b., provided that an individual use occupies no more than 60,000 square feet
42 of gross floor area.

43 *** **

Section 21.07.010D., General Provisions – Alternative Equivalent Compliance

The Alternative Equivalent Compliance procedure in the general provisions for Chapter 21.07 allows developers and designers to propose to meet the intent of a development standard through alternative means and methods. The proposed change on the next page provides for flexibility in the application of pedestrian frontage standards proposed on pages 9-11 by amending subsection 21.07.010D.2, *Alternative Equivalent Compliance – Applicability*, to add 21.07.060F., *Pedestrian Frontage Standard*, to the list of sections in Title 21 for which alternative equivalent compliance is available.

Section 21.07.010E., General Provisions – Urban Neighborhood Development Context Area (New)

A new subsection of the general provisions for Chapter 21.07 is proposed to delineate areas in the Anchorage Bowl where older, urban patterns of development and more accessible alternatives to single-occupancy motor vehicle travel provide a basis for tailored pedestrian frontage, driveway, and other development standards in Chapter 21.07 to better fit urban neighborhoods. It replaces existing code references (e.g., from the alley access requirements) to a map in the *Anchorage 2040 Land Use Plan (LUP)*.

Anchorage’s *Comprehensive Plan* establishes urban neighborhood development contexts in a variety of its individual plans. The *2040 LUP* identifies “Traditional Neighborhood Design” areas and “Transit-supportive Development” corridors on its Land Use Plan Map. Area-specific plans such as the *Fairview Neighborhood Plan* and *Spenard Corridor Plan* also call for alternative development standards to promote more urban, walkable forms of development and housing at market-feasible cost.

The Title 21 Site Access Amendments ordinance proposes increasing pedestrian standards throughout Anchorage, including in areas outside the urban contexts. The proposed urban neighborhood contexts map offers flexibility in the code, for example, to tailor a higher standard in urban neighborhood contexts. Applying the same standard municipal-wide would miss an opportunity to make an even greater change in urban neighborhoods. The Municipality anticipates additional zoning reforms in the future for some of the urban neighborhoods identified in the *2040 LUP* (e.g., 2040 Action 7-4 to create form-based traditional neighborhood zones) and the area-specific plans. However, such reforms will take additional funding, resources, and time. In the meantime, the amendments on pages 4 through 6 that follow will allow Title 21 to respond to urban environments at least generally, and it leaves the opportunity and desire to continue to improve Title 21 and to make the zoning reforms envisioned by the community.

Line (s) #	Comment on Change
16-35	<p>Purpose The purpose section establishes the specific Urban Neighborhood Development Context Area and describes its basic characteristics. The purpose statement, its neighborhood descriptions, and the illustrations are explanatory intent statements rather than regulatory requirements. For Title 21 guidance on the interpretation of intent statements, see the rules of construction and interpretation in section 21.15.020B.</p> <p><i>Discussion:</i> Traditional urban patterns of development include a fine-grained street grid with smaller lot sizes and more route choices and sidewalks. They tend to have a higher density of residential development and are closer to urban centers with jobs and services. The Municipality has also invested in higher levels of public transportation service in these areas. Urban neighborhoods tend to be prioritized for infill, redevelopment, new housing, and revitalization in municipal plans.</p>

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.010 General Provisions**

4 *** **

5 **D. Alternative Equivalent Compliance.**

6 *** **

7 **2. Applicability.**

8 The alternative equivalent compliance procedure shall be available only for the following sections
9 of this title:

10 *** **

11 c. Subsection 21.07.060F., Pedestrian Frontage Standard;

12 **d.** Subsection 21.07.060G., Pedestrian Amenities;

13 *** **

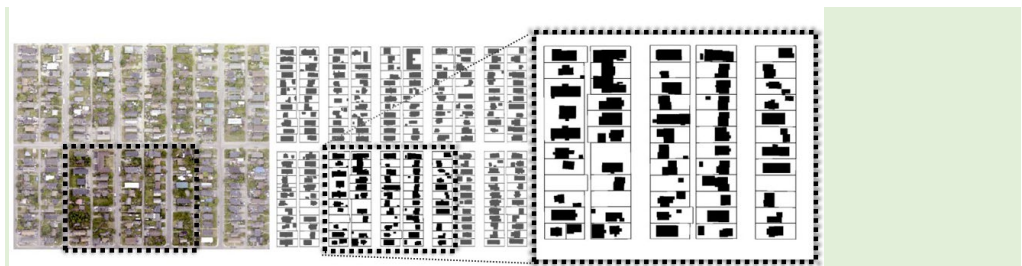
14 **E. Urban Neighborhood Development Context Area**

15 **1. Purpose**

16 This section establishes an Urban Neighborhood Development Context Area that reflects the
17 existing and desired characteristics of Anchorage’s older, urban neighborhoods identified by the
18 Comprehensive Plan, Land Use Plan Map as Traditional Neighborhood Design areas. Some are
19 also identified in neighborhood and district plans. This section provides a basis for development
20 regulations tailored to the characteristics of these neighborhoods and fulfills their role as
21 recommended in the Comprehensive Plan.

22 **a.** This area is intended to include Anchorage’s original urban neighborhoods of Government
23 Hill, South Addition, and Fairview, and post-war era (1950s-1960s) neighborhoods such
24 as Spenard, Airport Heights, and Russian Jack Park. It also includes transit-supportive
25 development corridors and designated mixed-use centers.

26 **b.** This area recognizes urban neighborhood characteristics including a more highly
27 interconnected street system, smaller lot and block sizes, and access to pedestrian
28 facilities and public transit. Some neighborhoods have consistent or frequent sidewalks
29 and alleys. Buildings and residences often have moderate front setbacks and orient to the
30 street, with landscaping or walkways (instead of parking) comprising most of their street
31 frontage. Transit-supportive development corridors and mixed-use centers contribute to a
32 compact, accessible land use pattern.



34 *Regular street grid of city blocks and alleys in Mountain View.*

ANNOTATION FOR PAGE 5

21.07.010E., Urban Neighborhood Development Context Area (cont'd)

The amendments on the next page are a continuation of the amendments from page 4 establishing the Urban Neighborhood Development Context Area, including its purpose statement, applicability to Title 21 regulations, and its geographic extent.

<i>Line (s) #</i>	<i>Comment on Change</i>
12-16	Applicability The urban neighborhood context applies only to development and design standards of chapter 21.07. It cannot be used to override zoning districts or district-specific standards (chapter 21.04), allowed uses (chapter 21.05), or district-specific lot and building dimensions (chapter 21.06).
18-32	Establish the Neighborhood Development Context Areas. This subsection 21.07.015C. establishes the specific Urban Development Context Area with a map. (The map of the urban neighborhood context area is on page 6.) This section includes clarifications for interpreting the map, identifying specific zoning districts, public facility campuses, and certain neighborhood subdivisions that the urban neighborhood context area does or does not include.

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.010 General Provisions**

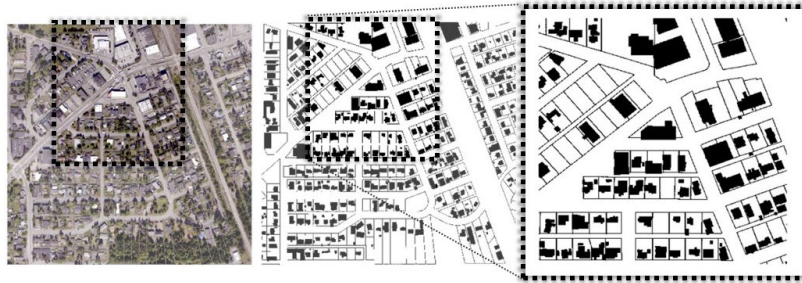
4 *** **

5 **E. Urban Neighborhood Development Context Area**

6 *** **

7 **1. Purpose**

8 *** **



9
10 *More relaxed street grid along a transit corridor in Spenard.*

11
12 **2. Applicability**

13 The neighborhood development context areas in this section shall apply as a basis for area-specific
14 development standards in this chapter 21.07. It shall not be used as a basis to change the allowed
15 uses or review and approval procedures of the underlying zoning district, district-specific
16 dimensional standards, or any other standards of this title.

17
18 **3. Urban Neighborhood Development Context Area Established.**

19 The urban neighborhood development context area is delineated in map 21.07-1, with the following
20 clarifications:

- 21 **a.** Birchwood Park, DeBarr Vista, and Green Acres Subdivisions (and Additions) are included
22 (marked "A" on map).
- 23
- 24 **b.** The Downtown (DT) zoning districts are not included. The university and medical
25 campuses of the UMED District are also not included.
- 26
- 27 **c.** The properties of Merrill Field Airport, Alaska Railroad Terminal Reserve, and Port of
28 Alaska are not included.
- 29
- 30 **d.** Broadmore Estates, Bentzen Addition, Creekside Park, Town Square, Tudor Park,
31 University Park, and Windemere Subdivisions (and their Additions) are not included
32 (marked "B" on map).
- 33
- 34

ANNOTATION FOR PAGE 6

21.07.010E., Urban Neighborhood Development Context Area (cont'd)

Page 6 provides the map of the urban neighborhood context area, to be included in the Title 21 code as part of subsection 21.07.010E.

The Planning Department and municipal GIS services will also be able to maintain a zoom-in, interactive version of the map online for code users and the public.

<i>Line (s) #</i>	<i>Comment on Change</i>
8-9	<p>Map 21.07-1, Urban Neighborhood Development Context Area The map legend indicates that the dark grey-shaded area with black outlines depicts the extent of the designated urban neighborhood development context area.</p> <p>The boundary lines tend to follow major streets (labelled on the map) or use greenbelts or other major geographic landmarks as boundaries.</p> <p>This area is based on neighborhood plans, the Anchorage 2040 Land Use Plan Map's designated "Traditional Neighborhood Design" and "Transit-Supportive Development" areas, and analyses of existing neighborhood types documented in the Anchorage 2040 Land Use Plan's <i>Appendix A: Planning Atlas</i> (Map CC-1: Neighborhood Contexts).</p>

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.010 General Provisions**

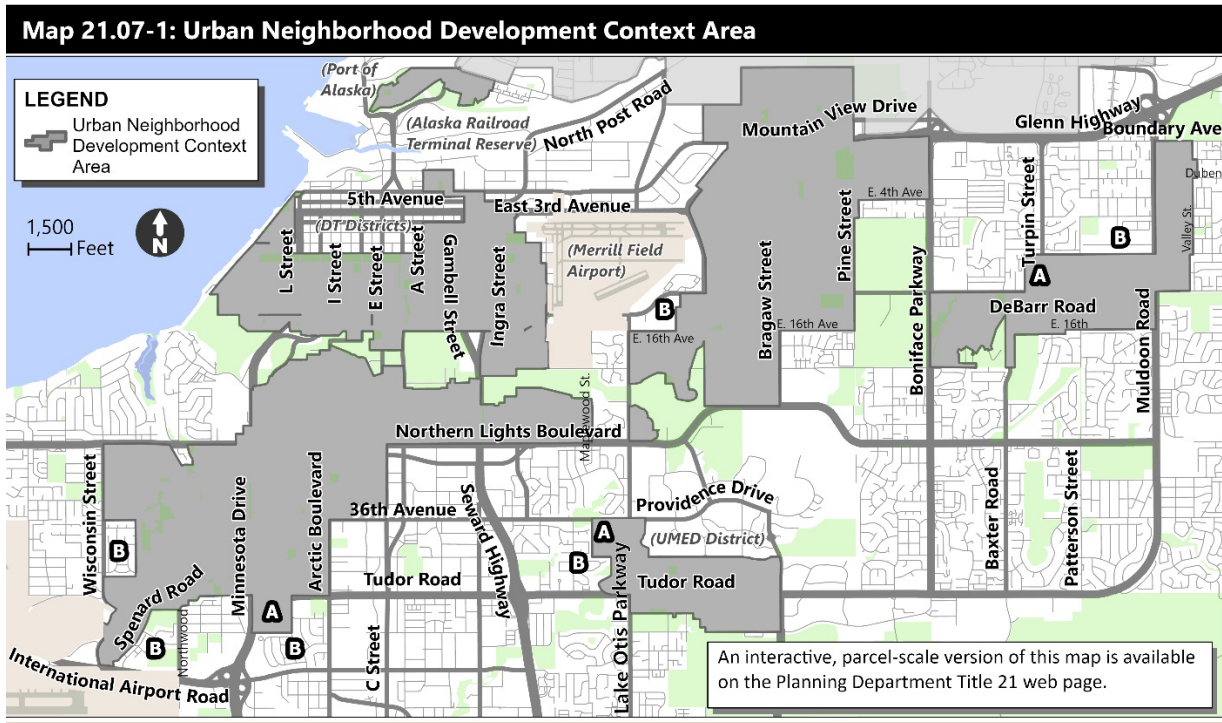
4 *** **

5 **E. Urban Neighborhood Development Context Area**

6 *** **

7 **3. Urban Neighborhood Development Context Area Established.**

8 *** **



9

10 *** **

ANNOTATION FOR PAGE 7

Section 21.07.060E.2., Standards for Pedestrian Facilities (Sidewalks)

Current Section 21.07.060E. establishes the pedestrian walkway requirements in Title 21. It includes existing requirements for public sidewalks, trails, and pathways, and for on-site walkways within development properties.

The following clarifications on the next page are recommended to subsection E.2., Sidewalks.

<i>Line (s) #</i>	<i>Comment on Change</i>
9-10	Provide a reference to existing applicable sidewalk design standard in Chapter 21.08. This is a clarification of existing standards.
11-18	Clarify that the requirement to provide sidewalks in subsection b. applies to street improvement projects and not to private property developments. Existing sidewalk requirements for development projects are provided in subsection f. beginning on line 11.
20-21	Clarify sidewalk requirement applies along all street frontages. This is a clarification of an existing standard based on feedback from municipal zoning/land use review staff.

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **E. Standards for Pedestrian Facilities**

6 *** **

7 **2. Sidewalks**

8 a. All sidewalks shall be designed to comply with the standards of the Design Criteria Manual
9 (DCM) and Municipality of Anchorage Standard Specifications (MASS) and shall be
10 improved in accordance with subsection 21.08.050H.

11 b. Street improvement projects and new streets in subdivisions [I]in all class A zoning districts
12 except for industrial districts[,] shall install sidewalks [SHALL BE INSTALLED]on both
13 sides of all streets (local, collector, arterial, public or private, including loop streets). Where
14 indicated in the comprehensive plan, a pathway may replace a sidewalk on one side. Street
15 improvement projects [I]in industrial zoning districts shall install[,] a sidewalk [SHALL BE
16 INSTALLED]on one side of all local streets, and on both sides of local streets if the new
17 sidewalks would connect to existing sidewalks on both ends and the needed sidewalk
18 length is no greater than one quarter mile.

19 *** **

20 f. Development on lots along existing streets in class A zoning districts shall install sidewalks
21 on all lot frontages abutting streets in the following situations:

22 i. In the R-4, R-4A, and commercial zoning districts[,] AND]

23
24 ii. In the DT zoning districts as set forth in the provisions of chapter 21.11,
25 Downtown.

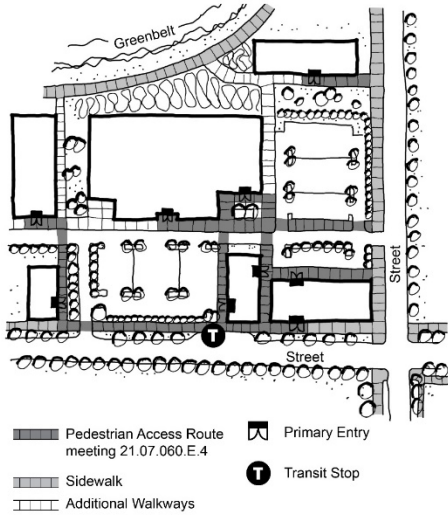
26
27 *** **

ANNOTATION FOR PAGE 8

Section 21.07.060E.4., Standards for Pedestrian Facilities – On-site Walkways

Current Subsection 21.07.060E.4. establishes the requirements for on-site walkways within development properties. The changes on page 8 propose the following consolidations and clarifications:

Line (s) #	Comment on Change
11-16	<p>Enhance existing illustration of continuous pedestrian access. The illustration at right is an enlarged version of the proposed replacement illustration on the next page. This clarifies (does not change) existing standards.</p> <p>Illustrations are not regulatory but rather help convey the intent of the regulations (Section 21.15.010, <i>Rules of Interpretation</i>).</p>
18-20	<p>Consolidate residential walkway requirements and alternative pedestrian access options from Section 21.07.110 Residential Design Standards. This merges and standardizes existing on-site pedestrian connection requirements for residential uses being deleted from sections 21.07.110C.5. (page 31 lines 22-32) and 21.07.110D.3.c. (page 36 lines 5-7), which are proposed to be deleted from those sections.</p>
22-28	<p>Consolidate and clarify on-site walkway width and design requirements. The redundant walkway width and improvements standards are consolidated into this subsection c. The redundant, identical standards for walkway width and improvements are being deleted from subsection 21.07.060F.3. (page 12, lines 29-40).</p> <p>Exclusion of vehicle overhang from the minimum unobstructed sidewalk width is deleted from this subsection because vehicle overhang into sidewalks is currently addressed in the off-street parking section of Title 21 and is unnecessary to repeat here.</p>



1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **E. Standards for Pedestrian Facilities**

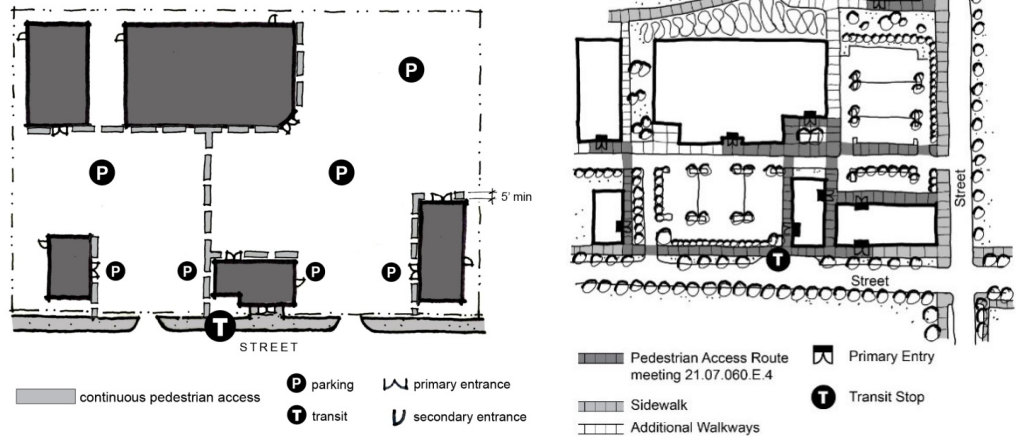
6 *** **

7 **4. On-Site Pedestrian Walkways**

9 **b. On-Site Pedestrian Connections**

11 ii. All primary building entrances on a site shall be connected to the street by a
12 convenient system of walkways. This includes multiple primary entrances into one
13 building, and primary entrances in separate buildings on a site.

14 *(Replace existing illustration on the left with
15 illustration on the right.)*



16 *** **

17
18 **iv.** The primary front entrance of a residential dwelling shall be connected to the street
19 by a walkway as provided in i. through ii. above, or by the dwelling's individual
20 driveway, or by a shared parking courtyard meeting 21.07.060G.21.

21 *** **

22 **c. Walkway Clear Width and Improvements**

23 *** **

24 **i.** The minimum width of a required pedestrian walkway shall be five feet of
25 unobstructed clear width, [EXCLUDING VEHICULAR OVERHANG,]except where
26 otherwise stated in this title. A walkway that provides access to no more than four
27 residential dwelling units may provide an unobstructed clear width of three feet.

28 **ii.** Walkways shall be improved in accordance with subsection 21.08.050H.

29 *** **

Section 21.07.060F., Pedestrian Frontage Standard (New)

Proposed subsection 21.07.060F. consolidates and revises existing pedestrian-oriented frontage standards from other parts of Title 21. (Existing subsection 21.07.060F., *Pedestrian Amenities*, is re-numbered to G.). Pedestrian frontage standards in various sections of current Title 21 include accessible building entries, street-facing buildings with windows; entries, and active interior spaces, and maximum front setbacks or other limitations on parking facilities in front. The consolidated standards in 21.07.060F. will revise, standardize, and simplify the Title 21 pedestrian frontage standards, and make them more generally applicable for consistency among uses. It also tailors pedestrian frontage standards to urban neighborhood context areas versus the suburban contexts in the rest of the Municipality. The urban neighborhood standards support more efficient land use, re-use of urban lots, and alternative transportation modes.

Consolidated Title 21 sections include:

- Mixed-use development standards in commercial zoning districts (21.04.030G. and H., pages 2-3).
- Standards for street-facing windows, building placement, and other requirements for developments that received parking reductions (21.07.090F.4, deleted by companion ordinance A.O. 2022-80(S)).
- Street-facing windows, prominent entrance, garage façade, and building orientation requirements in the Title 21 Residential Design Standards (21.07.110, deletions shown on pages 29-38).
- Limits to the width of street-facing garage doors on lots with alleys (21.07.110F.4., page 41).

Line (s) #	Comment on Change
6-16	Purpose: Consolidate pedestrian frontage intent statements from other parts of Title 21. The purpose statement objectives a, b, and c on lines 10-16 are transferred and revised from Section 21.07.110, <i>Residential Design Standards</i> intent statements that address pedestrian oriented frontages and pedestrian access, shown as deleted on page 68.
18-20	Applicability: Tailor frontage standards specific to the urban neighborhood contexts. Subsection 2 sets the stage for area-specific frontage standards (in subsection 4 on page 10) for the Urban Neighborhood Context Area (21.07.010E., pages 4–6) to match urban neighborhood development character and objectives. Lines 19-20 establish that a separate set of frontage standards apply to the rest of Anchorage. Currently, various parts of Title 21 include a variety of frontage standards that apply solely based on zoning districts, use types, or (until recently) site-specific parking reductions. The proposed changes would no longer apply the same frontages in urban and suburban neighborhoods.
21-27	Exemptions: Carry forward and expand exemptions for developments and areas of the Municipality currently exempted from pedestrian frontage standards. Subsection a. on lines 21-22 exempts changes of use and minor building renovations in existing buildings. Current code does not always exempt existing buildings from pedestrian frontage standards, depending on the Title 21 section. Subsections b, c, and d. on lines 23-27 expand on exemptions from Residential Design Standards (21.07.110B.1; 21.07.110C.2.; 21.07.110D.1.) being consolidated into this section. In subsection b., and exemption for Girdwood, Downtown, and Industrial zones is expanded to include PLI and other zones that would be inappropriate to be included or that have their own standards. In subsection c., an exemption from 21.07.110D.1.for homes built prior to 2014 is changed to 2016.
28-35	Administrative Adjustments: Expand flexibility and relief from Residential Design Standards where they are not practical for projects on existing sites or there is a better design option. Subsection a. on lines 29-30 ties to the amendment on page 4, lines 11-13. Subsection b. includes the energy efficiency exception from 21.07.110C.3.c. (p. 31).

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **F. Pedestrian Frontage Standard**

6 **1. Purpose**

7 The pedestrian frontage standard requires site planning and building orientation toward
8 neighborhood streets and sidewalks to facilitate pedestrian access and reduce automobile parking
9 congestion. Objectives include to:

10 **a.** Organize and orient buildings around public streets and associated frontages in a way that
11 frames streets as positive public space, promotes pedestrian activity, and connects to
12 multiple modes of transportation.

13 **b.** Provide clearly defined, safe pedestrian access to building entries that invites people of all
14 abilities and minimizes conflicts with vehicles and parking.

15 **c.** Place active indoor spaces, entrances, and windows on street-facing building facades to
16 improve the visual connection to the street and promote a safe, secure neighborhood.

17 **2. Applicability**

18 Subsection 4. applies to development in the urban neighborhood development context area
19 established in 21.07.010E. Subsection 5. applies to development in the other areas of the
20 municipality. The following are exempt:

21 **a.** Changes of use and other developments that comprise building modifications of less than
22 50 percent of the total improvement value of the building(s) on the site.

23 **b.** Development in Girdwood, Downtown (DT), CE-DO, CE-EVO, PLI, PR, TA, or industrial
24 zoning districts.

25 **c.** Single-family and two-family dwellings constructed prior to January 1, 2016, or on lots of
26 20,000 square feet or larger, or in Class B zoning districts.

27 **d.** Uses without habitable floor area, such as utility substations.

28 **3. Administrative Adjustments**

29 **a.** The Alternative Equivalent Compliance procedure in 21.07.010D. may be used to propose
30 alternative means of complying with the intent of this section.

31 **b.** The director shall approve administrative relief if the applicant demonstrates the adjustment
32 is necessary to compensate for some practical difficulty of the site. This includes reducing
33 the window area requirement by the amount needed to comply with 5-star or affordable
34 housing (21.15.040) energy rating requirements. The department shall keep record of the
35 approved exception with written findings supporting the exception on file.

ANNOTATION FOR PAGE 10

Subsection 21.07.060F.4., Pedestrian Frontage Standard in Urban Neighborhood Context (New)

Subsection 21.07.060F.4. consolidates and simplifies pedestrian-oriented frontage standards that have been scattered across other parts of Title 21, and tailors them for the urban neighborhood context.

<i>Line #</i>	<i>Comment on Change</i>
Table 21.07-2	<p>Reformat pedestrian frontage standards from other parts of Title 21 into a table. Provide an illustration to visualize the development standards in the table. Each row in the table is a standard and begins with a capital letter that corresponds to the letters labelling the illustration. The table carries forward how Title 21 has applied frontage standards on up to 2 street frontages.</p>
Table, row A	<p>Replace maximum front setbacks and limits on front parking coverage. Existing maximum setbacks are removed from Title 21 commercial district-specific standards in 21.04.030 (page 3). These required half the building façade to be within 20 feet of the street. Additionally, regulations to discourage siting buildings behind parking facilities are removed from the criteria for parking reduction strategies in 21.07.090F. (recently deleted by A.O. 2022-80(S)), and from the residential multifamily building orientation menu in 21.07.110C.6. (pages 32-33). These restricted front parking facilities coverage to no more than 50% of the lot area between the building and the street.</p> <p>All are replaced by a consolidated, more flexible, and generally applicable standard in Table 21.07-2, row A, which requires half of the building façade to have pedestrian facilities and landscaping rather than parking facilities in front of it, with an exception for duplexes in footnote 1. For lots with rear alley access in urban neighborhoods like South Addition and Fairview, parking lots (other than a driveway) will be prohibited between the building façade and the street.</p>
Table, rows B and C	<p>Merge, simplify, and tailor residential garage width and projection limitations for Urban Neighborhood Contexts. Table row B consolidates the current 10-foot maximum garage door width on residential properties with alleys (page 41, line 27), the 50% limitation on properties that received parking reductions in urban contexts (deleted by A.O. 2022-80(S)), the 67% façade width limit on garages on homes and duplexes in 21.07.110D.4.b. (page 36, line 14); and multifamily menu choices 21.07.110C.6.e., f., g., and h. (pages 32-33).</p> <p>The revised garage width and projection limitations are tailored for urban neighborhood contexts. Table row B is adapted from the single-family and two-family design standard in 21.07.110D.4. (page 36 lines 9-13), and the multifamily building orientation menu choices 21.07.110C.6.e., f., g., and h. (pp. 32-33). See the exception for duplexes in footnote 1.</p>
Table, rows D and E	<p>Consolidate, simplify, and tailor urban window requirements. Façade window requirements are consolidated from district-specific standards in 21.04.030G. and H. (pages 2 - 3); former parking reductions criteria in 21.07.060F.4. (deleted by A.O. 2022-80); and residential standards in 21.07.110C.3. (page 30) and 21.07.110D.5. (page 38). The proposed standard for urban contexts matches the former 15% residential and 25% commercial window criteria for urban area-specific parking reductions. The requirements are relaxed on secondary streets frontages are relaxed.</p>
Table, rows F and G.	<p>Consolidate primary entrance requirements and multifamily building orientation menu. Table rows F and G consolidate the primary entrance standards for mixed-use and residential developments from 21.04.030G.7.B. (page 2) and 21.07.110C.9. and D.3. (page 35). Row F carries forward the more flexible residential standard from page 35 instead of the maximum setback-based mixed-use standard for locating entries from page 1. Row G. carries forward specific elements of 21.07.110C.9. and the building orientation menu in 21.07.110C.6. The applicability of the requirements is expanded to non-residential commercial uses.</p> <p>In row G., rather than selecting 3 items from the multifamily building orientation menu in 21.07.110C.6. as under current code, the applicant would select 2 pedestrian amenities from 21.07.060F. (renumbered to 21.07.060G. on page 12 and revised to bring in more menu choices).</p>

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

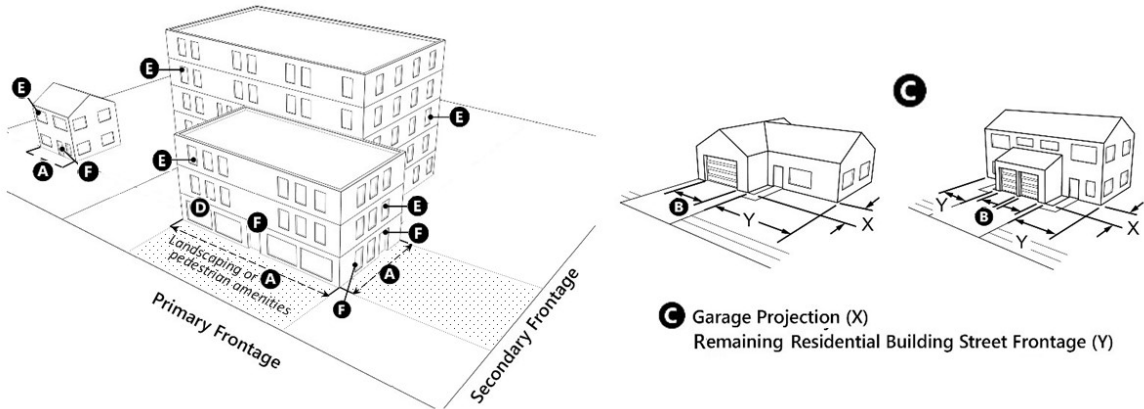
5 **F. Pedestrian Frontage Standard**

6 *** **

7 **4. Standard for Urban Neighborhood Contexts**

8 The standards of table 21.07-2, apply to the primary frontage and one secondary frontage.

9



10
11

Table 21.07-2: Pedestrian Frontage Standard for Urban Neighborhood Contexts

Site Elements for Pedestrian-Oriented Frontages	Standard
<u>A. Minimum required ground-floor, street-facing building elevation with on-site walkways, pedestrian amenities, or landscaping in front—and no off-street automobile parking or circulation</u>	50% of building elevation width ¹ <u>Lots with alley access: no parking allowed in front of building except in driveways for individual dwellings</u>
<u>B. Maximum allowed width of residential garage entrance(s) on ground-floor street-facing building elevation</u>	40% of building elevation width ¹
<u>C. Maximum allowed distance a garage may project out in front of the rest of the street-facing residential building elevation</u>	No more than 50% of the width of the non-garage portion of the street-facing building, on other lots
<u>D. Minimum required visual access windows or primary entrances on non-residential ground-floor street-facing building elevation ²</u>	25% of wall area on primary frontage 15% of wall area on second frontage
<u>E. Minimum required windows or primary entrances on residential and upper-floor non-residential street-facing elevations ²</u>	15% of wall area on primary frontage 10% of wall area on second frontage
<u>F. Requirement for primary entrance facing the street or visible from the street via an unobstructed line of sight</u>	Residential: Meet 21.07.060G.14., Covered, Visible Residential Entrance Other uses: at least one entrance
<u>G. Minimum required number of pedestrian amenities from 21.07.060G. (in addition to G.14.) in multifamily, mixed-use, townhouse, group housing, and non-residential developments.</u>	2 pedestrian amenities
¹ Exception: On lots without alley access in R-2A, R-2D, and R-2M zoning districts, two-family (duplex) developments may have a minimum of 20% in A. above and a maximum of 67% in B. above.	
² Visual access windows shall have a sill height of no more than four feet above finished grade. Rules for measuring window area as a percentage of building wall area are provided in 21.15.020P. Visual access windows (“Window, providing visual access”), primary entrance, and ground-floor wall area are defined in 21.15.040.	

ANNOTATION FOR PAGE 11

Subsection 21.07.060F.5., Pedestrian Frontage Standard in Other Areas of the Municipality (New)

Section 21.07.060F.5. (Table 21.07-3) consolidates pedestrian-oriented frontage standards from other parts of Title 21 that are generally applicable to development in the Anchorage Bowl and Chugiak-Eagle River. It adjusts, simplifies, and relaxes these existing frontage standards for suburban neighborhood contexts. The standards in Table 21.07-3 are generally more relaxed than the equivalent standards for urban neighborhood contexts in Table 21.07-2 (page 10). Both tables use the illustration on page 10.

<i>Line (s) #</i>	<i>Comment on Change</i>
Table 21.07-3	Reformat pedestrian frontage standards from other parts of Title 21 into a table. Each row in the table is a development standard and begins with a capital letter. The letters correspond to the labels in the illustration on page 9. The table brings forward the current approach in other parts of Title 21 of applying frontage standards to 2 frontages. <u>Table 21.07-3 exempts non-residential buildings located more than 100 feet from the street rights-of-way from rows A, D, E, and F.</u>
Table, row A	Replace maximum front setbacks and other restrictions on front parking coverage. Row A consolidates and revises/replaces the maximum setbacks for mixed-uses in 21.04.030 (page 3) and the residential multifamily building orientation menu choices in 21.07.110C.6. (pages 32-34). It also replaces similar criteria for getting reduced parking requirements in 21.07.090F., which were recently deleted by A.O. 2022-80(S). These restricted front parking facilities coverage to no more than 50% of the lot area between the building and the street. Row A allows up to 80% of the building façade width to have parking in front, is required on one street frontage (instead of two), and broadens its applicability to include non-residential commercial uses.
Table, rows B and C	Merge and simplify limitations on residential garage width and projections. Row B merges the current 67% limitation on garages for single- and two-family residences from 21.07.110D.4.b. (page 36), and the multifamily/townhouse building orientation choices 21.07.110C.6.e., f., g., and h. (pages 32-34). Row C brings forward the residential garage standard from 21.07.110D.4.a. (page 36) for single- and two-family buildings and corresponds to building orientation menu choices 21.07.110C.6.e., f., g., and h. (deleted on pages 32-34) for townhouse and multifamily buildings.
Table, rows D and E	Consolidate, simplify, and revise suburban window requirements. Row E replaces and simplifies requirements for residential street-facing windows and entries from sections 21.07.110C.3. (page 30) and 21.07.110D.5. (page 38). The 10% residential window requirement is carried forward but relaxed to 5% on the secondary frontage. Row D carries forward a window requirement for mixed-use developments from 21.04.030, and a façade window criterion for reduced parking requirements in 21.07.090F. deleted by A.O. 2022-80(S). Applicability is broadened to commercial uses.
Table, rows F and G	Consolidate primary entrance requirements and multifamily building orientation. Table rows F and G consolidate the entrance standards for mixed-use and residential developments from 21.04.030G.7.B. (page 2) and 21.07.110C.9. and D.3. (page 35). Row F carries forward the more flexible residential standard from page 33 instead of the maximum setback-based mixed-use standard for locating entries from page 2. Row G. carries forward elements of 21.07.110C.9. and the building orientation menu in 21.07.110C.6. (pp. 32-34), which required applicants to select 3 choices (Row G requires 1 menu choice). The applicability of the requirements is expanded to non-residential commercial uses.

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **F. Pedestrian Frontage Standard**

6 **5. Standard for Areas Outside of Urban Neighborhood Contexts**

7 The standards of table 21.07-3, apply to the primary frontage and one secondary frontage.

Table 21.07-3: Pedestrian Frontage Standard – Outside of Urban Neighborhood Contexts

<u>Site Elements for Pedestrian-Oriented Frontages</u>	<u>Standard</u>
<u>A. Minimum required ground-floor, street-facing building elevation with on-site walkways, pedestrian amenities, or landscaping in front—and no off-street automobile parking or circulation ¹</u>	<u>20% of building elevation width, on one frontage</u>
<u>B. Maximum allowed width of residential garage entrance(s) on ground-floor street-facing building elevation</u>	<u>67% of building elevation width</u>
<u>C. Maximum allowed distance a garage may project out in front of the rest of the street-facing residential building elevation</u>	<u>No more than the width of the non-garage portion of the street-facing building elevation</u>
<u>D. Minimum required visual access windows or primary entrances on non-residential ground-floor street-facing building elevation ^{1,2}</u>	<u>15% on primary frontage</u> <u>10% on secondary frontage</u>
<u>E. Minimum required windows or primary entrances on residential and upper-floor non-residential street-facing elevations ^{1,2}</u>	<u>10% on primary frontage</u> <u>5% on secondary frontage</u>
<u>F. Requirement for primary entrance facing the street or visible from the street via an unobstructed line of sight ¹</u>	<u>Residential: Meet 21.07.060G.14., Covered, Visible Residential Entrance.</u> <u>Other uses: at least one entrance.</u>
<u>G. Minimum required number of pedestrian amenities from 21.07.060G. (in addition to G.14.) in multifamily, mixed-use, townhouse, group housing, and non-residential developments.</u>	<u>1 pedestrian amenity</u>

¹ Exception: Non-residential buildings located more than 100 feet from the applicable street rights-of-way are exempt from A., D., E., and F. above.

² Visual access windows shall have a sill height of no more than four feet above finished grade. Rules for measuring window area as a percentage of building wall area are provided in 21.15.020P. Visual access windows (“window, providing visual access”), primary entrance, and ground-floor wall area are defined in 21.15.040.

8

9

ANNOTATION FOR PAGE 12

Section 21.07.060G., Pedestrian Amenities Menu (re-numbered from 21.07.060F.)

Current Section 21.07.060F. (re-numbered to G.) is a one-stop menu of pedestrian amenities and their supplementary design standards. It is referenced by other sections and chapters of Title 21. These pedestrian amenities are used as credit toward bonus incentives, menu choices, and development standards in various parts of Title 21. For example, these pedestrian amenities receive credit toward the proposed Pedestrian Frontage Standard in rows F and G in tables 21.07-2 and -3 (pages 10 and 11).

The changes to Section 21.07.060G. proposed on pages 11-17 include renaming, revising, and adding pedestrian amenity menu choices, resulting in the revised outline of subsections below. Amenities G.3. and G.14. through G.20. below are amended or added by this ordinance and are underlined. Only these underlined subsections being revised or added are shown in the Site Access amendment ordinance.

- | | |
|--|---|
| G.3. <u>Enhanced On-Site Walkway</u> | G.13. Sheltered Transition Space |
| G.4. Ice-Free (Snow Melting) Walkway | <u>G.14. Separated Walkway to the Street</u> |
| G.5. Plaza or Courtyard | <u>G.14. Covered, Visible Residential Entrance</u> |
| G.6. Housing Courtyard | <u>G.15. Enhanced Primary Entrance</u> |
| G.7. Transit Stop or Shelter | <u>G.16. Enhanced Transparency</u> |
| G.8. Pedestrian Shelter | <u>G.17. Pedestrian Frontage Free of Parking...</u> |
| G.9. Arcade (or Building Recess) | <u>G.18. Site Entry Feature</u> |
| G.10. Atrium, Galleria, or Winter Garden | <u>G.19. Pedestrian-Interactive Building</u> |
| G.11. Sun Pocket (or Sun Trap) | <u>G.20. Enhanced Street Sidewalk</u> |
| G.12. Reflected Sunlight | <u>G.21. Shared Parking Courtyard</u> |

<i>Line (s) #</i>	<i>Comment on Change</i>
7-28	<p>Purpose and applicability of pedestrian amenities section 21.07.060G.</p> <p>Improve clarity and brevity. Simplify the explanation regarding when the design standards for the pedestrian amenities in section 21.07.060G. are required. Clarify that the design standards for the pedestrian amenities apply only when a pedestrian amenity is being used to count toward a requirement or incentive in Title 21.</p> <p>No substantive changes are proposed to applicability or administrative adjustments.</p>
29-40	<p>Delete redundant definition and regulation for on-site walkways.</p> <p>This definition of a walkway is proposed to be deleted from this section because the same definition for this term is provided in the Title 21 glossary in 21.15.040.</p> <p>The walkway standards deleted on lines 35-40 duplicate the standards for walkway width and design provided in 21.07.060E.4.c. (page 8, lines 22-28).</p> <p><i>Discussion:</i> This consolidation removes redundancy and confusion by placing walkway standards in one location of Title 21. This change will assist development applicants seeking to meet the walkway requirements to become eligible for parking reductions.</p>

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **G[F]. Pedestrian Amenities Menu**

6 *** **

7 **1. Purpose**

8 T[HE PURPOSE OF T]his section [IS TO]defines and provides standards for pedestrian amenities
9 that are used in developments to satisfy a requirement, menu choice, or incentive in this title[MAY
10 BE REQUIRED OR INCLUDED IN A MENU OF CHOICES TO MEET A REQUIREMENT, OR
11 LISTED AS A SPECIAL FEATURE THAT CAN COUNT TOWARD A BONUS INCENTIVE
12 ANYWHERE IN THIS TITLE. FOR EXAMPLE, ANOTHER SECTION OF THIS TITLE MAY LIST A
13 PEDESTRIAN AMENITY AS A SPECIAL FEATURE FOR WHICH BONUS FLOOR AREA MAY
14 BE GRANTED.] The standards [CONTAINED] in this section give predictability for applicants and
15 the public, [DECISION-MAKERS, AND THE COMMUNITY] for the minimum acceptable standards
16 for pedestrian amenities. It [ALSO ENSURES THE] encourages amenities that will improve and
17 enhance the community[TO THE BENEFIT OF ALL,] and respond to Anchorage's[THE] northern
18 latitude climate. [THIS TITLE PROVIDES FLEXIBILITY TO ENCOURAGE AND ALLOW FOR
19 CREATIVITY AND UNIQUE SITUATIONS THROUGH THE ALTERNATIVE EQUIVALENT
20 COMPLIANCE AND MINOR MODIFICATIONS PROCESS.]

21 **2. Applicability.**

22 Pedestrian amenities shall meet the minimum standards of this section [IN ORDER]to be credited
23 toward a requirement, menu choice, or [AS A SPECIAL FEATURE BONUS] incentive of this title,
24 except where specifically provided otherwise in this title. The standards of this section do not apply
25 to amenities that are not counted toward a requirement, menu choice, or incentive under this title.

26 **3. Administrative Adjustments.**

27 The alternative equivalent compliance procedure set forth in subsection 21.07.010D. may be used
28 to propose alternative means of complying with the standards of this subsection 21.07.060G[F].

29 **[3. WALKWAY]**

30 [A WALKWAY IS A SURFACE THAT CONNECTS TWO POINTS FOR PEDESTRIAN USE, AS
31 DEFINED IN CHAPTER 21.15. A WALKWAY MAY BE IN A PUBLICLY DEDICATED
32 PEDESTRIAN EASEMENT. EXAMPLES INCLUDE PEDESTRIAN CONNECTIONS WITHIN ONE
33 DEVELOPMENT SITE, MID-BLOCK, BETWEEN SUBDIVISIONS, OR LEADING FROM
34 STREETS TO PUBLIC AMENITIES, SUCH AS SCHOOLS OR PARKS.]

35 [A. A WALKWAY SHALL HAVE A MINIMUM UNOBSTRUCTED CLEAR WIDTH OF FIVE
36 FEET, EXCEPT WHERE OTHERWISE STATED IN THIS TITLE. A WALKWAY THAT
37 PROVIDES ACCESS TO NO MORE THAN FOUR RESIDENTIAL DWELLING UNITS
38 MAY HAVE AN UNOBSTRUCTED CLEAR WIDTH OF THREE FEET.]

39 [B. WALKWAYS SHALL BE IMPROVED IN ACCORDANCE WITH SUBSECTION
40 21.08.050H.]

ANNOTATION FOR PAGE 13

**21.07.060F.4., Primary Pedestrian Walkway
(Re-named and re-numbered to 21.07.060G.3., Enhanced On-Site Walkway)**

This is an existing pedestrian amenity referenced by menus in various parts of Title 21.

<i>Line (s) #</i>	<i>Comment on Change</i>
7-46	<p>Clarify and simplify this pedestrian amenity and make its supplementary design and dimensional standards more flexible for applicants. It is renamed to “Enhanced On-site Walkway,” to help distinguish it from public sidewalks and to remove confusion caused by the word “primary”.</p> <p>Its existing dimensional and design standards for pedestrian movement zones, building interface zones, and vehicle areas buffer zones in the walkway cross-section are simplified and clarified.</p> <p>Substantive changes to the standards for this amenity are specified below.</p>
20-21	<p>Delete the minimum 50-foot length requirement. This change makes small infill developments with building entrances closer than 50 feet to the street eligible to get credit for this amenity.</p>
22-23	<p>Reduce minimum required walkway width. For walkways providing access to 4 or fewer dwellings, the minimum clear width of the pedestrian movement zone portion of the walkway is reduced from eight to six feet.</p> <p>The minimum total required width of the walkway, including its building interface zone, vehicle areas buffer zone, and pedestrian movement zone, is reduced from 14 feet to 12 feet.</p> <p>The required buffer space from adjoining parking facilities is reduced from 4 feet to 2 feet.</p>
36-42	<p>Change “pedestrian scale lighting” from a requirement to a menu option. The requirement for pedestrian lighting on lines 41-42 is merged into the requirement for “pedestrian features” on line 36 (defined in 21.15.040, see page 44), which includes ornamental lighting for architecture and landscaping, and pedestrian-scale area lighting, as options.</p> <p>For this reason, the number of pedestrian features required on line 36 is increased from 1 to 2 features. Pedestrian lighting is no longer required to be selected in addition as a separate amenity from line 36.</p>

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **G[F]. Pedestrian Amenities Menu**

6 *** **

7 **4. Enhanced On-Site[PRIMARY PEDESTRIAN] Walkway**

8 An enhanced on-site[A PRIMARY PEDESTRIAN] walkway is intended to provide an option for
9 applicants to receive credit for exceeding the minimum development standards for walkways and
10 improving pedestrian convenience, comfort, and safety on the site. Enhanced on-site walkways
11 provide additional width [UNOBSTRUCTED CLEAR WIDTH OF AT LEAST EIGHT FEET] for
12 pedestrian movement [WITH ADDITIONAL SPACE INCORPORATING FEATURES ALONG THE
13 WALKWAY SUCH AS STOREFRONT SIDEWALK SPACE, ROOM FOR RESIDENTIAL STOOPS
14 OR BUILDING FOUNDATION PLANTINGS,] and peripheral space that accommodates
15 landscaping, furniture, and utilities. [AS ESTABLISHED GENERALLY IN SUBSECTION F.1 AND
16 F. 2 ABOVE, THE STANDARDS OF THIS SUBSECTION APPLY ONLY WHERE THE SPECIFIC
17 TERM "PRIMARY PEDESTRIAN WALKWAY" IS LISTED AS A REQUIREMENT, MENU CHOICE,
18 OR SPECIAL FEATURE THAT COUNTS TOWARD A BONUS. THIS SUBSECTION IS NOT A
19 GENERALLY APPLICABLE REQUIREMENT FOR OTHER LARGE WALKWAYS.]

20 **a.** [A PRIMARY PEDESTRIAN WALKWAY SHALL BE DEVELOPED AS A CONTINUOUS
21 PEDESTRIAN ROUTE EXTENDING FOR AT LEAST 50 FEET.]

22 **[B.]** An enhanced on-site[PRIMARY PEDESTRIAN] walkway shall have a pedestrian
23 movement zone with a continuous,[N] unobstructed walkway clear width of at least eight
24 feet, or six feet where providing access only to four or fewer residential dwelling units.
25 Where adjacent to a ground-floor building elevation it shall also have a [SIDEWALK
26 STOREFRONT OR] building interface zone a minimum of two feet in width for building
27 foundation landscaping or [THREE FEET IN WIDTH OF SIDEWALK] space for opening
28 doors or seating and transition pedestrian spaces. In addition to the pedestrian movement
29 zone and any building interface zone, the enhanced on-site walkway shall have a buffer
30 space of at least two[FOUR] feet in width where abutting motor vehicle parking lots,
31 circulation aisles, or driveways [SHALL BE INCORPORATED AS PART OF THE
32 WALKWAY WHEN ABUTTING ANY STREET OR VEHICLE AREA,]. The buffer space
33 shall [TO] accommodate [STREET TREES,]landscaping beds, fencing or bollards, light
34 poles, utilities, benches, and other furnishings[OBJECTS TO BE KEPT CLEAR OF THE
35 WALKWAY].

36 **b[C].** A minimum of two pedestrian features as defined by this title (21.15.040) shall be provided
37 along the enhanced walkway, with at least one for every 50 feet of the walkway length[AT
38 LEAST ONE PEDESTRIAN FEATURE AS DEFINED BY THIS TITLE SHALL BE
39 PROVIDED FOR EVERY 50 FEET OF LENGTH ALONG A PRIMARY PEDESTRIAN
40 WALKWAY].

41 **[D.]** A PRIMARY PEDESTRIAN WALKWAY SHALL BE ILLUMINATED WITH PEDESTRIAN
42 SCALE LIGHTING.]

43 **c[E].** [A]Enhanced on-site[PRIMARY PEDESTRIAN] walkways shall provide continuous,
44 direct[LY] connections from building primary entrance(s) to surrounding public streets and
45 sidewalks[,] and be publicly accessible or available to all residents of the development[AT
46 ALL TIMES].

47 *** **

ANNOTATION FOR PAGE 14

Subsection 21.07.060G.13., Separated Walkway to Street and Covered (Relocated/Revised)
Subsection 21.07.060G.14., Covered, Visible Residential Entrance (Relocated/Revised)
Subsection 21.07.060G.15., Enhanced Primary Entrance (Relocated/Revised)

Pedestrian facility requirements and menu choices are moved from other parts of Title 21 to become additional Pedestrian Amenities Menu choices.

<i>Line (s) #</i>	<i>Comment on Change</i>
7-11	<p>Add subsection 21.07.060G.13., Separated Walkway to the Street. Multifamily Building Orientation Menu item 21.07.110C.6.j., <i>Separated Walkway to the Street</i>, is proposed to be relocated from the Residential Design Standards (page 33, lines 31-34) to become a Pedestrian Amenity menu option in 21.07.060G.</p> <p>A clarification of the minimum separation from parking facilities is proposed on lines 10-11 (the deleted standard being on page 33, lines 33-34).</p>
12-23	<p>Add subsection 21.07.060G.14., Covered, Visible Residential Entrance.</p> <p>Relocate and merge the residential entryway requirements from the Residential Design Standards. The residential design standards are proposed to be removed from 21.07.110C.9. and D.3. (page 35) and C.6.k. (page 34). The replacement standard is referenced by the Pedestrian Frontage Standard tables 21.07-2 and 21.07-3 in row F (see pages 10-11).</p> <p>The current Residential Design Standards minimum area requirement of 16 square feet for the entry porch, stoop, or landing is carried forward for all household types.</p> <p>The minimum area of the sheltering roof over entrances for single-family and duplex homes from page 35 is reduced from 16 to 12 square feet, to be equivalent to the existing 12 square foot sheltering roof standard over multifamily and townhouse entries (page 35, lines 12-14).</p>
24-39	<p>Add subsection 21.07.060G.15., Enhanced Primary Entrance.</p> <p>Relocate and revise the content of the mixed-use development standard for prominent, accessible building entrances from section 21.04.030G.7.b. (pages 2-3). Convert the standard into a Pedestrian Amenity menu option through which applicants may earn credit toward Title 21 requirements. This makes the “Enhanced Primary Entrance” option available to residential and non-residential developments, in addition to mixed-use projects.</p> <p>Expand the options for how to make an enhanced entrance visible and inviting. Remove specifications for exact dimensions. Insert elements from the menu choices for multifamily and townhouse design standards subsection 21.07.110C.9., <i>Entryway Treatment</i>. By comparison to the deleted language from Section 21.04.030G.7.b. shown on pages 2-3 (starting on line 40 of page 2), the changes include:</p> <ul style="list-style-type: none"> • Adding building wall modulation to expand the recessed/projected entry choice; • Eliminating the min. 80 sf. space requirement for recessed and projected entries; • Adding to the list of detail features and changes in exterior finishes that get credit. • Adding extra landscaping as a menu choice; and • Adding “pedestrian features” (defined in 21.15.040 on page 44) as menu choices.

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **G[F]. Pedestrian Amenities Menu**

6 *** **

7 **13. Separated Walkway to the Street**

8 The development shall connect the building primary entrances to the street with a clear and direct
9 walkway that is not routed through a parking facility or across vehicle driveways or circulation aisles.
10 The minimum clear width portion of the walkway shall be separated from the parking facility by at
11 least five feet.

12 **14. Covered, Visible Residential Entrance**

13 A porch, stoop, or landing sheltered by a roof is intended to give visual emphasis to the building
14 entrance as an aid in wayfinding, and help provide safe, convenient access to residential buildings
15 from the street. The entrance shall meet the following standards:

16 a. The entrance shall incorporate a porch, stoop, or landing with an internal dimension of at
17 least 16 square feet, and a permanent, sheltering roof covering at least 12 square feet.

18 b. The porch, stoop, or landing shall be distinguished from adjoining areas and vehicle
19 parking by vertical separation or a change in surfacing material.

20 c. The building entrance shall also be visible (via an unobstructed line of sight) from a street
21 or face a common private open space (21.07.030), a plaza or courtyard (21.07.060G.6.),
22 a housing courtyard (21.07.060G.7.), or a shared parking courtyard (21.07.060G.21.) that
23 is visible from a street.

24 **15. Enhanced Primary Entrance**

25 An enhanced building entry is intended to provide an option for applicants to receive credit for a
26 more prominent and inviting primary pedestrian entrance. To receive credit, the enhanced primary
27 entrance shall incorporate at least three of the following features:

28 a. Outdoor sheltering roof feature projecting from the building façade such as an overhang,
29 portico, canopy, marquee with an inside dimension of at least 16 square feet;

30 b. Recessed and/or projected entrance or other building wall modulation with projections or
31 recesses in the building wall plane;

32 c. Changes in the building's main roofline such as arches, peaked roof forms, or terracing
33 parapets;

34 d. Changes in siding material or exterior finishes, or façade detail features such as tilework
35 that emphasize the entrance;

36 e. Entrance plaza, patio, or similar common private open space;

37 f. Landscaping not otherwise required by this title, such as integrated planters, landscape
38 accent lighting, or special paving treatments; or

39 g. One or more pedestrian features (21.15.040) such as pedestrian-scale lighting or seating.

ANNOTATION FOR PAGE 15

Subsection 21.07.060G.16., Enhanced Transparency (*Relocated/Revised*)
Subsection 21.07.060G.17., Pedestrian Frontage Free of Parking in Front (*Relocated/Revised*)
Subsection 21.07.060G.18., Site Entry Feature (*Relocated/Revised*)

More pedestrian facility provisions are moved from other parts of Title 21 to become additional Pedestrian Amenity Menu choices.

<i>Line (s) #</i>	<i>Comment on Change</i>
7-8	<p>Add Subsection 21.07.060G.16., <i>Enhanced Transparency</i>.</p> <p>Relocate the “Additional Transparency” menu choice from the building and site orientation menu being deleted from the Residential Design Standards in 21.07.110C.6.d. (page 32).</p> <p>This new menu choice gives credit to buildings that exemplify a stronger orientation to the public street. It offers a revised version of the multifamily and townhouse building orientation menu item 21.07.110C.6.d., in which windows or entries comprise 20% of the building facade. Switching the standard to be “...an additional 5%...” enables this menu choice to be available to more development types with different window requirements.</p>
10-14	<p>Add Subsection 21.07.060G.17., <i>Pedestrian Frontage Free of Parking in Front</i>.</p> <p>This new menu choice offers a more enhanced version of the multifamily and townhouse building orientation menu items 21.07.110C.6.e, f, and g, in which parking is placed beside or behind the building. It gives credit to buildings that exemplify a stronger orientation to the public street.</p>
15-24	<p>Add Subsection 21.07.060G.18., <i>Site Entry Feature</i>.</p> <p>Relocate the “Site Entry Feature” menu choice from the building and site orientation menu being deleted from the Residential Design Standards in 21.07.110C.6.m. (page 34). The relocated menu choice is made available to all uses as an option for earning credit in the Pedestrian Frontage Standards on pages 10-11 and other locations in Title 21.</p> <p>On line 23, in subsection d., the pedestrian lighting menu option provision from 21.07.110C.6.m. (page 34) is revised to become more flexible, by allowing the lighting to be either walkway area lighting or ornamental lighting. Ornamental (decorative) lighting could include up-lighting on trees or facades, or be built into objects or architectural facades. Area lighting could be included as part of the decorative lighting. This could improve the function or the aesthetics. See also the definition for “pedestrian feature” in 21.15.040 (page 44).</p>

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **G[F]. Pedestrian Amenities Menu**

6 *** **

7 **16. Enhanced Transparency**

8 Increase the windows and/or primary entrances on street-facing building elevations which are
9 subject to the window requirement in 21.07.060F., by an additional five (5) percent of the wall area.

10 **17. Pedestrian Frontage Free of Parking in Front**

11 In urban neighborhood development contexts (21.07.010E.), provide a street frontage without
12 driveways, parking, or loading facilities in front of at least 75% of the ground-floor, street-facing
13 building elevation. In other areas, provide such a frontage in front of at least 33% of the street-
14 facing building elevation.

15 **18. Site Entry Feature**

16 Highlight and define a pedestrian entrance to a development site using three or more of the
17 following elements:

18 **a.** Landscape treatment with seasonal color and trees, which clearly distinguishes and
19 highlights the site entry.

20 **b.** Plaza or courtyard as described in subsection 21.07.060G.5.

21 **c.** Identifying building primary entrance form including a covered entry, when the primary
22 entrance is within 50 feet of the site entrance.

23 **d.** Special paving, pedestrian walkway area lighting, ornamental lighting, and/or bollards.

24 **e.** Ornamental gate and/or fence.

25 *** **

ANNOTATION FOR PAGE 16

**Section 21.07.060F.16., Pedestrian-Interactive Use
(Re-named to 21.07.060G.19., Pedestrian-Interactive Building)**

This existing Pedestrian Amenity Menu choice is re-numbered, renamed, and revised.

<i>Line (s) #</i>	<i>Comment on Change</i>
7-11	<p>Simplify and reduce the standards for this existing pedestrian amenity.</p> <p>Revise this amenity to increase the opportunity for more developments to use this amenity option and receive credit for providing additional street-facing habitable floor area and façade transparency. The revised standard replaces similar menu choices from sections proposed to be deleted from the Residential Design Standards (21.07.110).</p> <p>Substantive changes to the standards for this amenity are described below.</p>
15-29	<p>Remove land use requirements.</p> <p>No longer limit which use types are allowed in the building. Instead focus on the building design, and let building uses change over time.</p>
37-40	<p>Remove the maximum setback requirement (i.e., build-to lines).</p> <p>Lines 37-40 retains the portion of the existing maximum setback requirement that requires at least 50% of the building façade to not have parking facilities located in front of it (per Title 21 chapter 21.06 general standards for maximum setbacks).</p>
32-36	<p>Reduce habitable floor space requirements within the building façade.</p> <p>This change reduces the percentage of the ground-floor, street-facing façade width that must be habitable floor space. Habitable floor area is defined in 21.15.040.</p>
41-46	<p>Reduce window area requirements for non-residential uses.</p> <p>The minimum window area for commercial uses is reduced from 67% to 50% of the ground-floor façade wall area, and to only 20% of upper-floor façade area.</p> <p>The residential window area requirement of 20% is carried forward.</p> <p>Both standards are revised to allow primary entries to count toward the minimum percentage.</p>

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **G[F]. Pedestrian Amenities Menu**

6 *** **

7 **19[16]. Pedestrian-Interactive Building[USE]**

8 A pedestrian-interactive building[USE] is intended to provide interior[GROUND-FLOOR] spaces
9 that [STRONGLY] engage the sidewalk with street-facing windows and entrances and [FEATURE]
10 activities and services that support neighborhood residents, and generally contribute to the activity
11 level and quality of the pedestrian[-ORIENTED] environment of the neighborhood or district. [THE
12 STANDARDS THAT FOLLOW APPLY WHERE THE TERM "PEDESTRIAN-INTERACTIVE USE"
13 IS LISTED IN THIS TITLE AS A REQUIREMENT, SPECIAL FEATURE FOR A BONUS, OR A
14 MENU CHOICE.]

15 [A. A PEDESTRIAN-INTERACTIVE USE SHALL BE ANY OF THE FOLLOWING USES THAT
16 ARE PERMITTED IN THE DISTRICT: RETAIL AND PET SERVICES; FINANCIAL
17 INSTITUTION PROVIDING BANKING SERVICES OPEN TO THE PUBLIC WITH AT
18 LEAST ONE EMPLOYEE ON SITE; FOOD OR BEVERAGE SERVICE; PERSONAL
19 SERVICE; CULTURAL FACILITY; OR THE FRONTAGE OF ENTRYWAYS OR
20 STAIRWAYS THROUGH WHICH SUCH USES ARE PRINCIPALLY ACCESSED;]

21 [B. RETAIL SALES USES THAT ARE PERMITTED IN THE DISTRICT SHALL BE
22 CONSIDERED PEDESTRIAN-INTERACTIVE USES, EXCEPT FOR THE FOLLOWING
23 TYPES OF RETAIL SALES USES: FUELING STATION; BUILDING MATERIALS
24 STORE.]

25 [C. THE FOLLOWING USES SUPPORTING RESIDENTIAL NEIGHBORHOOD AND
26 HOUSING DEVELOPMENT ARE ALSO CONSIDERED PEDESTRIAN-INTERACTIVE
27 USES WHEN PERMITTED IN THE DISTRICT: RESIDENTIAL DWELLINGS WITH
28 INDIVIDUAL FRONT ENTRIES ALONG THE STREET; ELEMENTARY SCHOOL;
29 MIDDLE OR HIGH SCHOOL; HEALTH SERVICES; CHILD CARE CENTER.]

30 **a[D].** A pedestrian-interactive building[USE] shall provide a primary entrance facing the street.
31 Entrances at building corners facing a street may be used to satisfy this requirement.

32 **b[E].** A pedestrian-interactive building[USE] shall contain habitable floor area at least 24 feet
33 deep extending along a minimum of 50 percent of the [FULL]length of the ground-floor,
34 street-facing building elevation in Urban Neighborhood Contexts, and 30 percent
35 elsewhere. The habitable floor area may include[ALLOWING FOR] pedestrian [AND
36 VEHICLE] entrances, entry lobbies or atriums, and stairwells.

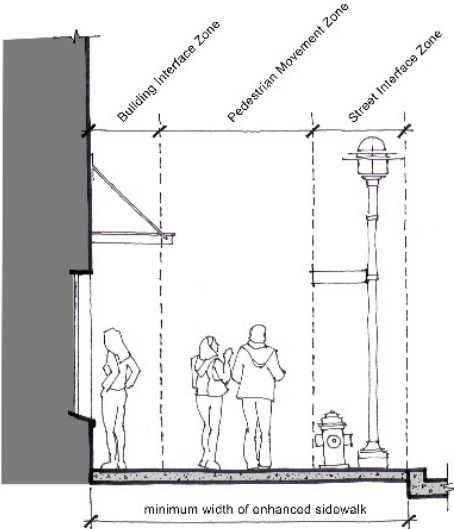
37 **c[F].** At least 50 percent of the street-facing building elevation width of a pedestrian-interactive
38 building shall have no parking facilities in front[USE SHALL COMPLY WITH SUBSECTION
39 21.06.030C.5., MAXIMUM SETBACKS, BUT THE EXCEPTIONS OF SUBSECTION
40 21.06.030C.5.D. SHALL NOT BE AVAILABLE].

41 **d[G].** Street-facing [GROUND-FLOOR] wall areas of a non-residential[PEDESTRIAN-
42 INTERACTIVE] use shall be 50[67] percent visual access windows or primary entrances
43 on the ground floor, and 20 percent windows above the ground floor.[, EXCEPT THAT
44 SUCH]

45 **e.** Street-facing wall areas for residential uses[DWELLINGS] shall be at least 20 percent
46 [VISUAL ACCESS] windows or primary entrances.

ANNOTATION FOR PAGE 17

**Section 21.07.060F.17., Enhanced Sidewalk Option
(Re-named to Section 21.07.060G.19., Enhanced Street Sidewalk)**

Line (s) #	Comment on Change
1-30	<p>Clarify, simplify, and improve flexibility of the “enhanced sidewalk” pedestrian amenity. Clarify the name to “Enhanced <u>Street Sidewalk</u>” to help distinguish it from other pedestrian amenities. Substantive changes to the standards for this amenity are described below and continue on page 18.</p>
20-25	<p>Clarify the minimum unobstructed pedestrian movement zone width of 6 feet and the minimum width of the building interface zone of 2 feet. Retain the overall minimum width of 12 feet for the entire cross-section of the enhanced-street sidewalk.</p>
15-17 and 25-28	<p>Broaden potential usability beyond “main street” commercial frontages to be more inclusive of other kinds of “complete streets” especially for residential developments and neighborhood settings. This refers to the municipal “complete streets” term for a street which provides a good pedestrian environment in addition to motor vehicle access.</p> <p>A definition for Complete Street is added in 21.15.040 on page 44.</p>
29-30	<p>Clarify existing commercial main street sidewalk illustration and add a second, residential sidewalk illustration.</p> <p>The existing commercial sidewalk illustration to be replaced appears below. Width dimensions are proposed to be added to each of the three zones in the sidewalk cross section. A residential sidewalk illustration is proposed to be added to help code users visualize how the building interface zone and street interface zone would be in a residential setting.</p> 

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **G[F]. Pedestrian Amenities Menu**

6 *** **

7 **19[16]. Pedestrian-Interactive Building[USE]**

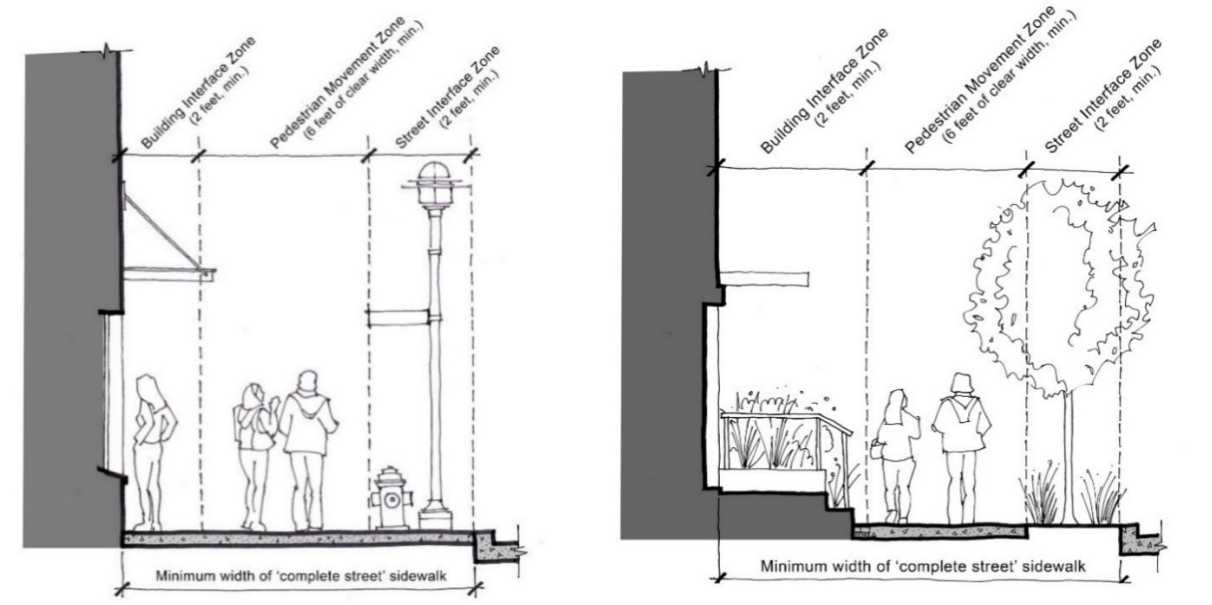
8 *** **

9 **f[H].** Where a building has three or more street frontages, these criteria apply along only two of
10 the frontages.

11 **20[17]. Enhanced Street Sidewalk[OPTION]**

12 An enhanced street sidewalk is intended to provide an option for applicants to receive credit for
13 exceeding the minimum development standards for sidewalks. An enhanced street sidewalk
14 promotes sidewalk widening and streetscape enhancements to support higher levels of pedestrian
15 activity, comfort, and safety in the district[AND ACCESS IN MIXED-USE DEVELOPMENTS]. An
16 enhanced street sidewalk with “complete[MAIN] street” (21.15.040) style amenities may be
17 provided in lieu of required site perimeter landscaping [WHERE IT IS LOGICAL TO SUPPORT A
18 PEDESTRIAN ZONE]as determined through an administrative site plan review, and subject to the
19 following:

- 20 a. The enhanced street sidewalk cross-section[WIDTH] shall be at least 12 feet wide, and
21 include a pedestrian movement zone, building interface zone, and street interface zone
22 (21.15.040). The pedestrian movement zone shall have a walkway clear width of at least
23 six feet. The street interface zone shall be at least two feet wide from back-of-curb, and
24 four feet wide along major arterials. Where a building adjoins the sidewalk, there shall be
25 a building interface zone at least two feet wide [AT LEAST PART OF THE
26 DEVELOPMENT’S FRONTAGE ALONG THE ENHANCED SIDEWALK SHALL FEATURE
27 A PRINCIPAL BUILDING WITH A 20-FOOT MAXIMUM SETBACK IN COMPLIANCE
28 WITH SUBSECTION 21.06.030C.5].



29 **Enhanced Street Sidewalks in [A]Commercial and Residential Settings**

ANNOTATION FOR PAGE 18

Section 21.07.060G.20., Enhanced Street Sidewalk (Continued)

<i>Line (s) #</i>	<i>Comment on Change</i>
9-37	<p>Provide further clarifications, simplification, and reorganization of standards for the enhanced street sidewalk pedestrian amenity.</p> <p>The deleted text on lines 9-10 is moved to the bottom paragraph on the page (lines 36-37). The deleted text on lines 11-14 is replaced by a minimum width of 2 feet for building interface zones and a minimum width of 6 feet for pedestrian movement zones that is added to page 17 lines 22-25. The resulting standards are more flexible.</p> <p>On lines 15-19, the landscaping requirements are relaxed and made more flexible. This includes reducing the tree requirement from “two-thirds” to “half” of the site perimeter landscaping tree requirement and adding a new second sentence with more shrub and perennial landscaping choices.</p> <p><i>Discussion:</i> A shrub has woody skeleton that doesn't die back. Perennials die back to the root each winter then return. An iris is an example perennial. Typical landscape architect professional practice is to differentiate shrubs and perennials in a site plan.</p> <p>Suspended pavement system is the generic term for a system that allows the tree trunk to stick up above the sidewalk and its roots to extend below the sidewalk. The soil bed extends below the sidewalk instead of being contained within a planter on the sidewalk. Silva Cells is a common brand.</p> <p>An Alaska tree will typically survive with 250 cubic yards of soil per tree, but that is a bare minimum. 300 cubic yards is a round number that should yield a decent street tree to achieve the intent of the standard. This information is a part of the arctic design manual for landscape architects in Alaska.</p>

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 *** **

3 **21.07.060 Transportation and Connectivity**

4 *** **

5 **G[F]. Pedestrian Amenities Menu**

6 *** **

7 **20[17]. Enhanced Street Sidewalk[OPTION]**

8 *** **

9 [I. A PUBLIC USE EASEMENT SHALL BE RECORDED FOR ANY PART OF THE
10 DESIGNATED SIDEWALK TO BE LOCATED WITHIN THE SUBJECT PARCEL.]

11 [II. PHYSICAL OBSTRUCTIONS WITHIN THE SIDEWALK'S BUILDING
12 INTERFACE ZONE, SUCH AS LANDSCAPING, ENTRY STOOPS, OR
13 SEATING, SHALL EXTEND NO MORE THAN TWO FEET INTO THE MINIMUM
14 REQUIRED 12 FOOT WIDTH, SO THAT AT LEAST TEN FEET REMAIN.]

15 b. The enhanced street sidewalk shall provide at least half[TWO-THIRDS] the number of
16 trees and shrubs that would otherwise be required for site perimeter landscaping. Shrubs
17 are not required if perennials are substituted for shrubs on a three to one basis or if the
18 tree planting bed is provided as a suspended pavement system with a minimum of 300
19 cubic yards of soil per tree.

20 c. The enhanced street sidewalk may be placed wholly or in part within a right-of-way, subject
21 to approval of the traffic engineer and municipal engineer.

22 i. The enhanced street sidewalk shall be subject to the applicable requirements of
23 title 24, including sections 24.30.020., *Permit to use Public Places*, and 24.90,
24 *Encroachment Permit*.

25 [II. IMPROVEMENTS WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE
26 CONSISTENT WITH THE DCM AND MASS.]

27 ii[III]. Existing improvements that meet the standards of the enhanced street sidewalk
28 may be counted towards the requirements of this section, subject to approval by
29 the director.

30 iii[IV]. The owner shall maintain landscaping and amenities for the enhanced street
31 sidewalk within the right-of-way[, AND COMPLY WITH THE PROVISIONS FOR
32 REMOVAL OF SNOW AND ICE IN AMC 24.80.090, 100, AND 110].

33 iv[V]. Where the right-of-way is not adequate or cannot be configured to accommodate
34 the enhanced street sidewalk, then the development shall be set back from the
35 street frontage as necessary to accommodate part of the improvements within the
36 property. A public use easement shall be recorded for any part of the designated
37 sidewalk width to be located within the subject parcel.

38 **21[19]. Shared Parking Courtyard**

39 *(Note: No changes to Shared Parking Courtyard subsection)*

40 *** **

41 *** **

ANNOTATION FOR PAGE 19

Section 21.07.090A., Off-Street Parking and Loading – Purpose

The amendments to the purpose statement of the off-street parking section on this page adds an objective regarding the public hearing draft driveway regulations. This objective was originally recommended in A.O. 2022-80 as approved by the Planning and Zoning Commission.

This subsection determines the applicability of Title 21 parking facility and driveway development standards. The change on page 18 brings the statement into consistency with the recently amended parking requirements in subsection 21.07.090E.1. which reads, "All areas used for off-street parking shall be constructed as parking facilities that meet the standards of section 21.07.090H. ..."

With the recent removal of parking minimums from Title 21, this change ensures all areas utilized regularly for parking are designed, dimensioned, and engineered to accommodate motor vehicles, ADA accessibility, water quality treatment and storm water runoff management and avoid impacts to other site facilities, neighboring properties, and abutting streets.

<i>Line (s) #</i>	<i>Comment on Change</i>
3-7	Purpose Subsection A.5.: Add an objective regarding the driveway regulations to the Purpose statement for the Off-Street Parking and Loading section. This objective indicates the intent of the public hearing draft driveway and vehicle access regulations shown on pages 20-27.
12-13	Applicability Subsection H.2.: Clarify that all areas used for off-street parking must meet the parking and loading facility design standards in 21.07.090H. This section contains the code standards for driveway and parking dimensions, water and snow management, surfacing, and landscaping. This change brings H.2. into alignment with the recently adopted parking requirements in subsection 21.07.090E.1.d.

1 **21.07.090 Off-Street Parking and Loading**

2 *** **

3 **A. Purpose**

4 *** **

5 **5.** Manage the width and frequency of driveways along street frontages to reduce conflicts with traffic
6 and provide space for street snow clearing and storage, on-street parking (where appropriate), and
7 pedestrian facilities.

8 *** **

9 **H. Parking and Loading Facility Design Standards**

10 *** **

11 **2. Applicability.**

12 These standards apply to all areas used for off-street parking, and any parking facility or loading
13 facility [INCLUDING ALL PARKING SPACES] in a development, except where stated otherwise.
14 A temporary parking lot shall comply with all applicable development requirements of this title for
15 surface parking lots and parking lot landscaping, except when associated with another temporary
16 use permitted pursuant to section 21.05.080.

17 *** **

18

19

ANNOTATION FOR PAGE 20

Section 21.07.090H.8., Vehicular Access and Circulation

This section of the Title 21 *Off-Street Parking and Loading* requirements establishes the vehicle driveway and on-site circulation layout and design standards for site developments. It interacts with the Municipal Driveway Standards, a policy document that is separate from Title 21 and that primarily applies to driveways within the public street right-of-way.

In current Title 21, some driveway and circulation requirements are unclear or left unstated or arranged out of order within the subsections of 21.07.090H.8. Others are in different parts of Title 21, such as the residential driveway and alley access requirements. The revisions on pages 20-27 subdivide subsection H.8 into four topic-specific subsections organized in a sequence of site planning steps, from the general location of site access (H.9.) to the details of on-site circulation aisle curbs and sight triangles in (H.11.):

- H.8. Vehicle Access and Circulation – General (*establishes a general framework*)
- H.9. Vehicular Access and Parking Location
- H.10. Access to Parking Spaces
- H.11. Driveway Design and Dimensions

These revisions clarify the vehicle access standards and references to the Municipal Driveway Standards, consolidate driveway and alley access standards from Section 21.07.110., *Residential Design Standards*, and propose amendments such as reducing minimum width requirements for residential access driveways.

The changes to H.8. on the next page simplify and add flexibility to the general standards:

<i>Line (s) #</i>	<i>Comment on Change</i>
8-13; 28-32	Remove all plan submittal requirements from this section. The parking reforms in A.O. 2022-80(S) consolidated all parking facility plan submittal requirements from this subsection into section 21.07.090D., <i>Parking Lot Layout and Design Plan</i> , the generally applicable submittal requirements for parking facility layout plans.
23-28	Remove circulation delineation standards and the requirement to provide curbed end islands from H.8.b. to the appropriate subsection on page 26, lines 14-19.
33-38	Delete an unused requirement in subsection H.8.c.

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.090 Off-Street Parking and Loading**

2 *** **

3 **H. Parking and Loading Facility Design Standards**

4 *** **

5 **8. Vehicular Access and Circulation – General**

6 Parking lots and structures access, layout, and dimensions shall be designed for a safe and orderly
7 flow of traffic throughout the site, as provided in [THE] subsections 9. through 12. that follow.

8 [A. KEY ELEMENTS]

9 [THE PARKING FACILITY LAYOUT, CIRCULATION, AND DESIGN PLAN SHALL
10 ADDRESS THE FOLLOWING ELEMENTS AS THEY RELATE TO PARKING LOTS,
11 INCLUDING BUT NOT LIMITED TO: FIRE LANES, EMERGENCY ACCESS, DRIVE-
12 THROUGH, QUEUING SPACES, PASSENGER LOADING ZONES, PEDESTRIAN
13 CIRCULATION, AND LOADING BERTHS.]

14 [B. CIRCULATION PATTERNS]

15 [INTERNAL] Vehicular access and circulation patterns and the location and traffic direction
16 of all circulation aisles, driveways, and queuing lanes shall be designed and maintained in
17 accordance with [THE MUNICIPAL DRIVEWAY STANDARDS CURRENTLY
18 ESTABLISHED BY THE TRAFFIC ENGINEER, AND WITH]accepted principles of traffic
19 engineering and safety as determined by the traffic engineer. [, PER THE TRAFFIC
20 ENGINEER'S REVIEW BASED ON THE CURRENT MANUALS OF THE INSTITUTE OF
21 TRANSPORTATION ENGINEERS AND THE URBAN LAND INSTITUTE, AND THE
22 MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES OR THE SUCCESSOR
23 DOCUMENTS. CIRCULATION PATTERNS WITHIN PARKING FACILITIES SHALL BE
24 WELL DEFINED WITH PAVEMENT MARKING AND SIGNAGE, CURBS,
25 LANDSCAPING, LANDSCAPED ISLANDS, AND/OR OTHER SIMILAR FEATURES. IN
26 ORDER TO DEFINE CIRCULATION AND PROVIDE BETTER SIGHT DISTANCE,
27 CURBED END ISLANDS SHALL BE REQUIRED AT THE END OF EACH ROW OF
28 PARKING SPACES. WHERE LOADING FACILITIES OR ON-SITE REFUSE
29 COLLECTION ARE PROVIDED, COMMERCIAL TRUCK CIRCULATION SHALL BE
30 CONSIDERED, AND TRUCK TURNING RADII SHALL BE SHOWN ON THE PARKING
31 FACILITY LAYOUT, CIRCULATION, AND DESIGN PLAN WHEN REQUIRED BY THE
32 TRAFFIC ENGINEER.]

33 [C. PARKING SPACES ALONG MAJOR SITE ENTRANCE DRIVES]

34 [THE PROVISION, LOCATION, DESIGN, AND DIMENSIONS OF PARKING SPACES ON
35 A MAJOR ACCESS DRIVEWAY THAT SERVES AS AN ENTRY OR EXIT FOR A LARGE
36 ESTABLISHMENT WITH MULTIPLE LOTS, TRACTS, OR BUSINESSES, SHALL
37 CONFORM TO MUNICIPAL STANDARDS FOR ON-STREET PARKING AND BE
38 SUBJECT TO REVIEW AND APPROVAL BY THE TRAFFIC ENGINEER.]

39

ANNOTATION FOR PAGE 21

Section 21.07.090H.9., Vehicular Access Location (New)

This page provides a new subsection H.9. that follows on H.8., *Vehicular Access and Circulation – General*. This section consolidates and clarifies municipal regulations governing the location of vehicle accessways from public streets onto property.

Line (s) #	Comment on Change
6-10	<p>H.9.a.: Provide a Reference to the Municipal and State Regulations that Govern the Location of Access Driveways. Clarify the intent to minimize the number of driveway curb cuts along roadways to the minimum necessary for safe and convenient site access. Clarify the primary access location requirements in the Municipal Driveway Standards.</p>
11-22	<p>H.9.b.: Move the residential alley access standard from the Title 21 Residential Design Standards into section H.9., and revise. Current Title 21 section 21.07.110F.4., which places limitations on front yard driveways where rear alley access is available to encourage residences to get driveway access from alleys, is deleted on page 41. The revised alley access standard in H.9.b. includes the following changes:</p> <ul style="list-style-type: none"> • Apply the alley access standard to the Urban Neighborhood Development Context Area mapped in 21.07.010 (pages 4-6), instead of to the Traditional Neighborhood Design areas designated on Map 2-1: Anchorage 2040 Land Use Plan Map, in the <i>Anchorage 2040 Land Use Plan</i>. The new map in 21.07.010 is easier for code users to access, and provides a more accurate outline of the older, urban neighborhoods for which this provision is intended. The proposed amendment carries forward the current code approach to not require alley access in outlying, suburban parts of town. Although alleys have been platted in a few places in suburban parts of the Bowl, such plat layouts are often inconsistent with the surrounding street network. • Subsection b.i. (line 15): Allow multifamily developments to have one 12-foot-wide driveway to the primary street frontage for every 50 feet of street frontage width, just as individual single-family residences on 50-foot-wide lots and two-family and townhouse dwelling units are allowed up to one driveway under the current Title 21. Allow one driveway per every 50 feet of frontage for all unit types, for consistency. • Subsection b.ii. (line 18): In addition to the current allowance for a 12-foot wide driveway on the primary street frontage, allow multiple-unit developments on corner lots to have a second front yard driveway, to the secondary street frontage. • Subsection b.iii. (line 21): Tighten the administrative relief provisions by replacing the existing provision (deleted on page 41, lines 18-26) with an Administrative Variance in 21.03.240J.7. (page 1). See page 1 for information about the substantive changes.
23-30	<p>H.9.c.: Relocate Existing Provisions for Cross-Access and Shared Access. Currently, shared access and access across lot lines is allowed thru a shared access agreement, as provided in 21.07.090H.8.h. at the end of the driveways section (page 27 lines 13-24). It is moved from that location to H.9.c. to be with other site access locational standards.</p>
31-34	<p>H.9.d.: Clarify Entitlements for Paired Driveways and Limit Total Width. Add a provision that clarifies driveways attached side by side across lot lines are allowed by right. A 28-foot maximum combined width is proposed to avoid combined driveway widths that exceed the holding capacity of snow clearing plows. The 28-foot standard is based on existing/past practices in Traffic Engineering administrative reviews of paired driveways.</p>

1 **21.07.090 Off-Street Parking and Loading**

2 *** **

3 **H. Parking and Loading Facility Design Standards**

4 *** **

5 **9. Vehicular Access and Parking Location**

6 **a. Street Access Location**

7 The number and spacing of driveways, including minimum distance from street
8 intersections, shall be as provided in the *Municipal Driveways Standards* and this section.
9 Access to streets owned by the state of Alaska requires compliance with state driveway
10 standards, department of transportation and public facilities approval and driveway permit.

11 **b. Alley Access Requirement**

12 Where a residential use is served by an alley in the Urban Neighborhood Development
13 Context Area (21.07.010E.), vehicle access shall take place from the alley, except for direct
14 vehicle access to the street that is limited to the following:

15 **i.** One driveway no wider than 12 feet at any point, except that lot frontages 100 feet
16 or wider on a primary street frontage may have one driveway per 50 feet of lot
17 frontage on that street.

18 **ii.** One additional driveway no wider than 12 feet providing access to the secondary
19 street frontage on corner lots for multifamily, townhouse, or two-family
20 developments.

21 **iii.** The director and traffic engineer may approve an administrative variance from the
22 requirements of this subsection, as provided in 21.03.240J.7.

23 **c. Cross-Access and Shared Access with Adjacent Sites**

24 Parking facilities serving a site, whether located on that same lot or on an adjacent lot, may
25 be connected by means of a common access driveway within or between the interior of
26 such lots. Where a property receives its access to the street through a shared driveway
27 with another lot, a shared access easement shall be provided on the plat, or a shared
28 access agreement running with the land shall be recorded by the municipality, as approved
29 and executed by the director, guaranteeing the continued availability of the shared access
30 between the properties.

31 **d. Paired Residential Driveways Allowed**

32 Driveways for two single-family, two-family, or townhouse units may be attached side-by-
33 side across a property line, provided the maximum combined width of the driveway is 28
34 feet and other standards of this section are met.

Section 21.07.090H.9., Vehicular Access and Parking Location (Continued)

Section 21.07.090H.10., Access to Parking Spaces (New)

Line (s) #	Comment on Change
5-11	<p>H.9.e., Garage Door Setback: Require Traffic Engineer review and approval where garage bay doors are proposed to be set back by less the length of a parking stall from public streets and sidewalks. Also require a “no parking” sign. Set a minimum setback of 4 feet unless the traffic engineer approves otherwise.</p> <p>This requirement is so that vehicles parked in the driveway in front of the garages will be less likely to hang out into the street or sidewalk. Vehicles overhanging into the public ROW interfere with pedestrians, traffic, and street maintenance and other service vehicles. The parking stall size requirements referenced by this new provision accommodate typical new vehicle dimensions. The minimum 4-foot setback clarifies the current practice of the Municipality to require garage doors to be set back four feet from streets and alleys for adequate sight distance.</p> <p><i>Discussion:</i> Previously, Title 21 required a minimum number of parking spaces per dwelling, which in most cases ensured that developments would provide an adequate-length parking space in the driveway in front of garages. The new standard in H.9.e. is proposed for consideration in response to the removal of Title 21 minimum parking requirements by A.O. 2022-80(S). It is intended to ensure there will be adequate space for parked vehicles in front of garage doors where residents and their guests are anticipated to park their cars even if the development site plan designate the driveway as a parking space.</p>
12-32	<p>H.10., Access to Parking Spaces:</p> <p>Collect existing parking space access standards from H.8. into a more focused subsection H.10. and provide more flexibility and non-discretionary approvals.</p> <p>Subsections H.10.a. and b. on lines 13-21 bring forward and clarify existing parking space access standards from 2.07.090H.8.e.i. deleted on page 24 lines 24-30.</p> <p>Subsection H.10.c. on lines 22-26 modifies the existing provisions regarding dead-end parking aisles by allowing non-discretionary (“by-right”) approval of dead-end parking aisles that are no more than 100 feet long. The provision being replaced is on page 27 lines 7-9.</p> <p>H.10.d.: Replace existing requirements for all vehicle turnarounds and back-out maneuvering to take place on site. This basic existing requirement (shown as deleted on page 26, lines 31-43) is carried forward with revised wording so that vehicles do not back out of parking spaces into the public street. The existing provision currently exempts single-family and two-family dwellings, and states that three- and four-unit multifamily structures may be exempted, subject to discretionary approval.</p> <p><i>(H.10.d. is continued on page 23, with further changes from current code...)</i></p>

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.090 Off-Street Parking and Loading**

2 *** **

3 **H. Parking and Loading Facility Design Standards**

4 *** **

5 **e. Garage Door Setbacks**

6 Street-facing garage bay doors that are proposed to be set back from the public right-of-
7 way by less than the length of a standard parking space (as defined in table 21.07-8,
8 Parking Angle, Stall, and Aisle Dimensions) shall be reviewed and approved by the traffic
9 engineer, unless there is a sign posted for “no parking.” Garages shall be set back from
10 public rights-of-way including alleys by at least four feet unless the traffic engineer
11 approves otherwise, to ensure adequate sight distance for turning and maneuvering.

12 **10. Access to Parking Spaces**

13 **a. Access to Parking Spaces**

14 Each parking space shall open to a parking aisle or driveway of such width and design as
15 provided in subsection 21.07.090H.12. to provide safe and efficient means of vehicular
16 access with no more than a standard two-movement entrance or exit from the parking
17 space and without having to move another vehicle. Stacked and tandem parking spaces
18 are allowed exceptions as provided in 21.07.090H.12.

19 **b. Backing Distances**

20 Adequate ingress and egress to each parking space shall be provided without backing
21 more than 25 feet.

22 **c. Dead-End Parking Aisles**

23 Any parking bay that does not provide two means of vehicle egress shall provide, at the
24 closed end, an extension of the parking aisle at least nine feet in depth, designated and
25 signed as a “no parking” turn-around area. Dead-end parking bays longer than 100 feet
26 shall be subject to discretionary review and approval by the traffic engineer.

27 **d. Vehicle Maneuvering**

28 Off-street parking facilities shall be designed so that all vehicle maneuvers occur on the
29 development site and not in the public right-of-way, and vehicles enter and exit the right-
30 of-way in a forward motion, except the following are exempt:

31 **i. Parking for single-family, two-family, and mobile home dwellings on individual lots,**
32 **accessing a local street.**

Section 21.07.090H.10., Access to Parking Spaces (Cont'd)

Line (s) #	Comment on Change
9-17	<p>H.10.d., Vehicle Maneuvering (cont'd)</p> <p>Subsection H.10.d.ii., beginning on line 9, exempts parking areas with only a few parking spaces serving multifamily, townhouse, and non-residential developments from the on-site maneuvering requirement if the parking area accesses only a local street and is no more than 20-feet wide. This proposed exemption would be by-right. An illustration is provided.</p> <p>For residential infill developments, this change gives 3- and 4-plex uses a similar entitlement as single-family and duplex dwellings have (see page 22, lines 31-32), for more consistent treatment of developments that have similar driveway and parking characteristics.</p> <p><i>Discussion:</i> This change responds to challenges in current Title 21 driveway and maneuvering requirements for 3- and 4-unit developments on urban lots. Urban lots that have zoning entitlement for multifamily use are often under-developed as duplexes. A review of other cities' zoning codes indicates that some waive or except 3- to 4-unit developments from the on-site maneuvering requirements.</p> <p>The provision also carries forward the existing <i>discretionary</i> approval for additional spaces (more than 4 spaces) without on-site turnarounds in multifamily developments with up to four dwelling units, that may be approved by the Traffic Engineer in appropriate circumstances such as lots on dead-end streets, cul-de-sacs, or other local streets with low traffic volumes.</p>
18-21	<p>Clarify regulations that allow using alleys for vehicle maneuvering aisles and simplify the approval process. The current regulations are shown as deleted on page 27, lines 10-12.</p> <p>The current regulations require discretionary approval by the Municipal Traffic Engineer. The proposed regulations would allow parking spaces to back out into an alley "by-right," without need for discretionary approval by the Traffic Engineer.</p> <p>In the current regulations (being deleted on page 27, lines 10-12) Traffic Engineer must explain to applicants that there needs to be a setback from the garage door face to the alley because motor vehicles entering and exiting a garage from the alley will need enough space to carry out the turning movement. Most alleys are 20 feet wide or less. The proposed regulations apply the minimum parking aisle widths from the Title 21 <i>Table 21.07-8: Parking Angle, Stall, and Aisle Dimensions</i> to enable by-right approvals without need for Traffic Engineer approval. For example, if the minimum parking aisle width is 24 feet, and the improved portion of the alley is only 18 feet wide, then the garage bay door will need to be set back at least 6 feet from the rear lot line abutting the alley (18 + 6 = 24).</p>

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.090 Off-Street Parking and Loading**

2 *** **

3 **H. Parking and Loading Facility Design Standards**

4 *** **

5 **10. Access to Parking Spaces**

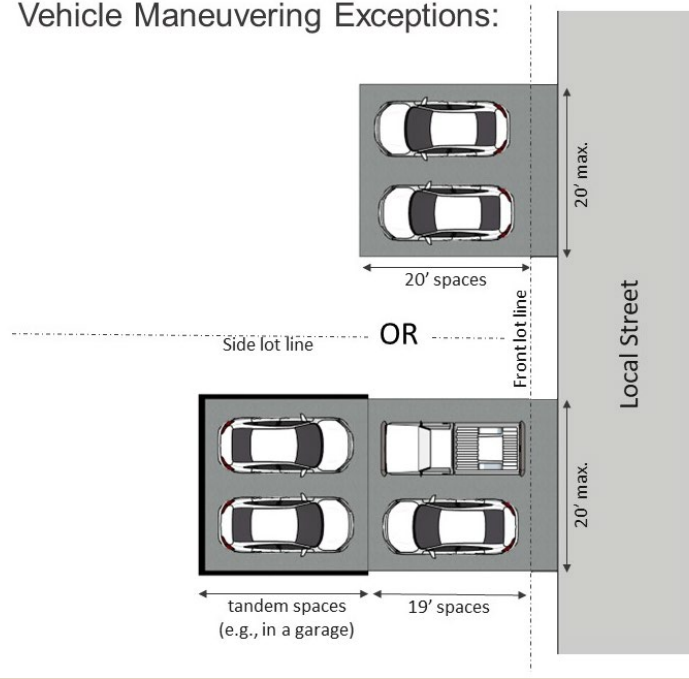
6 *** **

7 **d. Vehicle Maneuvering**

8 *** **

9 **ii.** For other developments, parking comprising only one or two parking spaces whose
10 only access is to a local street, provided that the vehicle area occupies no more
11 than 20 feet of the lot frontage in the front setback. The number of spaces may be
12 increased to four spaces if arranged in tandem for residential dwellings as provided
13 in 21.07.090K.12.k. Additional spaces for multifamily developments with up to four
14 dwelling units may be approved by the traffic engineer in appropriate
15 circumstances such as lots on dead-end streets, cul-de-sacs, or other local streets
16 with low traffic volumes.

Vehicle Maneuvering Exceptions:



17
18 **iii.** The usable portion of an alley may be credited as circulation and/or parking aisle
19 space for parking areas. The maneuvering area between the end of the parking
20 space and the opposite side of the improved alley shall meet the parking aisle
21 width in table 21.07-8, *Parking Angle, Stall, and Aisle Dimensions*.

17
18
19
20
21
22

Section 21.07.090H.11., Driveway Design and Dimensions (New)

This section consolidates and clarifies existing municipal regulations governing the width of driveway access from public streets onto property and provides by-right reductions in minimum driveway width.

<i>Line (s) #</i>	<i>Comment on Change</i>
6-16	<p>H.11.a., Parking Lot Entries/Driveway Approaches</p> <p>Carry forward existing regulations for driveway width within the public right-of-way, with clarifications. Provides more specific terminology and an illustration to clarify that the existing regulations for driveway width apply only to the portion of the driveway that is in the street right-of-way. Removes redundant references to State of Alaska regulations that are provided elsewhere, in subsection H.9.a.</p> <p>The suggested term, “Driveway Approach,” is also proposed to be defined on page 43.</p>
17-23	<p>H.11.b., Curb Openings and Public Walkway Crossings.</p> <p>Address grading of driveways as they cross sidewalks. This amendment responds to public feedback from urban neighborhood residents about unsafe/ unpleasant pedestrian experience of walking on sidewalks across driveway openings where the driveway is set at different grades from the sidewalk, and the importance of replacing sidewalks to as near as level running slope as possible when infill development occurs.</p>
24-33	<p>H.11.c., Driveway Approach (in ROW) as a Percentage of Lot Frontage Width</p> <p>Carry forward the current maximum allowed driveway width as a percentage of the lot frontage and consolidate redundant driveway width requirements for residential uses from 21.07.110F.3. (page 39 lines 6-30).</p> <p>The merged sections simplify the text and clarify that it applies to only the “driveway approach” portion of the driveway, meaning the portion of the driveway that is within the street right-of-way (ROW). No substantive changes are proposed to the standard as merged.</p>

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.090 Off-Street Parking and Loading**

2 *** **

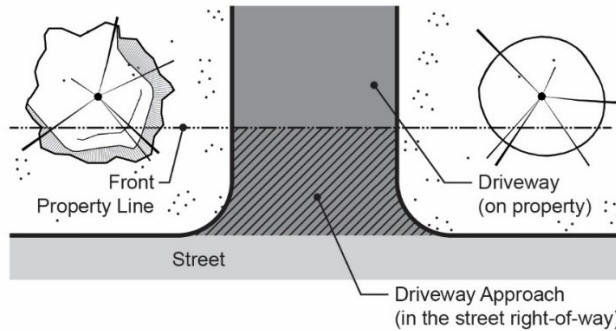
3 **H. Parking and Loading Facility Design Standards**

4 *** **

5 **11. Driveway Design and Dimensions**

6 **a[D]. Parking Lot Entries/Driveway Approaches**

7 Entries and driveway approaches providing access from the street edge to the front
8 property line [TO PARKING LOTS] shall conform to the municipal driveway standards
9 [CURRENTLY ESTABLISHED BY THE TRAFFIC ENGINEER] and this subsection
10 21.07.090H.11. Access to streets owned by the state of Alaska requires compliance with
11 state driveway standards as provided in 21.07.090H.9. [DEPARTMENT OF
12 TRANSPORTATION AND PUBLIC FACILITIES APPROVAL, AND A CURRENT VALID
13 STATE OF ALASKA DRIVEWAY PERMIT. INGRESS AND EGRESS TO PARKING
14 FACILITIES SHALL BE DESIGNED TO MAINTAIN ADEQUATE SIGHT DISTANCE AND
15 SAFETY AND AS PRESCRIBED IN THE MUNICIPAL DRIVEWAY STANDARDS.]



16

17 **b. Curb Openings and Public Walkway Crossings**

- 18 i. Curb cut and curb returns at driveway openings to the street shall be provided as
19 prescribed in the municipal driveway standards.
- 20 ii. Public walkways shall be maintained or restored to the maximum running slope
21 and cross-slope prescribed by M.A.S.S. and A.D.A., except that in the urban
22 neighborhood context areas (21.07.010E.), public walkways on local streets shall
23 be restored to a level running grade to the extent reasonably feasible.

24 **c. Driveway Approach (in ROW) as a Percentage of Lot Frontage Width**

25 [I. RESIDENTIAL USES]
26 [RESIDENTIAL DRIVEWAY ENTRANCES SHALL COMPLY WITH
27 SUBSECTION 21.07.110F.3., DRIVEWAY WIDTH.]

28 [II. NONRESIDENTIAL USES]
29 The total width of the driveway approach [ENTRANCES TO A NONRESIDENTIAL LOT]
30 from a street shall not exceed 40 percent of the frontage of the lot, or 33 percent of the
31 frontage if the platting authority or traffic engineer finds that conditions warrant it, provided
32 the following: [UNLESS THE APPLICANT PROVIDES FOR SNOW STORAGE IN A
33 MANNER APPROVED BY THE DECISION-MAKING BODY.]

Section 21.07.090H.11., Driveway Design and Dimensions (Cont'd.)

Line (s) #	Comment on Change
7-17	<p>H.11.c., Driveway Approach (cont'd):</p> <p>Subsections i. and ii.: Merge, simplify, and correct the residential driveway width exceptions i. and ii. from page 39 lines 31-38 that allow for driveways to be a minimum allowed width. In response to the removal of minimum off-street parking requirements by A.O. 2022-80(S), add “on-street parking” to the list of issues including snow storage that should be considered in granting an exception.</p> <p>Subsection iii.: Move the flag lot exception from page 39 lines 39-41 to page 25, lines 16-18. No changes are proposed.</p>
18-39	<p>11.d., Minimum Driveway Width: Clarify the minimum driveway width requirements and offer by-right driveway width exceptions, relative to the Municipal Driveway Standards, parking aisle widths, and internal driveways on site.</p> <p>Subsection 11.d. will provide specific, by-right exceptions from the minimum required width of driveways set forth in the Municipal Driveway Standards and provide minimum widths within Title 21 that can be referenced by the other Title 21 driveway standards in subsections H.8. through H.11.</p> <p>Subsection i. (line 22): Clarifies that the minimum width of parking aisles is set forth in a different section (H.12.) and does not apply to other driveways.</p> <p>Subsection ii. (lines 23-24): Clarifies that the minimum required width of driveway approaches within the ROW is provided in the Municipal Driveway Standards, except as stated otherwise in this section.</p> <p>Subsection iii. (lines 25-27): Clarifies the minimum width of a driveway for a single dwelling is 12 feet, regardless of housing land use type. Supports allowing 12-foot-wide front driveways for dwellings with rear alley access in H.9.b.i. (page 21 line 15).</p> <p>Subsection iv. (lines 25-29): Allows multifamily developments to have one-lane-wide two-way driveways to smaller parking facilities that receive access from local streets. This cuts in half the minimum driveway width for small multifamily developments of 3 to approximately 6 units, because such developments do not produce enough traffic to need 24-foot-wide driveways. The proposed minimum width of the one-way driveway is 10-feet on straight segments and 12-feet on curving segments and in the ROW driveway approach. The analytical basis for one-lane driveways is provided on pages 61-64 in “Attachment 6.3: Background Research” of the 2022 PZC Staff Issue-Response Packet from PZC Case 2022-0026, which was Exhibit D for the draft A.O. 2022-80.</p> <p>Subsection v. (lines 30-32): Provides for greater flexibility and clarity, by allowing one-way driveways serving multifamily uses to be 12-feet wide, by-right.</p> <p>Subsection vi. (lines 35-39): Provides for administrative exceptions and flexibility, and a reference to other applicable codes.</p>

1 **21.07.090 Off-Street Parking and Loading**

2 *** **

3 **H. Parking and Loading Facility Design Standards**

4 *** **

5 **11. Driveway Design and Dimensions**

6 *** **

7 **c. Driveway Approach (in ROW) as a Percentage of Lot Frontage Width**

8 *** **

9 **i.** The driveway approach is always allowed to have the minimum driveway width
10 provided by subsection d. The traffic engineer may approve more than the
11 minimum driveway width, provided the traffic engineer determines that snow
12 storage, on-street parking, traffic flow and safety, and the neighborhood context
13 are addressed.

14 **ii.** The driveway approach shall not exceed the maximum driveway width established
15 in the municipal driveway standards.

16 **iii.** Flag lots are exempt from the percentage limitations but shall have a maximum
17 driveway approach width of 20 feet. Abutting flag lots may share a driveway
18 approach up to 24 feet wide (12 feet per lot).

19 **d. Minimum Driveway Width**

20 The minimum required width of driveways including the driveway approach within the street
21 ROW and the portion of the driveway on the development property is as follows:

22 **i.** The minimum width of parking aisles shall be as set forth in 21.07.090H.12.

23 **ii.** The minimum width of the driveway approach shall be as set forth in the municipal
24 driveway standards, except as modified or clarified by subsections iii. through vi.

25 **iii.** The minimum width of a driveway providing access to a single-family, two-family,
26 townhouse, mobile home, or other individual dwelling from a local street or alley is
27 12 feet.

28 **iv.** The minimum width of a driveway providing access for up to 10 parking spaces
29 servicing a townhouse or multifamily residential use from a local street or alley is 12
30 feet, except that straight (non-curving) driveway segments on the development
31 property (i.e., not in the driveway approach) may be 10 feet.

32 **v.** The minimum width of a driveway providing access for more than 10 parking
33 spaces servicing a townhouse or multifamily use from a local street or alley is 12
34 feet for one-way traffic and 20 feet for two-way traffic.

35 **vi.** The traffic engineer may reduce the driveway width to relieve hardship on small or
36 narrow lots or increase the required width to meet vehicle turning and maneuvering
37 needs, in accordance with accepted principles of traffic engineering and safety as
38 determined by the traffic engineer. Minimum driveway widths are also subject to
39 other applicable codes, including the Fire Code.

ANNOTATION FOR PAGE 26

Section 21.07.090H.11., Driveway Design and Dimensions (*Continued*)

<i>Line (s) #</i>	<i>Comment on Change</i>
7-19	<p>Clarify existing regulations for driveway design and dimensions for driveway throat length, sight distance triangles, and circulation definition through pavement marking and end islands. The content on lines 14-19 is moved from subsection 21.07.090H.8.b (page 20 lines 23-28).</p> <p>For reference, the Title 21 driveway throat length definition is provided on page 44.</p>
20-43	<p>Delete existing driveway and circulation regulations being relocated or replaced:</p> <ul style="list-style-type: none"> • Subsection [E] on lines 20-23 is replaced by subsection 10.d. on page 22. • Subsection [I.] on lines 24-30 is replaced by subsections 10.a. and 10.b. on page 22. • Subsection [II.] on lines 31-35 is replaced by 10.d. on page 22. • Subsection [III.] on lines 36-43 is replaced by 10.d.i. and ii. starting on page 22 line 29 through page 23 line 17.

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.090 Off-Street Parking and Loading**

2 *** **

3 **H. Parking and Loading Facility Design Standards**

4 *** **

5 **11. Driveway Design and Dimensions**

6 *** **

7 **e. Driveway Throat Length**

8 The driveway throat shall be of sufficient length to enable the street and walkways in the
9 ROW and the circulation, parking, and walkways in the development site to function without
10 interfering with each other, as provided in the municipal driveway standards.

11 **f. Sight Distance**

12 Ingress and egress to parking facilities shall be designed as prescribed in the municipal
13 driveway standards to maintain adequate sight distance and safety.

14 **g. Circulation Definition**

15 Circulation patterns within parking facilities shall be well defined with pavement marking
16 and signage, curbs, landscaping, and/or other similar features. Curbed end islands shall
17 be provided at the end of each row of parking spaces to define circulation and provide sight
18 distance at internal intersections of parking aisles, driveways, and/or on-site roadways.
19 Shared parking courtyards meeting 21.07.060G.20. may depart from this requirement.

20 **[E. PARKING AND MANEUVERING]**

21 [ALL CIRCULATION AISLES, DRIVEWAYS, AND VEHICLE MANEUVERING AREAS
22 REQUIRED BY THIS SECTION SHALL BE LOCATED ENTIRELY OFF-STREET AND ON THE
23 PROPERTY UNLESS SPECIFICALLY PROVIDED OTHERWISE BY THIS SECTION.]

24 **[I. ACCESS TO PARKING SPACES]**

25 [TO ENSURE SAFE AND EFFICIENT VEHICULAR ACCESS TO PARKING SPACES,
26 EACH OFF-STREET PARKING SPACE SHALL OPEN DIRECTLY ON A PARKING
27 AISLE OR DRIVEWAY OF SUCH WIDTH AND DESIGN AS PROVIDED IN TABLE
28 21.07-7 AND THE ILLUSTRATIONS THAT FOLLOW THE TABLE. ADEQUATE
29 INGRESS AND EGRESS TO EACH PARKING SPACE SHALL BE PROVIDED
30 WITHOUT BACKING MORE THAN 25 FEET.]

31 **[II. MANEUVERING AREA]**

32 [OFF-STREET PARKING FACILITIES SHALL BE DESIGNED WITH SUFFICIENT
33 MANEUVERING ROOM SO THAT ALL MANEUVERS ASSOCIATED WITH THE
34 PARKING SHALL OCCUR IN THE OFF-STREET PARKING FACILITY, AND THAT
35 ALL VEHICLES ENTER THE ABUTTING STREET IN A FORWARD MOTION.]

36 **[III. SOME DWELLINGS EXEMPTED]**

37 [SINGLE-FAMILY, TWO-FAMILY, TOWNHOUSE, AND MOBILE HOME DWELLINGS
38 ON INDIVIDUAL LOTS SHALL BE EXEMPTED FROM THIS SUBSECTION.
39 MULTIFAMILY DWELLINGS WITH UP TO FOUR UNITS SHALL BE EXEMPTED
40 FROM THIS SUBSECTION IN APPROPRIATE CIRCUMSTANCES IF APPROVED
41 BY THE TRAFFIC ENGINEER. APPROPRIATE CIRCUMSTANCES MAY INCLUDE
42 LOTS WITH ALLEY ACCESS, LOTS LOCATED ON LOW-VOLUME STREETS, AND
43 LOTS LOCATED ON DEAD-END STREETS OR CUL-DE-SACS.]

ANNOTATION FOR PAGE 27

Section 21.07.090H.11., Driveway Design and Dimensions (Cont'd.)

<i>Line (s) #</i>	<i>Comment on Change</i>
7-24	Delete existing driveway and circulation regulations being relocated or replaced: <ul style="list-style-type: none"><li data-bbox="321 438 1097 472">• Subsection [F.] on lines 7-9 is replaced by 10.c. on page 22.<li data-bbox="321 510 1166 543">• Subsection [G.] on lines 10-12 is replaced by 10.d.iii. on page 23.<li data-bbox="321 579 1114 613">• Subsection [H] on lines 13-24 is replaced by 9.c. on page 21.

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.090 Off-Street Parking and Loading**

2 *** **

3 **H. Parking and Loading Facility Design Standards**

4 *** **

5 **11. Driveway Design and Dimensions**

6 *** **

7 [F. DEAD-END PARKING AISLES]

8 [DEAD-END PARKING AISLES MAY BE ALLOWED ONLY WITH THE APPROVAL OF THE
9 TRAFFIC ENGINEER.]

10 [G. ALLEYS]

11 [SUBJECT TO SAFETY APPROVAL BY THE TRAFFIC ENGINEER, THE USABLE PORTION
12 OF AN ALLEY MAY BE CREDITED AS CIRCULATION AND/OR PARKING AISLE SPACE.]

13 [H. CROSS ACCESS AND JOINT ACCESS WITH ADJACENT SITES]

14 [THE PLAN SHALL SHOW EXISTING PARKING AND CIRCULATION PATTERNS ON
15 ADJACENT PROPERTIES AND POTENTIAL CONNECTIONS. REQUIRED PARKING LOTS
16 SERVING A SITE, WHETHER LOCATED ON THAT SAME LOT OR ON AN ADJACENT LOT,
17 MAY BE CONNECTED BY MEANS OF A COMMON ACCESS DRIVEWAY WITHIN OR
18 BETWEEN THE INTERIOR OF SUCH LOTS. APPLICANTS ARE ENCOURAGED TO
19 PROVIDE SHARED VEHICLE AND PEDESTRIAN ACCESS TO ADJACENT PROPERTIES
20 FOR CONVENIENCE, SAFETY, AND EFFICIENT CIRCULATION. AN ACCESS EASEMENT
21 SHALL BE PROVIDED ON THE PLAT, OR A SHARED ACCESS AGREEMENT RUNNING
22 WITH THE LAND SHALL BE RECORDED BY THE MUNICIPALITY, AS APPROVED AND
23 EXECUTED BY THE DIRECTOR, GUARANTEEING THE CONTINUED AVAILABILITY OF
24 THE SHARED ACCESS BETWEEN THE PROPERTIES.]

ANNOTATION FOR PAGE 28

Section 21.07.090H.12., Dimensions of Parking Spaces and Aisles (re-numbered from H.9.)

Current Subsection 21.07.090H.9. provides the Title 21 dimensional standards for parking space sizes and circulation aisle widths.

The subsection is re-numbered to H.12. Its allowance for smaller parking spaces in urban neighborhoods is edited to refer to the Urban Neighborhood Development Context Area created in subsection 21.07.010E. (page 3) instead of a similar area mapped in the *Anchorage 2040 Land Use Plan*.

The parking space dimensional standards were revised by A.O. 2022-80(S). No substantive changes to the parking space dimensional standards are proposed by the Title 21 Site Access amendments.

<i>Line (s) #</i>	<i>Comment on Change</i>
7	Make a technical correction by deleting a reference to “required” parking. The term “required” parking was holdover text from when parking spaces were required before the adoption of A.O. 2022-80(S).
10-13	Simplify and update a map reference for the entitlement to smaller parking spaces in older, urban neighborhoods. Apply the entitlement allowing SMALL parking space dimensions to the Urban Neighborhood Development Context Area mapped in 21.07.010 (pages 4-6), instead of to the Traditional Neighborhood Design areas designated on Map 2-1: Anchorage 2040 Land Use Plan Map, in the <i>Anchorage 2040 Land Use Plan</i> . The new map in 21.07.010 is more easily accessible to code users, and provides a more accurate outline of the older, urban neighborhoods.
Table 21.07-8, footnote 1	Simplify and update a map reference for the entitlement to reduce the length of parking stalls older, urban neighborhoods. Apply the entitlement allowing 19-foot-long STANDARD parking spaces to the Urban Neighborhood Development Context Area mapped in 21.07.010 (pages 4-6), instead of to the Traditional Neighborhood Design areas designated on Map 2-1: Anchorage 2040 Land Use Plan Map, in the <i>Anchorage 2040 Land Use Plan</i> .

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.090 Off-Street Parking and Loading**

2 *** **

3 **H. Parking and Loading Facility Design Standards**

4 *** **

5 12[9]. **Dimensions of Parking Spaces and Aisles**

6 *** **

7 a. **Minimum Dimensions for [REQUIRED] Parking**

8 Parking shall meet or exceed the STANDARD parking space and aisle dimensions set forth
9 in table 21.07-8[6], except as follows:

- 10 i. Parking facilities in the Downtown (DT) zoning districts, and the Urban
11 Neighborhood Contexts delineated in subsection 21.07.010E. [TRADITIONAL
12 NEIGHBORHOOD DESIGN AREAS DESIGNATED ON MAP 2-1: ANCHORAGE
13 2040 LAND USE PLAN MAP IN THE ANCHORAGE 2040 LAND USE PLAN,] may
14 meet the SMALL parking space and aisle dimensions in table 21.07-8[6]. Up to 30
15 percent of parking in the Downtown (DT) zoning districts may meet the COMPACT
16 parking dimensions, if signed for compact vehicles only.

17 *** **

TABLE 21.07-8[6] PARKING ANGLE, STALL, AND AISLE DIMENSIONS

Parking Angle (Degrees)	Parking Space Type	Space Width	Curb Length (Width Projection)	Space Depth (Vehicle Projection)	Aisle Width 1-way	Aisle Width 2-way	Typical Parking Bay Width (Module)	Interlock Reduction	Overhang Allowance
***	***	***							
¹ Developments in the <u>Urban Neighborhood Development Context Areas delineated in 21.07.010E.</u> [TRADITIONAL NEIGHBORHOOD DESIGN AREAS DESIGNATED ON MAP 2-1: ANCHORAGE 2040 LAND USE PLAN MAP IN THE ANCHORAGE 2040 LAND USE PLAN] may use the following alternative STANDARD parking space depth (Vehicle Projection): 19' 0" for 90-degree parking angle; 20' 8" for 75-degree parking angle; 20' 11" for 60-degree parking angle, and 19' 6" for 45-degree parking angle. Parking bay width may decrease as a result.									

18 *** **

19 *** **

ANNOTATION FOR PAGE 29

Section 21.07.110, Residential Design Standards

The Residential Design Standards establish a variety of development standards specific to household residential developments and building types, including single-family, two-family, townhouse, multifamily, and mixed-use dwellings.

The amendments on pages 29 through 38 delete a series of residential design standards for pedestrian access and pedestrian-oriented/street-facing building frontages. The standards being removed are proposed to be merged into a more streamlined set of Pedestrian Frontage Standards in proposed new section 21.07.060F. on pages 9-11. Pedestrian-oriented frontage and walkway requirements from other parts of Title 21 (such as Section 21.04.030 on pages 2 and 3) are also consolidated into 21.07.060F.

The deletions on the next page are to purpose statements related to pedestrian accessibility. They are proposed to be replaced by the objectives for the new Pedestrian Frontage Standard section on page 9, lines 10-16.

<i>Line (s) #</i>	<i>Comment on Change</i>
4-10	Delete and merge these general objectives from the residential design standards into the into section 21.07.060F. on page 9 lines 10-16.
15-22	Delete and merge these objectives from the multifamily/townhouse design standards into the into section 21.07.060F. on page 15 lines 11-20.

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.110 Residential Design Standards**

2 **A. Purpose**

3 *** **

4 [4. ENHANCE PUBLIC SAFETY BY PROMOTING LINES OF SIGHT TO RESIDENTIAL ENTRIES,
5 CLEARLY DEFINING TRANSITIONS FROM PUBLIC TO MORE PRIVATE RESIDENTIAL
6 OUTDOOR SPACES, AND DESIGNING ENTRANCES TO BE PROMINENT.]

7 [5. LOCATE ACTIVE LIVING SPACES, ENTRANCES, AND WINDOWS TO IMPROVE THE
8 PHYSICAL AND VISUAL CONNECTION FROM RESIDENCES TO THE STREET, AND FOSTER
9 OPPORTUNITIES FOR CASUAL SURVEILLANCE OF THE STREET AND OUTWARDLY
10 EXPRESSED PROPRIETORSHIP OF THE NEIGHBORHOOD.]

11 *** **

12 **C. Standards for Multifamily and Townhouse Residential**

13 **1. Purpose**

14 *** **

15 [C. PROMOTE BUILDING PLACEMENT AND ORIENTATION THAT INTERFACES WITH THE
16 NEIGHBORHOOD STREET AND ENGAGES THE PEDESTRIAN, TO CONTRIBUTE TO PUBLIC
17 SAFETY, ATTRACTIVE STREET FRONTAGES, PEDESTRIAN ACCESS, AND A SENSE OF
18 NEIGHBORHOOD AND COMMUNITY.]

19 *** **

20 [G. PROMOTE A SAFE LIVING ENVIRONMENT THROUGH “EYES ON THE STREET” DESIGN
21 ELEMENTS, SUCH AS PLACEMENT OF WINDOWS, AND TRANSITION SPACES BETWEEN
22 THE PUBLIC REALM AND THE MORE PRIVATE AREAS OF THE DEVELOPMENT.]

23 *** **

24

25

ANNOTATION FOR PAGE 30

Section 21.07.110, Residential Design Standards (Continued)

Subsection 21.07.110C.3.: Standards for Multifamily and Townhouse Residential, Windows Facing the Street.

The requirements for street-facing windows are replaced by a reference to the new Pedestrian Frontage Standard section.

<i>Line (s) #</i>	<i>Comment on Change</i>
5-16	Consolidate multifamily and townhouse street-facing window requirements into Section 21.07.060F., Pedestrian Frontage Standards (Tables 21.07-2 and 21.07-3 on pages 10 and 11).
17-27	Move the rules of measurement for window and wall area to Section 21.15.020, Rules of Interpretation. See page 42.

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.110 Residential Design Standards**

2 *** **

3 **C. Standards for Multifamily and Townhouse Residential**

4 *** **

5 **3. Pedestrian-Oriented Street Frontage Standards [WINDOWS FACING THE STREET]**

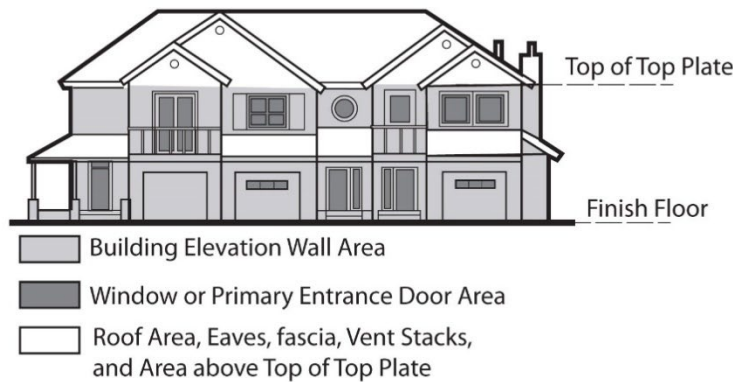
6 Provisions for site planning and building orientation in relationship to street frontages and
7 pedestrian access are provided in subsection 21.07.060F., *Pedestrian Frontage Standard*.

8 **[A. WINDOWS]**

9 [WINDOWS OR PRIMARY ENTRANCE DOORS SHALL BE PROVIDED ON EACH
10 BUILDING ELEVATION FACING A STREET OR THAT HAS PRIMARY ENTRANCES TO
11 DWELLINGS (UP TO A MAXIMUM OF TWO ELEVATIONS). AT LEAST 10 PERCENT
12 OF THE WALL AREA OF THE BUILDING ELEVATION SHALL BE WINDOWS OR
13 PRIMARY ENTRANCE DOORS. THE DIRECTOR MAY ELIMINATE OR REDUCE THE
14 REQUIREMENT ON ONE ELEVATION (IN INSTANCE WHERE UP TO TWO APPLY)
15 BASED ON SITE SPECIFIC CIRCUMSTANCES (SUCH AS INFILL PROJECTS WHERE
16 AN ENTRY FACES AN ADJACENT PROPERTY RATHER THAN THE STREET).]

17 **[B. WALL AREA CALCULATION]**

18 [THE AREA OF VERTICAL WALL SURFACE MEASURED FROM FINISHED FLOOR OF
19 EACH LEVEL, INCLUDING GARAGES TO TOP OF TOP PLATE. IN THE CASE OF A
20 BASEMENT WALL, CALCULATE THE AREA OF VERTICAL WALL SURFACE
21 MEASURED FROM FINISHED GRADE TO THE TOP OF THE FINISHED FLOOR ABOVE
22 OR TOP OF TOP PLATE, WHICHEVER IS APPLICABLE. ANY WALL AREA ABOVE THE
23 TOP PLATE SHALL NOT APPLY, INCLUDING ANY GABLE ENDS. THE FAÇADE WALL
24 AREA OF STORIES BELOW GRADE PLANE, SUCH AS BELOW GRADE PARKING,
25 ARE EXEMPT FROM THE MEASUREMENT OF WALL AREA. THE ROOF, INCLUDING
26 EAVES, FASCIA, AND VENT STACKS, IS ALSO EXEMPT, AS ILLUSTRATED BELOW.]



27

28 *** **

29

ANNOTATION FOR PAGE 31

Section 21.07.110, Residential Design Standards (Continued)

Section 21.07.110C., Residential Design Standards, Standards for Multifamily and Townhouse Residential (Continued)

<i>Line (s) #</i>	<i>Comment on Change</i>
7-20	<p>21.07.110C.3., Windows Facing the Street</p> <p>Delete and consolidate multifamily and townhouse street-facing window requirements into Section 21.07.060F., Pedestrian Frontage Standards.</p> <p>The energy efficiency exception is provided and revised on page 9, lines 31-35, in subsection 21.07.060F.3.b.</p>
22-32	<p>21.07.110C.5., Pedestrian Access</p> <p>Consolidate the multifamily and townhouse pedestrian access requirement into Section 21.07.060E.4.b., On-Site Pedestrian Walkways. See page 8, lines 18-20.</p> <p>This is a part of consolidating redundant residential walkway requirements into one place, in the generally applicable on-site pedestrian walkway requirements in Title 21. Helps eliminate redundancies and inconsistencies in language.</p>

1 **21.07.110 Residential Design Standards**

2 *** **

3 **C. Standards for Multifamily and Townhouse Residential**

4 *** **

5 **3. Pedestrian-Oriented Street Frontage Standards [WINDOWS FACING THE STREET]**

6 *** **

7 [C. ENERGY EFFICIENCY EXCEPTION]
8 [A REDUCTION IN REQUIRED WINDOW AREA IS PERMITTED IF DEMONSTRATED
9 BY CALCULATION BY AN ENERGY RATER CERTIFIED BY THE STATE OF ALASKA
10 THAT INSTALLING THE REQUIRED WINDOW AREA WILL REDUCE THE ENERGY
11 RATING BELOW A 5-STAR ENERGY RATING.]

12 [D. ADDITIONS AND RENOVATIONS]
13 [ONLY THE WALL AREA AFFECTED BY EITHER AN ADDITION OR A RENOVATION
14 SHALL BE REQUIRED TO COMPLY WITH THE OPENING REQUIREMENTS.
15 UNAFFECTED WALL AREAS NEED NOT COMPLY. GARAGE ADDITIONS ARE
16 EXEMPT FROM OPENING REQUIREMENTS. THE DIRECTOR MAY ELIMINATE OR
17 REDUCE THE REQUIREMENT FOR WINDOW OPENINGS ON
18 ADDITIONS/RENOVATIONS BASED ON PROJECT SPECIFIC CIRCUMSTANCES
19 (SUCH AS THE NATURE OF THE SPACES BEING ADDED, BUILDING ORIENTATION,
20 OR STRUCTURAL REQUIREMENTS).]

21 *** **

22 [5. PEDESTRIAN ACCESS]
23 [WALKWAY CONNECTIONS FROM PRIMARY FRONT ENTRANCES TO THE STREET ARE
24 REQUIRED IN ACCORDANCE WITH SUBSECTION 21.07.060E.4., EXCEPT THAT
25 DEVELOPMENTS MAY PROVIDE ONE OF THE FOLLOWING ALTERNATIVES INSTEAD:]

26 [A. [PRIMARY FRONT ENTRANCES FOR INDIVIDUAL DWELLINGS MAY CONNECT TO
27 THE STREET BY THE DWELLING UNIT'S INDIVIDUAL DRIVEWAY IF SUCH IS
28 PROVIDED;]

29 [B. A PARKING COURTYARD MAY BE PROVIDED IN CONFORMANCE WITH
30 SUBSECTION 21.07.060F.18., OR]

31 [C. OTHER METHODS, AS APPROVED BY THE DIRECTOR, THAT PROVIDE SAFE,
32 CONVENIENT, AND ADEQUATE PEDESTRIAN ACCESS.]

33

34

ANNOTATION FOR PAGE 32

Section 21.07.110, Residential Design Standards (Continued)

Section 21.07.110C.6., Residential Design Standards, Standards for Multifamily and Townhouse Residential – Building and Site Orientation Menu

<i>Line (s) #</i>	<i>Comment on Change</i>
5-34	<p>Section 21.07.110C.6., Building and Site Orientation Menu</p> <p>Delete the building and site orientation menu requirements from the multifamily residential design standards and consolidate elements of these requirements in a streamlined form into the proposed Pedestrian Frontage Standards in 21.07.060F., Tables 21.07-2 and 21.07-3 (pages 10 and 11). The tables refer to the Pedestrian Amenities Menu options in 21.07.060G. (starting on page 12).</p> <p>Move elements of menu choices b, c., and d. from the deleted building orientation menu to the section 21.07.060G., <i>Pedestrian Amenities Menu</i>, so that they may remain available to earn credit toward Title 21 requirements and incentives.</p>

1 **21.07.110 Residential Design Standards**

2 *** **

3 **C. Standards for Multifamily and Townhouse Residential**

4 *** **

5 [6. BUILDING AND SITE ORIENTATION MENU]

6 [BUILDINGS SHALL BE ORIENTED TO SURROUNDING STREETS, SIDEWALKS, COMMON
7 PRIVATE OPEN SPACES, AND THE NEIGHBORHOOD PUBLIC REALM THROUGH AT LEAST
8 THREE OF THE FOLLOWING MENU CHOICES. THE DIRECTOR MAY REDUCE THE
9 REQUIREMENT TO TWO MENU CHOICES IF HE OR SHE DETERMINES THAT THE PRIMARY
10 OBJECTIVE OF THE MENU OPTION IS ACHIEVED OR THAT THE SPECIFIC SITE LIMITS
11 AVAILABILITY OF A MAJORITY OF THE OPTIONS.]

12 [A. COURTYARD HOUSING]

13 [ARRANGE OR CONFIGURE THE BUILDING(S) TO ENCLOSE AND FRAME A
14 HOUSING COURTYARD AS DESCRIBED IN SUBSECTION 21.07.060F.7.]

15 [B. ORIENTATION OF LIVING SPACES AND WINDOWS—GROUND FLOOR]

16 [PROVIDE WINDOWS AND/OR ENTRANCES TO HABITABLE LIVING SPACES
17 COMPRISING, AT LEAST 10 PERCENT OF THE GROUND-FLOOR WALL AREA OF UP
18 TO TWO BUILDING ELEVATIONS FACING A STREET OR HAVING A PRIMARY FRONT
19 ENTRANCE. WINDOWS IN A GARAGE DOOR DO NOT COUNT TOWARDS THE
20 MINIMUM AREA IN THIS SECTION.]

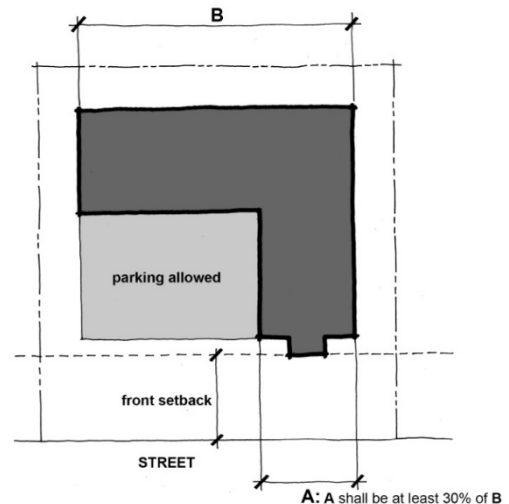
21 [C. ORIENTATION OF LIVING SPACES AND WINDOWS—OVERALL DEVELOPMENT]

22 [PROVIDE WINDOWS AND/OR ENTRANCES TO HABITABLE LIVING SPACES,
23 COMPRISING AT LEAST 10 PERCENT OF THE TOTAL WALL AREA OF UP TO TWO
24 BUILDING ELEVATIONS FACING A STREET OR HAVING A PRIMARY FRONT
25 ENTRANCE. WINDOWS IN A GARAGE DOOR DO NOT COUNT TOWARDS THE
26 MINIMUM AREA OF THIS SECTION.]

27 [D. ORIENTATION OF LIVING SPACES AND WINDOWS—ADDITIONAL
28 TRANSPARENCY]

29 [INCREASE THE PERCENTAGE OF THE WALL AREA COMPRISED OF WINDOWS
30 AND/OR PEDESTRIAN ENTRANCES TO 20 PERCENT IN SUBSECTIONS 6.B. AND/OR
31 6.C. ABOVE.]

32 [E. STREET FRONTAGE – PARKING
33 BESIDE OR BEHIND THE BUILDING]
34 [NO MORE THAN 70 PERCENT OF A
35 STREET-FACING BUILDING
36 ELEVATION SHALL HAVE PARKING
37 FACILITIES (INCLUDING GARAGES)
38 BETWEEN IT AND THE STREET.]



Section 21.07.110, Residential Design Standards (Continued)

Section 21.07.110C.6., Residential Design Standards, Standards for Multifamily and Townhouse Residential – Building and Site Orientation Menu (Continued)

<i>Line (s) #</i>	<i>Comment on Change</i>
5-34	<p>Section 21.07.110C.6. Building and Site Orientation Menu (cont'd)</p> <p>Delete the building and site orientation menu requirements from the multifamily residential design standards and consolidate elements of these requirements in a streamlined form into the proposed Pedestrian Frontage Standards in 21.07.060F., Tables 21.07-2 and 21.07-3 (pages 10 and 11).</p> <p>Move menu choice j., <i>Separated Walkway from the Street</i> (lines 31-34) from the deleted building orientation menu to section 21.07.060G., Pedestrian Amenities Menu (page 14, lines 7-11) so that it may continue to be available as an option applicants may choose to earn credit toward Title 21 requirements and bonuses.</p>

CODE LANGUAGE to be added is underlined>. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.110 Residential Design Standards**

2 *** **

3 **C. Standards for Multifamily and Townhouse Residential**

4 *** **

5 [6. BUILDING AND SITE ORIENTATION MENU]

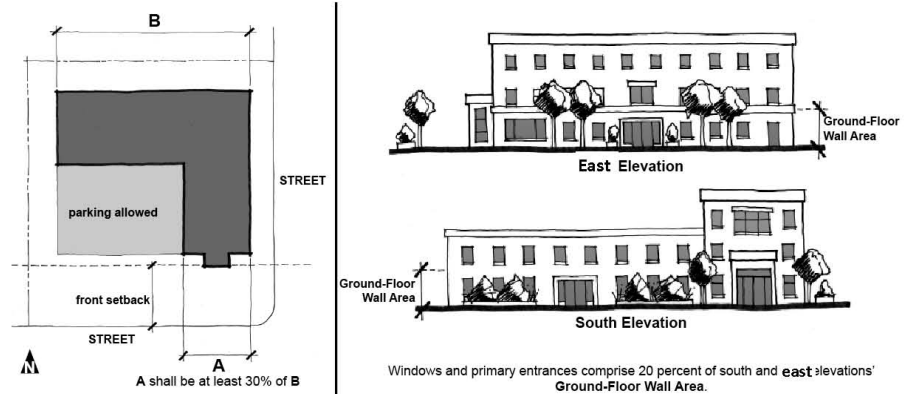
6 *** **

7 [F. STREET FRONTAGE – LIMITED PARKING WIDTH]

8 AS AN ALTERNATIVE TO 6.E. ABOVE, LIMIT DRIVEWAYS AND PARKING FACILITIES
9 TO NO MORE THAN 50 PERCENT OF THE TOTAL SITE AREA BETWEEN THE
10 BUILDING AND A STREET, AND GARAGES TO NO MORE THAN 50 PERCENT OF THE
11 STREET FACING BUILDING ELEVATION.

12 [G. STREET CORNER BUILDING]

13 [ON A CORNER LOT, PROVIDE CHOICES 6.E. AND 6.F. ON BOTH STREET
14 FRONTAGES, SUCH THAT THE BUILDING (INCLUDING ITS HABITABLE FLOOR
15 AREA WITH WINDOWS) IS PLACED NEAREST THE CORNER, AND ANY PARKING
16 FACILITIES ARE LOCATED BESIDE OR BEHIND THE BUILDING AWAY FROM THE
17 STREET CORNER. COMPLIANCE WITH THIS FEATURE COUNTS FOR MEETING
18 TWO.]



19 [H. LIMITED FRONT-FACING GARAGE WIDTH FOR TOWNHOUSES]

20 [WHERE GARAGES FOR INDIVIDUAL TOWNHOUSE-STYLE DWELLING UNITS FACE
21 THE STREET OR ARE ON THE SAME BUILDING ELEVATION AS THE PRIMARY
22 FRONT ENTRY TO THE DWELLINGS, LIMIT THE GARAGE DOOR WIDTH TO NO
23 MORE THAN 50 PERCENT OF THE WIDTH OF EACH DWELLING, OR UP TO 67
24 PERCENT PROVIDED THE BUILDING FAÇADE ACHIEVES ONE MORE FEATURE
25 THAN REQUIRED IN SUBSECTION C.7., BUILDING ARTICULATION MENU.]

26 [I. ENHANCED SIDEWALK]

27 PROVIDE AN ENHANCED SIDEWALK CONFORMING TO SUBSECTION
28 21.07.060F.17. ON AT LEAST ONE STREET FRONTAGE THAT IS NOT LESS THAN
29 100 LINEAR FEET.
30

31 [J. SEPARATED WALKWAY TO THE STREET]

32 [CONNECT ALL PRIMARY ENTRANCES TO THE STREET BY A CLEAR AND DIRECT
33 WALKWAY SEPARATED FROM AND NOT ROUTED THROUGH A PARKING
34 FACILITY.]

ANNOTATION FOR PAGE 34

Section 21.07.110, Residential Design Standards (Continued)

Section 21.07.110C.6., Standards for Multifamily and Townhouse Residential – Building and Site Orientation Menu (Continued)

<i>Line (s) #</i>	<i>Comment on Change</i>
7-41	<p>Section 21.07.110C.6., Building and Site Orientation Menu (Cont'd)</p> <p>Delete the building and site orientation menu requirements from the multifamily residential design standards and consolidate elements of these requirements in a streamlined form into the proposed Pedestrian Frontage Standards in 21.07.060F., Tables 21.07-2 and 21.07-3 (pages 10 and 11).</p> <p>Move menu choice m., <i>Site Entry Feature</i> (lines 25-37) from the deleted building orientation menu to the Pedestrian Amenities section of Title 21 (see page 15, lines 18-24) so that it may continue to be available as an option applicants may choose to earn credit toward Title 21 requirements and bonuses.</p>

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.110 Residential Design Standards**

2 *** **

3 **C. Standards for Multifamily and Townhouse Residential**

4 *** **

5 [6. BUILDING AND SITE ORIENTATION MENU]

6 *** **

7 [K. VISIBLE FRONT ENTRIES]

8 [I. PLACE THE PRIMARY FRONT ENTRANCE(S) (AT LEAST ONE SHARED
9 PRIMARY FRONT ENTRANCE FOR A MULTIFAMILY STRUCTURE OR AT
10 LEAST 50 PERCENT OF INDIVIDUAL UNIT ENTRANCES) ON A STREET-
11 FACING BUILDING ELEVATION, OR FACING A COMMON PRIVATE OPEN
12 SPACE THAT IS VISIBLE FROM AND HAS DIRECT ACCESS TO THE STREET.]

13 [II. AS AN ALTERNATIVE, PLACE THE PRIMARY FRONT ENTRANCE(S) ON A
14 FAÇADE AT AN ANGLE OF UP TO 90 DEGREES FROM THE STREET, WHERE
15 THERE IS AN UNOBSTRUCTED LINE OF SIGHT FROM THE STREET OR
16 SIDEWALK EDGE (ABUTTING THE SITE) TO THE ENTRY DOOR, AND A
17 SHELTERING ROOF STRUCTURE NO MORE THAN 12 FEET ABOVE THE
18 FLOOR OF THE ENTRY AS A PERMANENT ARCHITECTURAL FEATURE
19 PROJECTING FROM THE FAÇADE.]

20 [L. ENHANCED FRONT YARD LANDSCAPING]

21 [PROVIDE ONE LEVEL HIGHER OF SITE OR PARKING LOT PERIMETER
22 LANDSCAPING ALONG THE STREET FRONTAGES (UP TO A MAXIMUM OF TWO
23 FRONTAGES) THAN OTHERWISE REQUIRED BY THIS TITLE. FOR EXAMPLE, IF L1
24 LANDSCAPING IS REQUIRED, PROVIDE L2 LANDSCAPING.]

25 [M. SITE ENTRY FEATURE]

26 [I. HIGHLIGHT AND DEFINE A PEDESTRIAN AND/OR VEHICLE ENTRANCE TO
27 A DEVELOPMENT SITE USING THREE OR MORE OF THE FOLLOWING
28 ELEMENTS:]

29 [II. LANDSCAPE TREATMENT WITH SEASONAL COLOR AND TREES, WHICH
30 CLEARLY DISTINGUISHES AND HIGHLIGHTS THE SITE ENTRY.]

31 [III. PLAZA OR COURTYARD AS DESCRIBED IN SUBSECTION 21.07.060F6. OR
32 7.]

33 [IV. IDENTIFYING BUILDING PRIMARY ENTRANCE FORM INCLUDING A
34 COVERED ENTRY, WHEN THE PRIMARY ENTRANCE IS WITHIN 40 FEET OF
35 THE SITE ENTRANCE.]

36 [V. SPECIAL PAVING, PEDESTRIAN SCALE LIGHTING, AND/OR BOLLARDS.]

37 [VI. ORNAMENTAL GATE AND/OR FENCE.]

38 [N. DIRECTOR APPROVAL]

39 [OTHER METHODS, AS APPROVED BY THE DIRECTOR, THAT PROVIDE
40 APPROPRIATE BUILDING AND SITE LAYOUTS RELATIVE TO THE SURROUNDING
41 NEIGHBORHOOD AND STREETS.]

42 *** **

ANNOTATION FOR PAGE 35

Section 21.07.110C.9., Standards for Multifamily and Townhouse Residential – Entryway Treatment

Section 21.07.110D.3., Standards for Some Single-Family and Two-Family Residential Structures – Primary Entrance

<i>Line (s) #</i>	<i>Comment on Change</i>
5-27	<p>21.07.110C.9., Entryway Treatment: Delete and consolidate the multifamily and townhouse entryway standards into Section 21.07.060.</p> <p>The basic multifamily residential entry standard on lines 5-10 is consolidated into 21.07.060G.14. (page 14, lines 12-23). The standard was very similar to the single-family/two-family primary entrance standard (lines 34-39 on page 35). No substantive changes are proposed to the multifamily entryway design, which still includes a sheltered landing or stoop at least four feet square in size, except a visibility requirement has been incorporated from the multifamily building orientation menu choice 6.k. (deleted on page 33) and from the single-family entrance visibility requirement. The relocated residential entry standard is referenced from the Pedestrian Frontage Standards in Tables 21.07-2 and 21.07-3 (Row F) on pages 10-11.</p> <p>The examples of features listed on lines 12-27 on page 35 are merged with extra entry features from other parts of Title 21 into a new Pedestrian Amenity menu choice in 21.07.060G.15., <i>Enhanced Primary Entrance</i> (page 15 lines 24-39).</p>
31-40	<p>21.07.110D.3., Primary Entrance: Delete and consolidate the single-family and two-family entrance standards into Section 21.07.060.</p> <p>The single-family/two-family primary standard from line 31 through page 36 line 7 is consolidated into 21.07.060G.14. (page 15, lines 12-23). The standard was nearly identical to the multifamily/townhouse entrance standard on lines 6-11 above, and they are proposed to be merged in 21.07.060G.14. The wording of the standard has been restated as part of the merger, from requiring at least 16 square feet to requiring minimum inside dimension of four feet square, which is the current multifamily wording. The entry visibility requirement has been clarified using wording from the multifamily design standards.</p> <p>The relocated residential entry standard is referenced from the Pedestrian Frontage Standards in Tables 21.07-2 and 21.07-3 (see row F. in each table) on pages 10 and 11.</p>

1 **21.07.110 Residential Design Standards**

2 *** **

3 **C. Standards for Multifamily and Townhouse Residential**

4 *** **

5 [9. ENTRYWAY TREATMENT]

6 [PRIMARY FRONT ENTRANCES SHALL BE GIVEN EMPHASIS AND PHYSICAL ACCESS AS
7 FOLLOWS. A PORCH, LANDING, PATIO, OR OTHER SEMI-PRIVATE OUTDOOR ENTRYWAY
8 SPACE WITH A MINIMUM DIMENSION OF FOUR FEET SHALL BE PROVIDED THAT IS
9 DISTINGUISHED FROM ADJACENT AREAS AND VEHICULAR PARKING BY A VERTICAL
10 SEPARATION OR CHANGE IN SURFACE MATERIAL. EXAMPLES OF FEATURES THAT MEET
11 THE INTENT OF THE SECTION INCLUDE THE FOLLOWING:]

12 [A. OUTDOOR SHELTERING ROOF STRUCTURE SUCH AS AN OVERHANG, RECESS,
13 PORTICO, OR OTHER PERMANENT ARCHITECTURAL FEATURE COVERING AT
14 LEAST 12 SQUARE FEET.]

15 [B. FAÇADE VARIATION THROUGH WALL MODULATION (CHANGES IN WALL PLANE)
16 OR CHANGES IN EXTERIOR FINISHES (COLOR OR MATERIALS) THAT RELATE TO
17 AND EMPHASIZE THE ENTRY.]

18 [C. ENTRY DETAIL ELEMENTS SUCH AS DOUBLE DOORS, ENTRY SIDE LIGHT OR
19 TRANSOM WINDOWS, OR PLANTER BOXES.]

20 [D. FRONT STEPS CREATING A VERTICAL SEPARATION, AND/OR VERTICAL
21 VEGETATION OR LOW FRONT FENCING, TO DEFINE THE TRANSITION FROM
22 PUBLIC TO MORE PRIVATE OUTDOOR ENTRYWAY SPACE.]

23 [E. LANDSCAPING AND PEDESTRIAN FEATURES, NOT OTHERWISE REQUIRED BY
24 THIS TITLE, SUCH AS INTEGRATED PLANTERS, PEDESTRIAN-SCALE LIGHTING OR
25 ACCENT LIGHTING, OR SPECIAL PAVING TREATMENTS.]

26 [F. OTHER METHODS AS APPROVED BY THE DIRECTOR THAT EMPHASIZE PRIMARY
27 FRONT ENTRANCES.]

28 *** **

29 **D. Standards for Some Single-Family and Two-Family Residential Structures**

30 *** **

31 **3. Pedestrian-Oriented Street Frontage Standards** [PRIMARY ENTRANCE]

32 Provisions for site planning and building orientation in relationship to street frontages and
33 pedestrian access are provided in subsection 21.07.060F., *Pedestrian Frontage Standard*.

34 [A. A PORCH OR LANDING WITH AT LEAST 16 SQUARE FEET SHALL BE PROVIDED AT
35 THE PRIMARY ENTRANCE. THE PORCH OR LANDING SHALL BE COVERED BY A
36 ROOF OF AT LEAST 16 SQUARE FEET.]

37 [B. THE PRIMARY ENTRANCE OF EACH RESIDENCE AND THE WALKWAY TO THAT
38 ENTRANCE SHALL BE CLEARLY VISIBLE FROM THE STREET. PRIMARY
39 ENTRANCES SHALL NOT BE LOCATED ON THE REAR OF THE STRUCTURE.]

ANNOTATION FOR PAGE 36

Section 21.07.110, Residential Design Standards (Continued)

Section 21.07.110D.3., Standards for Some Single-Family and Two-Family Residential Structures – Primary Entrance (Continued)

Section 21.07.110D.4., Standards for Some Single-Family and Two-Family Residential Structures – Garages

<i>Line (s) #</i>	<i>Comment on Change</i>
5-7	<p>Single-Family/Two-Family Primary Entrance Standard (Continued)</p> <p>Consolidate redundant residential walkway requirements into one place in the Title 21 generally applicable on-site pedestrian walkway requirements in Section 21.07.060E.4. (page 8, lines 18-20). No substantive changes are proposed to the single-family/two-family walkway standard as merged onto page 8.</p>
8-39	<p>Single-Family/Two-Family Garages</p> <p>Delete and consolidate this subsection of the residential design standards into the standards in section 21.07.060F., Pedestrian Frontage Standard, pages 9-11.</p>
9-13	<p>Relocate Subsection [A]. Move the limitation on how far a garage may project in front of the rest of the house to Table 21.07-2 (page 10) and Table 21.07-3 (page 11)—see row C in the tables. The wording has been revised for the table format, and an illustration provided above the tables.</p> <p>No substantive change to the standard is proposed outside of the Urban Neighborhood Development Context Area (in Table 21.07-3).</p> <p>In the Urban Neighborhood Contexts (Table 21.07-2), the maximum allowed front façade garage projection is proposed to be cut in half, and no garage projection allowed on lots with alley access such as in South Addition and Fairview.</p>
14-15	<p>Relocate/Revise Subsection [B]. Move the street-facing garage door width limitation to Table 21.07-2 (page 10) and Table 21.07-3 (page 11)—see row B in the tables.</p> <p>The garage door width standard is proposed to be tailored by urban vs. suburban neighborhood development context. In Table 21.07-2, it is merged with existing garage door width limitations from 21.07.110F.4. for properties with alley access to apply a stronger limitation in Urban Neighborhood Contexts. In Table 21.07-3, it is more relaxed allowing up to 67% of the building width to be garage) in suburban neighborhoods.</p>
16-39	<p>Delete Subsection [C]. Delete this menu requirement to select three additional façade design elements for narrow house façades with street-facing garages.</p>

1 **21.07.110 Residential Design Standards**

2 *** **

3 **D. Standards for Some Single-Family and Two-Family Residential Structures**

4 *** **

5 [C. A HARD-SURFACED PEDESTRIAN WALKWAY SHALL BE PROVIDED FROM THE
6 STREET, SIDEWALK, OR DRIVEWAY TO THE PRIMARY ENTRANCE. ROOF
7 DRAINAGE SHALL NOT FALL UPON THE WALKWAY.]

8 [4. GARAGES]

9 [A. WHERE A GARAGE (WITH NO HABITABLE FLOOR AREA ABOVE) EXTENDS FROM
10 THE REST OF THE STRUCTURE TOWARDS THE STREET, THE WIDTH OF THE NON-
11 GARAGE PORTION OF THE FRONT BUILDING ELEVATION SHALL BE NO LESS THAN
12 THE LENGTH THAT THE GARAGE EXTENDS FROM THE REST OF THE
13 STRUCTURE.]

14 [B. GARAGE DOORS FACING THE STREET SHALL COMPRISE NO MORE THAN 67
15 PERCENT OF THE TOTAL WIDTH OF A DWELLING'S BUILDING ELEVATION.]

16 [C. DWELLING UNITS WITH A STREET-FACING BUILDING ELEVATION THAT IS 40 FEET
17 WIDE OR NARROWER AND WITH GARAGE DOORS THAT FACE THE STREET SHALL
18 FEATURE AT LEAST ONE DESIGN ELEMENT FROM EACH OF THE THREE LISTS
19 BELOW.]

20 [I. LIST A:]

21 [(A) AT LEAST ONE DORMER THAT IS ORIENTED TOWARD THE
22 STREET.]

23 [(B) THE FRONT BUILDING ELEVATION HAS TWO OR MORE FACADES
24 THAT ARE OFFSET BY AT LEAST 16 INCHES. EACH FAÇADE OR A
25 COMBINATION OF OFFSET FACADES SHALL BE AT LEAST ONE
26 THIRD OF THE AREA OF THE BUILDING ELEVATION.]

27 [(C) FRONT-FACING BALCONY, ACCESSIBLE FROM A HABITABLE
28 ROOM, AT LEAST SIX FEET WIDE, THAT PROJECTS FROM A
29 FAÇADE AT LEAST TWO FEET AND IS ENCLOSED BY AN OPEN
30 RAILING.]

31 [II. LIST B:]

32 [(A) A PRIMARY ENTRANCE AREA WITH A COVERED PORCH OR
33 LANDING AT LEAST EIGHT FEET WIDE, INCORPORATING VISUAL
34 ENHANCEMENTS SUCH AS GABLED ROOF FORMS, ROOF
35 BRACKETS, FASCIA BOARDS, SIDE LIGHTS, AND/OR ORNAMENTAL
36 COLUMNS DIVIDED VISUALLY INTO TOP, MIDDLE, AND BOTTOM.]

37 [(B) A BAY WINDOW ON THE FRONT ELEVATION AT LEAST SIX FEET
38 WIDE THAT EXTENDS A MINIMUM OF 12 INCHES OUTWARD FROM
39 A FAÇADE, FORMING A BAY OR ALCOVE IN THE ROOM WITHIN.]

40 *** **

ANNOTATION FOR PAGE 37

Section 21.07.110, Residential Design Standards *(Continued)*

Section 21.07.110D.4., Standards for Some Single-Family and Two-Family Residential Structures – Garages *(Continued)*

<i>Line (s) #</i>	<i>Comment on Change</i>
6-27	Continuation of the deletion of the garage standards from page 36, lines 16-39.

1 **21.07.110 Residential Design Standards**

2 *** **

3 **D. Standards for Some Single-Family and Two-Family Residential Structures**

4 *** **

5 *** **

6 [4. GARAGES]

7 *** **

8 [(C) IF THE GARAGE IS MORE THAN ONE CAR WIDE, MULTIPLE GARAGE
9 DOORS ARE USED.]

10 [III. LIST C:]

11 [(A) WINDOWS AND PRIMARY ENTRANCE DOOR(S) THAT OCCUPY A
12 MINIMUM OF 25 PERCENT OF THE WALL AREA OF THE FRONT
13 ELEVATION. WINDOWS IN THE GARAGE DOOR DO NOT COUNT
14 TOWARDS THE 25 PERCENT.]

15 [(B) TRIM (MINIMUM THREE AND ONE HALF INCHES WIDE) OF A
16 DIFFERENT COLOR FROM THE PRIMARY SIDING COLOR, SHALL
17 OUTLINE ALL WINDOWS, DOORS, AND ROOF EDGES ON THE
18 FRONT BUILDING ELEVATION, AND MAY OUTLINE CORNERS AND
19 PROJECTIONS/RECESSES ON THE FRONT BUILDING ELEVATION.]

20 [(C) A MINIMUM OF TWO DIFFERENT SIDING MATERIALS AND/OR
21 PATTERNS ARE USED ON THE FRONT BUILDING ELEVATION.
22 DOORS AND TRIM DO NOT QUALIFY AS A TYPE OF SIDING
23 MATERIAL.]

24 [D. THE HOUSE MAY ENCROACH INTO THE PRIMARY FRONT SETBACK BY UP TO FIVE
25 FEET WHEN THERE IS NO GARAGE, OR WHERE THERE IS A GARAGE (ATTACHED
26 OR DETACHED) WHERE THE FRONT WALL OF THE GARAGE IS LOCATED AT LEAST
27 8 FEET BEHIND THE FRONT FAÇADE OF THE HOUSE.]

28 *** **

29

ANNOTATION FOR PAGE 38

Section 21.07.110, Residential Design Standards (Continued)

Section 21.07.110D.5., Standards for Some Single-Family and Two-Family Residential Structures – Windows

<i>Line (s) #</i>	<i>Comment on Change</i>
5-35	Delete and merge the single-family/two-family minimum window area requirement into the Pedestrian Frontage Standard window requirements in section 21.07.060F. on pages 9-11.
6-14	<p>Subsection [A]: Delete the window area requirement and consolidate into the Pedestrian Frontage Standard in Table 21.07-2 (page 10) for Urban Neighborhood Contexts and Table 21.07-3 (page 11) for other areas of the Municipality. See row E in the tables.</p> <p>The street-facing window requirements in the Urban Neighborhood Contexts (Table 21.07-2 on page 10) incorporate more stringent pedestrian-oriented façade and window requirements for mixed-use development in current Title 21 (see page 10 annotation for more details).</p> <p>The street-facing window requirements in the rest of the city (page 11) carry forward the current Title 21 window standard except to become more lenient on the secondary frontage.</p>
15-17	Consolidate rules for wall and window area calculation into the Title 21 rules of interpretation, in a new subsection 21.15.020P. See page 42.
18-21	Carry forward the energy efficiency exception with revised wording on page 9, lines 31-35.
22-26	Delete the Narrow Lot/Small Home Reduction, as the window area requirement is becoming even more lenient than this reduction for most of the city. Additionally, the current reduction refers to a standard in 21.07.110D.4.c. that is being deleted.
27-35	Delete the exception for additions and renovations, as the replacement requirements in 21.07.060F. only apply to buildings constructed after 2016 and only when the building renovation/addition project is more than 50% of the value of the existing building. See the exemptions in 21.07.060F.2. on page 9.
36-37	Delete an empty placeholder section about prohibited structures that does not contain any regulations.

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.110 Residential Design Standards**

2 *** **

3 **D. Standards for Some Single-Family and Two-Family Residential Structures**

4 *** **

5 [5. WINDOWS]

6 [A. MINIMUM WINDOW AREA REQUIREMENT]
7 WINDOWS AND PRIMARY ENTRANCE DOOR(S) SHALL OCCUPY A MINIMUM OF 10
8 PERCENT OF THE WALL AREA OF A BUILDING ELEVATION FACING A STREET OR
9 REQUIRED PRIVATE COMMON OPEN SPACE (UP TO A MAXIMUM OF TWO
10 ELEVATIONS). THE DIRECTOR MAY ELIMINATE OR REDUCE THE REQUIREMENT
11 ON ONE ELEVATION (IN INSTANCES WHERE UP TO TWO APPLY) BASED ON SITE
12 SPECIFIC CIRCUMSTANCES, STRUCTURAL REQUIREMENTS FOR SHEAR, AND
13 ORGANIZATION OF SPACES IN THE HOME (WINDOWS ARE NOT REQUIRED IN
14 ROOMS NOT NORMALLY INHABITED OR IN GARAGES).

15 [B. WALL AREA CALCULATION]
16 [WALL AREA CALCULATION SHALL BE IN ACCORDANCE WITH SUBSECTION
17 21.07.110C.3.B.]

18 [C. AN OVERALL REDUCTION IN REQUIRED WINDOW AREA MAY BE APPROVED IF
19 DEMONSTRATED BY CALCULATION BY AN ENERGY RATER CERTIFIED BY THE
20 STATE OF ALASKA THAT INSTALLING THE REQUIRED WINDOW AREA WILL
21 REDUCE THE ENERGY RATING BELOW A 5-STAR ENERGY RATING.]

22 [D. NARROW LOT/SMALL HOME REDUCTION
23 [AN OVERALL REDUCTION IN REQUIRED WINDOW AND PRIMARY ENTRANCE
24 DOOR AREA TO 7% MAY BE APPROVED WHEN THE CALCULATED WALL AREA IS
25 UNDER 500 SQUARE FEET. UNITS USING THIS REDUCTION SHALL COMPLY WITH
26 SUBSECTION 21.07.110D.4.C.]

27 [E. ADDITIONS AND RENOVATIONS]
28 [ON EXISTING STRUCTURES, ONLY THE WALL AREA AFFECTED BY EITHER AN
29 ADDITION OR A RENOVATION SHALL BE REQUIRED TO COMPLY WITH THE
30 OPENING REQUIREMENTS. UNAFFECTED WALL AREAS NEED NOT COMPLY.
31 GARAGE ADDITIONS ARE EXEMPT FROM OPENING REQUIREMENTS. THE
32 DIRECTOR MAY ELIMINATE OR REDUCE THE REQUIREMENT FOR WINDOW
33 OPENINGS ON ADDITIONS/RENOVATIONS BASED ON PROJECT-SPECIFIC
34 CIRCUMSTANCES (SUCH AS THE NATURE OF THE SPACES BEING ADDED,
35 BUILDING ORIENTATION, OR STRUCTURAL REQUIREMENTS).]

36 [E. PROHIBITED STRUCTURES]

37 [[RESERVED]]

38 *** **

39

ANNOTATION FOR PAGE 39

Section 21.07.110F.3., Residential Design Standards – Site Design – Driveway Width

The current Title 21 residential driveway width regulations are separate from, yet largely redundant to, the more comprehensive, generally applicable driveways regulations, in 21.07.090H.8. through 11. The residential driveway standards are proposed to be deleted from the Residential Design Standards on the next page and merged into 21.07.090H.8. through 11.

<i>Line (s) #</i>	<i>Comment on Change</i>
5-30	<p>Delete and merge the residential driveway width standards into the driveway standards in section 21.07.090H.11. on page 24 line 24 through page 25 line 18.</p> <p>The purpose statement on lines 6-12 is deleted and should be relocated to the objectives under the overall purpose statement for 21.07.090, Off-Street Parking and Loading, as shown in the draft A.O. 2022-80 as recommended by PZC Resolution No. 2022-021 (PZC Case 2022-0026).</p> <p>The limitation of the applicability to only the driveway approach within the ROW is carried forward as stated on page 24 line 29. No substantive changes are proposed to the standard as merged.</p>
31-38	<p>Move, simplify, and correct the driveway width exceptions i. and ii. that allow for driveways to be a minimum allowed width. These two exceptions are replaced by subsections 21.07.090H.11.c.i. on page 25 lines 9-13 and 11.d.iii. on page 25 lines 25-27.</p> <p><i>Discussion:</i> The current multifamily driveway width allowance of 14 feet is too wide (12 is adequate) and the single-family driveway width of 10 feet is too narrow (12 feet is needed).</p>
39-41	<p>Move the flag lot exception to page 25, lines 16-18. No changes proposed.</p>

1 **21.07.110 Residential Design Standards**

2 *** **

3 **[F]. Site Design**

4 *** **

5 **[3. DRIVEWAY WIDTH]**

6 **[A. PURPOSE]**

7 [THIS SECTION LIMITS THE WIDTH OF A DRIVEWAY AT THE PROPERTY LINE AND
8 AT THE STREET CURB. THE INTENT OF THESE LIMITATIONS IS TO PROVIDE
9 ADEQUATE SPACE FOR SNOW STORAGE WITHIN THE RIGHT-OF-WAY, TO HAVE
10 SPACE FOR ON-STREET PARKING WHERE APPROPRIATE, AND TO DISCOURAGE
11 THE MAJORITY OF THE FRONT AREA OF A LOT FROM BEING PAVED AND/OR USED
12 FOR VEHICLE PARKING.]

13 **[B. APPLICABILITY]**

14 **[I. THIS SECTION APPLIES TO DRIVEWAY THROAT WIDTH AT THE PROPERTY**
15 **LINE AND STREET CURB.]**

16 **[II. RESIDENTIAL DRIVEWAYS ARE ALSO SUBJECT TO THE MUNICIPAL**
17 **DRIVEWAY STANDARDS CURRENTLY ESTABLISHED BY THE TRAFFIC**
18 **ENGINEER. WHERE THERE IS A CONFLICT, THIS SECTION SHALL**
19 **GOVERN. ACCESS TO STREETS OWNED BY THE STATE OF ALASKA**
20 **REQUIRES COMPLIANCE WITH STATE DRIVEWAY STANDARDS, AS**
21 **PROVIDED IN SUBSECTION 21.07.090H.8.D.]**

22 **[III. WHEN A DRIVEWAY SERVES BOTH RESIDENTIAL AND NONRESIDENTIAL**
23 **PRINCIPAL USES, THE DRIVEWAY DIMENSIONS SHALL BE AS REQUIRED**
24 **FOR THE NONRESIDENTIAL USE, UNLESS APPROVED OTHERWISE BY THE**
25 **TRAFFIC ENGINEER.]**

26 **[C. PERCENT OF LOT FRONTAGE]**

27 [THE TOTAL WIDTH OF DRIVEWAY ENTRANCES TO A RESIDENTIAL LOT FROM A
28 STREET SHALL NOT EXCEED 40 PERCENT OF THE FRONTAGE OF THE LOT, OR 33
29 PERCENT OF THE FRONTAGE IF THE PLATTING AUTHORITY OR TRAFFIC
30 ENGINEER FINDS THAT CONDITIONS WARRANT IT.]

31 **[I. A DRIVEWAY FOR MULTIFAMILY DWELLINGS, MIXED-USE DWELLINGS, OR**
32 **A GROUP LIVING USE MAY ALWAYS BE AT LEAST 14 FEET WIDE.]**

33 **[II. A DRIVEWAY FOR A SINGLE-FAMILY, TWO-FAMILY, OR TOWNHOUSE**
34 **DWELLING MAY ALWAYS BE AT LEAST TEN FEET WIDE, PROVIDED THE**
35 **TRAFFIC ENGINEER DETERMINES SNOW STORAGE, TRAFFIC FLOW AND**
36 **SAFETY, AND THE URBAN CONTEXT ARE ADDRESSED, AND PROVIDED**
37 **TOWNHOUSE DRIVEWAYS ARE ATTACHED IN PAIRS TO THE MAXIMUM**
38 **EXTENT FEASIBLE.]**

39 **[III. FLAG LOTS ARE EXEMPT FROM THE PERCENTAGE LIMITATIONS, BUT**
40 **SHALL HAVE A MAXIMUM DRIVEWAY WIDTH OF 20 FEET. ABUTTING FLAG**
41 **LOTS MAY SHARE A DRIVEWAY UP TO 24 FEET WIDE (12 FEET PER LOT).]**

ANNOTATION FOR PAGE 40

Section 21.07.110F.3., Residential Design Standards – Site Design – Driveway Width *(Continued)*

<i>Line (s) #</i>	<i>Comment on Change</i>
7-24	Section 21.07.110F. Residential Design Standards – Site Design: Driveway Width Delete and merge the residential driveway width exceptions clause into the driveway standards in subsection 21.07.090H.11.c.i. See page 25 lines 10-13.

1 **21.07.110 Residential Design Standards**

2 *** **

3 **E[F]. Site Design**

4 *** **

5 **[3. DRIVEWAY WIDTH]**

6 *** **

7 **[D. EXCEPTIONS]**

8 [THE TRAFFIC ENGINEER AND THE PLANNING DIRECTOR MAY APPROVE A
9 DEPARTURE FROM THE STANDARDS OF THIS SECTION, SUCH AS A NARROWER
10 DRIVEWAY, IF DOCUMENTATION PREPARED BY A LICENSED PROFESSIONAL IN
11 THE STATE OF ALASKA REGISTERED WITH THE ALASKA STATE BOARD OF
12 REGISTRATION FOR ARCHITECTS, ENGINEERS AND LAND SURVEYORS
13 DEMONSTRATES THE DRIVEWAY STILL MEETS THIS CHAPTER'S STANDARDS
14 AND THE MUNICIPAL DRIVEWAY STANDARDS MEMO ISSUED BY THE MUNICIPAL
15 TRAFFIC ENGINEER, AND THAT CHANGE IS APPROPRIATE. APPROVAL SHALL BE
16 CONTINGENT ON FACTORS SUCH AS STREET CLASSIFICATION, STREET
17 TYPOLOGY, URBAN CONTEXT, TRAFFIC VOLUME AND SPEED, CURB RETURN
18 RADII, STREET TRAVEL LANE OFFSET FROM FACE OF CURB, PEDESTRIAN AND
19 BICYCLE FACILITIES, SNOW STORAGE, DRIVEWAY CONFIGURATION AND
20 LENGTH, SITE AND PROJECT CHARACTERISTICS, NUMBER OF VEHICLES
21 EXPECTED TO USE THE DRIVEWAY, AND COMPREHENSIVE PLAN POLICIES. THE
22 TRAFFIC ENGINEER AND PLANNING DIRECTOR MAY ALSO BE MORE RESTRICTIVE
23 THAN THE STANDARDS OF THIS SECTION, PROVIDED THEY DOCUMENT THE
24 RATIONALE.]

25 *** **

ANNOTATION FOR PAGE 41

21.07.110F.4., Residential Design Standards – Site Design – Alleys

Delete this section and merge and modify its standards in the generally applicable driveways standards of Title 21.

<i>Line (s) #</i>	<i>Comment on Change</i>
5-37	<p>Section 21.07.110F.4., Alleys</p> <p>Consolidate the residential alley access requirement into the driveway standards in section 21.07.090H.9.b. on page 21, lines 11-22. See page 21 annotation for full details.)</p> <p>The 10-foot garage door width limitation from lines 22-26 on the next page is merged into Table 21.07-2 (table row B) on page 10, where it is replaced by a more lenient width limitation based on a percentage of the dwelling front façade width.</p> <p>Front yard driveway width limitations from lines 22-26 are merged into subsections 21.07.090H.9.b. on page 21 (lines 15-20) and Table 21.07-2 (row A) on page 11.</p>

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.07.110 Residential Design Standards**

2 *** **

3 **[F]. Site Design**

4 *** **

5 **[4. ALLEYS]**

6 **[A. THE INTENT OF THIS SECTION IS TO PROMOTE VEHICLE DRIVEWAY ACCESS FROM**
7 **REAR ALLEYS AND REDUCE THE IMPACTS OF FRONT YARD DRIVEWAYS IN OLDER**
8 **URBAN NEIGHBORHOODS.]**

9 **[B. THIS SECTION APPLIES TO RESIDENTIAL DEVELOPMENTS LOCATED IN THE**
10 **TRADITIONAL NEIGHBORHOOD DESIGN AREAS DESIGNATED ON MAP 2-1:**
11 **ANCHORAGE 2040 LAND USE PLAN MAP IN THE ANCHORAGE 2040 LAND USE PLAN.]**

12 **[C. ACCESS TO PARKING FOR RESIDENTIAL USES SHALL BE FROM THE ALLEY WHEN**
13 **THE SITE ABUTS AN ALLEY, EXCEPT THAT STREET ACCESS IS PERMITTED IN ANY**
14 **OF THE FOLLOWING SITUATIONS:]**

15 **[I. ACCESS TO A TOWNHOUSE DWELLING ON A CORNER LOT MAY BE FROM THE**
16 **STREET FRONTAGE HAVING THE SECONDARY FRONT SETBACK OR THE**
17 **ALLEY.]**

18 **[II. DUE TO THE RELATIONSHIP OF THE ALLEY TO THE STREET SYSTEM AND/OR**
19 **THE PROPOSED HOUSING DENSITY OF THE DEVELOPMENT, THE TRAFFIC**
20 **ENGINEER DETERMINES THAT USE OF THE ALLEY FOR PARKING ACCESS**
21 **WOULD BE A SIGNIFICANT TRAFFIC IMPACT OR SAFETY HAZARD.]**

22 **[III. THE TRAFFIC ENGINEER DETERMINES THAT TOPOGRAPHY OR OTHER**
23 **NATURAL FEATURE OR PHYSICAL BARRIER MAKES ALLEY ACCESS**
24 **INFEASIBLE.]**

25 **[IV. THE ALLEY IS NOT IMPROVED AND TRAFFIC ENGINEER DETERMINES THAT**
26 **IMPROVEMENT IS NOT FEASIBLE.]**

27 **[V. A SINGLE-FAMILY DWELLING, TWO-FAMILY DWELLING, OR TOWNHOUSE**
28 **DWELLING WITH TWO UNITS, WITH ALLEY ACCESS MAY HAVE A GARAGE OR**
29 **DRIVEWAY THAT FACES THE STREET IF THE GARAGE DOOR IS NO WIDER**
30 **THAN 10 FEET AND THE DRIVEWAY NO WIDER THAN 12 FEET AT ANY POINT.]**

31 **[D. IN SITUATIONS WHERE A GROUP OF LOTS FRONT AN ENTIRE BLOCK ON ONE SIDE**
32 **OF A STREET BETWEEN TWO INTERSECTIONS, ABUT A MID-BLOCK ALLEY, AND ARE**
33 **BEING DEVELOPED TOGETHER, THEN PARKING ACCESS TO THE STRUCTURES**
34 **SHALL BE FROM THE ALLEY, AND BUILDING(S) MAY ENCROACH INTO THE FRONT**
35 **SETBACK BY UP TO FIVE FEET.]**

36 **[E. IF A NEW DEVELOPMENT INCLUDES ALLEYS, THE LOT DEPTH REQUIREMENT IS**
37 **REDUCED BY HALF THE WIDTH OF THE ALLEY AND THE LOT AREA REQUIREMENT**
38 **IS REDUCED BY 12 PERCENT FOR THOSE LOTS THAT ABUT AN ALLEY. VEHICULAR**
39 **ACCESS TO ALL DWELLING UNITS ON LOTS ABUTTING ALLEYS SHALL BE FROM THE**
40 **ALLEY, AND VEHICULAR ACCESS TO SUCH UNITS FROM THE STREET IS**
41 **PROHIBITED.]**

42 *** **

ANNOTATION FOR PAGE 42

Section 21.15.020, Rules of Construction and Interpretation

Current section 21.15.020 of Title 21 establishes generally applicable rules of measurement and calculations in applying Title 21 regulations, including how to measure distances or round fractions of whole numbers.

Various sections and chapters of Title 21 currently establish window requirements. The rules of measurement for window area as a percentage of the building façade are scattered and unclear. This proposed new subsection P. merges the rules for calculating window area requirements into one place in Title 21, for ease of reference and consistency of application.

No substantive changes are proposed to the rules for measuring required window area.

<i>Line (s) #</i>	<i>Comment on Change</i>
3-7	Express the basic measurement rule and provide an illustrative example. This basic information about the existing regulation is intended to provide clarity and consistency. Parts of the building that are exempt from wall area measurement are provided below, in subsection P.3. starting on line 20.
8-15	Provide an illustrated example measurement for ease of reference by code users.
16-19	Clarify existing Title 21 rules for what to include in window and entrance area that count toward the requirements. This provision includes providing references to applicable Title 21 definitions in 21.15.040.
20-32	Carry forward and merge existing exemptions for parts of the building wall area. Subsection 3. consolidates all exemptions listed in district-specific standards and the residential design standards. This clarifies that exemptions previously only listed as part of a window requirement in one section of Title 21 are intended to apply to window requirements in other sections. It includes the windows illustration from the deleted Residential Design Standards windows subsection (page 30).

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.15.020 Rules of Construction and Interpretation**

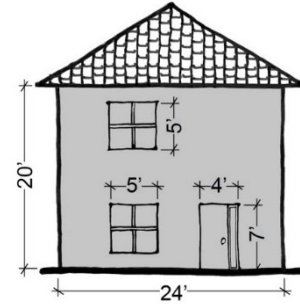
2 *** **

3 **P. Measurement of Windows and Entrances as a Percentage of Building Wall Area**

4 The percentage of building elevation wall area that is window and entrance openings is determined by
5 dividing the total square footage of the windows and primary entrances on the building elevation by the
6 gross square footage of the building elevation wall area (except for parts of the building exempted in 3.),
7 and then multiplying the resulting quotient by 100.

8 **1. Illustrated Example**

9 In the illustration, the building elevation wall area is 20 feet of
10 height x 24 feet of length = 480 square feet. The window and
11 primary entrance area is the area of the two windows (5 x 5
12 feet x 2) plus the area of the primary entrance and its sidelight
13 (4 x 7 feet) = 78 square feet. The percentage of the building
14 elevation wall area is found by dividing 78 by 480 then
15 multiplying by 100, to yield 16%.



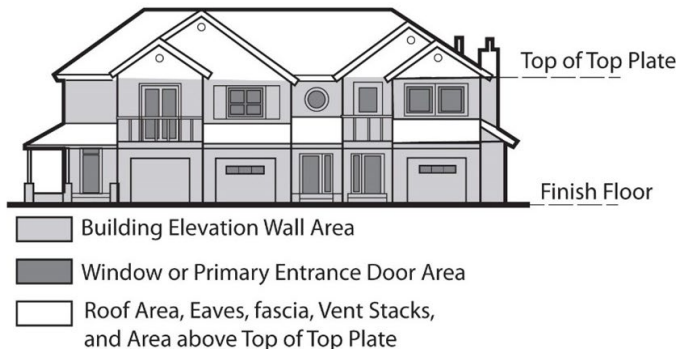
16 **2. Measuring Window and Primary Entrance Area**

17 “Window area” is defined in section 21.15.040, Definitions. Primary entrance area includes the
18 entrance opening and door frame but excludes trim, molding, and other features beyond the door
19 frame. “Primary entrance” is defined in 21.15.040, Definitions.

20 **3. Exempt Parts of the Building Elevation Wall Area**

21 The building elevation wall area is measured as the exterior wall plane surface area from finished
22 grade to the top of the wall on the building except that, for the purposes of measuring windows and
23 entrances as a percentage of building wall area, the following parts of the building are not included
24 in the wall area calculation:

- 25 **a.** Wall area above the topmost story’s top plate, such as gable ends of a roof enclosing a
26 cold attic, or parapet walls.
- 27 **b.** Roof surfaces, eaves, fascia, vent stacks, mechanical equipment and rooms, rooftop
28 access enclosures, and other roof appurtenances.
- 29 **c.** Wall area of stories below grade plane, such as below grade parking, up to the finished
30 floor of the story above grade plane.
- 31 **d.** Parts of a street-facing building elevation below the grade plane of the abutting sidewalk
32 (or edge of street pavement where there is no sidewalk).



33 *** **

ANNOTATION FOR PAGE 43

Section 21.15.040, Definitions

Section 21.15.040 sets forth the terms and definitions used throughout Title 21. Pages 43 and 44 below amend several definitions and add or delete several definitions to the closing chapter of Title 21.

The definitions for “Alley,” “Driveway,” and “Driveway Throat Width” on the facing page are current Title 21 definitions provided here for reference only with no changes proposed.

<i>Line (s) #</i>	<i>Comment on Change</i>
14-17	<p>Driveway Approach:</p> <p>Add an illustrated term and definition describing the part of a driveway that is within the public street right-of-way (i.e., not on the private property) and connects the private property to the edge of the street. This term and its illustration clarify existing Title 21 and Municipal Driveway Standards regulations that apply only to the portion of the driveway that is in the right-of-way, not on private property.</p> <p>The term is used to help clarify existing regulations on page 24 lines 6-16, page 24 line 29 through page 25 line 18, and page 25 lines 23-24.</p>

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.15.040 Definitions**

2 *** **

3 **Alley**

4 A permanent service right-of-way providing a secondary means of access to abutting properties.

5 *** **

6 **Complete Street.**

7 **See Street, Complete.**

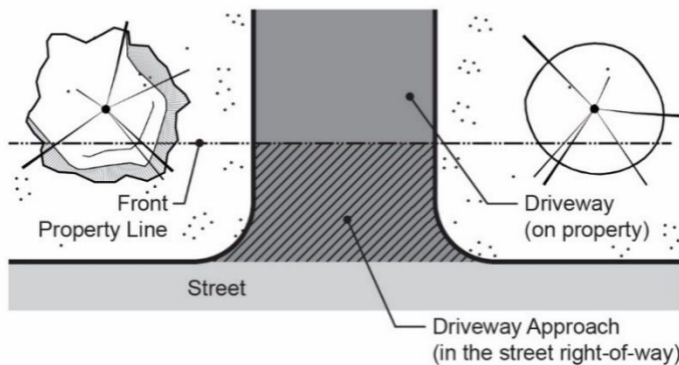
8 *** **

9 **Driveway**

10 The paved connection meeting municipal driveway standards located between the garage of a dwelling unit and
11 the adjacent street (public or private) or between a parking facility and the adjacent street (public or private). A
12 driveway is not a street. It does not provide primary frontage or address for buildings, nor is it primarily designed for
13 non-motorized pedestrian access.

14 **Driveway Approach**

15 **The portion of a driveway providing direct vehicle ingress and egress over public right-of-way to a property. The**
16 **driveway approach extends from the street edge to the front property line.**



17
18

19 **Driveway Throat Width**

20 The width of a driveway at the property line or street curb, measured from face of curb to face of curb (or, where
21 there are no curbs, between the edges of the driveway travel way surface), at the point of tangency. The
22 measurement does not include any medians contained in the driveway. For driveways with a curb return design at
23 the opening of the street curb, the measurement does not include additional width at the driveway opening created
24 by the curb return radii.

25 *** **

ANNOTATION FOR PAGE 44

Section 21.15.040, Definitions (Continued)

<i>Line (s) #</i>	<i>Comment on Change</i>
3-13	<p>Pedestrian Feature</p> <p>On lines 13-15, amend the definition to include pedestrian area lighting and ornamental lighting as “pedestrian features” that can receive credit toward Title 21 standards and menu choices, such as on page 13 line 36 or page 14 line 39.</p> <p>On lines 16-17, amend the definition to include cross-country ski racks and pedestrian-scale lighting as pedestrian transportation amenities, in response to public comments made during the public process for A.O. 2022-80.</p>
20-23	<p>Pedestrian Movement Zone</p> <p>Update and clarify this existing definition. It is used by several “Pedestrian Amenity” menu choices in 21.07.060G. Cross-reference to “walkway clear width.”</p>
28-33	<p>Complete Street</p> <p>Add definition from the Anchorage transportation planning “Complete Streets Policy” which supports the use of the term in the description for the “Enhanced Street Sidewalk” pedestrian amenity on page 17.</p>
35-37	<p>Walkway Clear Width</p> <p>Clarify this existing definition.</p>
38-39	<p>Walkway, Primary Pedestrian</p> <p>Remove this reference to a term and definition recently deleted by A.O. 2022-80(S). The term defined a pedestrian amenity in 21.07.060G.4. which is being renamed to “Enhanced On-Site Walkway” on page 13. The definition in 21.15.040 was redundant to the description of the amenity on page 13.</p>

CODE LANGUAGE to be added is underlined. Language to be deleted is [ALL CAPS IN BRACKETS].

1 **21.15.040 Definitions**

2 *** **

3 **Pedestrian Feature**

4 A permanent object that provides pedestrians with increased convenience, comfort, and utility, and which is publicly
5 accessible and not limited to a tenant or establishment such as seating for a restaurant. Pedestrian features include:

- 6 • Seating such as benches accommodating several people;
- 7 • Secondary/informal seating opportunities such as steps, pedestals, low walls, or edges of fountains,
8 accommodating several people;
- 9 • A space for standing with objects to lean against, such as bollards, short fences, or irregular building
10 facades, accommodating several people;
- 11 • A tree or raised planter;
- 12 • A work of art such as a water feature, sculpture, cultural exhibit, or clock feature;
- 13 • Pedestrian-scale area lighting designed to light outdoor walkways or pedestrian spaces;
- 14 • Ornamental lighting designed to illuminate architecture and/or landscaping for decorative and aesthetic
15 effect;
- 16 • A cross-country ski rack that can accommodate a ski lock and is located at building entrances within 500
17 feet of a ski trailhead;
- 18 • A winter city urban design feature such as a wind screen, or outdoor stove or space heater; or
- 19 • Other object supporting pedestrian utility, such as a gazebo or kiosk.

20 **Pedestrian Movement Zone**

21 The middle portion of an enhanced street sidewalk (21.07.060G.19.), comprising its walkway clear width located
22 between the sidewalk's street interface and building interface zones. The pedestrian movement zone provides for
23 the primary function of sidewalks[,] and is kept clear of any obstructions to pedestrian movement.

24 *** **

25 **Primary Entrance**

26 **See Entrance, Primary.**

27 *** **

28 **Street, Complete**

29 A street that is designed, used, and operated to enable safe access for all users, including pedestrians, bicyclists,
30 motorists, and public transportation users of all ages and abilities, and for all users to safely move through the
31 transportation network. For example, Anchorage Metropolitan Area Transportation Solutions (AMATS) adopted a
32 complete streets policy in 2018 for its urbanized planning area. The AMATS Complete Streets Policy is available
33 on the municipal website.

34 *** **

35 **Walkway Clear Width**

36 That portion of the total width of a walkway, trail, pathway, or sidewalk cross-section which is unobstructed,
37 continuous (without interruption or being split by obstructions), and kept clear for pedestrian movement.

38 [WALKWAY, PRIMARY PEDESTRIAN]

39 [SEE PRIMARY PEDESTRIAN WALKWAY.]

40 *** **



Planning Department
Long-Range Planning Division
PO Box 196650
Anchorage, Alaska 99519-6650

www.muni.org/Planning/2040Actions.aspx