

**Clean Version  
Public Hearing Draft Ordinance Language**

***Title 21 Text Amendment to Site Access Regulations***

**PZC Case No. 2023-0011**

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# Clean Version of Proposed Amended Zoning Code

This version of the proposed amendments shows amended code sections as they would look in the published code book after final adoption. It includes indications of where sections of code are removed. For additional detail, please refer to the *Annotated (Narrated) Version of the Public Hearing Draft Ordinance* (Attachment 3).

The text amendments are arranged in the order they appear in Anchorage’s *Title 21 Zoning Ordinance*. Only the sections of Title 21 that are being added or amended are included. Where a full subsection of current code is deleted, a note to the reader in italics and parentheses indicates the proposed deletion.

Asterisks (\*\*\*) indicates that all intervening parts of the chapter are unchanged between the section header above the asterisks and the language shown below the asterisks.

Proposed new sections are marked “(New)” in the table of contents below:

Zoning Code Section Affected by Proposed Changes	Page
21.03. Review and Approval Procedures	1
21.04. Zoning Districts ( <i>district-specific standards</i> )	2
21.07.010D. Alternative Equivalent Compliance ( <i>applicability</i> )	3
21.07.010E. Urban Neighborhood Development Context Area ( <i>new subsection E.</i> )	3
21.07.060E. Standards for Pedestrian Facilities ( <i>sidewalks and walkways</i> )	5
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21.07.060G. Pedestrian Amenities Menu ( <i>renumbered from F to G; various amendments</i> )	8
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21.15.020P. Rules of Construction and Interpretation ( <i>new subsection P.</i> )	19
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1 **CHAPTER 21.03: REVIEW AND APPROVAL PROCEDURES**

2 \*\*\* \*\*

3 **21.03.240 Variances**

4 \*\*\* \*\*

5 **J. Administrative Variances**

6 \*\*\* \*\*

7 **7. For Front Driveways on Multifamily and Townhouse Lots with Alleys**

8 Where a multifamily or townhouse development site is served by an alley in the Urban  
9 Neighborhood Development Context (21.07.010E.), the director and traffic engineer may grant an  
10 administrative variance from subsection 21.07.090H.9.b., *Alley Access Requirement*, provided:

11  
12 **[E. REDUCED PARKING RATIOS]**

13 [DEVELOPMENT IN THE R-3A DISTRICT SHALL BE ELIGIBLE FOR A REDUCTION OF  
14 THE MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SUBSECTION  
15 21.07.090F.6.]

16 **a[F].** There exist physical circumstances of the subject property such as topography, absence  
17 of alley improvements, exceptional lot configuration not shared by landowners in general,  
18 or adjoining street traffic patterns, and the traffic engineer determines that alley  
19 improvement and/or vehicle access are not feasible or would create a traffic impact or  
20 safety hazard;

21  
22 **[E. REDUCED PARKING RATIOS]**

23 [DEVELOPMENT IN THE R-3A DISTRICT SHALL BE ELIGIBLE FOR A REDUCTION OF  
24 THE MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SUBSECTION  
25 21.07.090F.6.]

26 **b[F].** Because of these physical circumstances, the strict application of the alley access  
27 requirement in 21.07.090H.9.b. would create an exceptional or undue hardship upon the  
28 property owner, and would deprive the owner of rights commonly enjoyed by other  
29 properties in the same zoning district;

30  
31 **[E. REDUCED PARKING RATIOS]**

32 [DEVELOPMENT IN THE R-3A DISTRICT SHALL BE ELIGIBLE FOR A REDUCTION OF  
33 THE MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SUBSECTION  
34 21.07.090F.6.]

35 **c[F].** The hardship is not self-imposed, the conditions and circumstances do not result from the  
36 actions of the applicant, and such conditions and circumstances do not merely constitute  
37 inconvenience; and

38  
39 **[E. REDUCED PARKING RATIOS]**

40 [DEVELOPMENT IN THE R-3A DISTRICT SHALL BE ELIGIBLE FOR A REDUCTION OF  
41 THE MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SUBSECTION  
42 21.07.090F.6.]

43 **d[F].** The administrative variance granted for additional driveway access shall be the minimum  
44 the traffic engineer determines is necessary to provide access for the development.

45 \*\*\* \*\*

46

1 **CHAPTER 21.04: ZONING DISTRICTS**

2 \*\*\* \*\*

3 **21.04.020 Residential Districts**

4 \*\*\* \*\*

5 **H. R-3A: Residential Mixed-Use District.**

6 \*\*\* \*\*

7 **2. District-Specific Standards**

8 \*\*\* \*\*

9 **[E. REDUCED PARKING RATIOS]**

10 [DEVELOPMENT IN THE R-3A DISTRICT SHALL BE ELIGIBLE FOR A REDUCTION OF  
11 THE MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SUBSECTION  
12 21.07.090F.6.]

13 **e[F]. Enhanced Street Street Sidewalk[ OPTION]**

14 An enhanced street sidewalk [ENVIRONMENT ]may be provided in lieu of required  
15 sidewalks and site perimeter landscaping, as defined in 21.07.060G.20[F.17].

16 \*\*\* \*\*

17 **21.04.030 Commercial Districts**

18 \*\*\* \*\*

19 **G. Standards for Mixed-Use Development in the B-1A and B-1B Districts**

20 \*\*\* \*\*

21 **[5. REDUCED PARKING RATIOS]**

22 [THE DEVELOPMENT IS ELIGIBLE FOR A REDUCTION OF UP TO FIVE PERCENT OF THE  
23 MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SECTION 21.07.090F.6.]

24 **5[6]. Enhanced Street Street Sidewalk[ OPTION]**

25 An enhanced street sidewalk [ENVIRONMENT ]may be provided in lieu of required sidewalks and  
26 site perimeter landscaping, as defined in 21.07.060G.20[F.17].

27  
28 *(Note: Subsection 21.04.020G.7., Building Orientation and Placement, is deleted.)*

30 **H. Standards for Mixed-Use Development in the B-3 District**

31 \*\*\* \*\*

32 **[3. BUILDING PLACEMENT AND ORIENTATION]**

33 [BUILDINGS ARE SUBJECT TO THE BUILDING PLACEMENT AND ORIENTATION  
34 STANDARDS FOR MIXED-USE DEVELOPMENT IN THE B-1A/B-1B DISTRICTS IN SECTION  
35 21.04.030G.7. ABOVE.]

37 **[4. REDUCED PARKING RATIOS]**

38 [THE DEVELOPMENT IS ELIGIBLE FOR A REDUCTION OF UP TO FIVE PERCENT OF THE  
39 MINIMUM NUMBER OF PARKING SPACES, AS PROVIDED IN SECTION 21.07.090F.6.]

40 *(Note: Subsections 21.04.020H.3., Building Placement and Orientation, is deleted.)*

42 **3[5]. Enhanced Street Street Sidewalk[ OPTION]**

43 An enhanced street sidewalk [ENVIRONMENT ]may be provided in lieu of required sidewalks and  
44 site perimeter landscaping, as provided in 21.07.060G.20[F.17].

45 \*\*\* \*\*

46 **21.04.070 Overlay Zoning Districts**

47 \*\*\* \*\*

48 **C. Commercial Center Overlay**

49 \*\*\* \*\*

1 [INSTEAD OF THE FIVE PERCENT REDUCTION PROVIDED IN SECTIONS 21.04.030G. AND  
2 H., ANY USE IS ELIGIBLE FOR A REDUCTION OF UP TO 10 PERCENT OF THE MINIMUM  
3 NUMBER OF PARKING SPACES, AS PROVIDED IN SECTION 21.07.090F.6.]

4 **5.[6]. Dimensional Standards**

5 Uses in the B-1B district are exempt from the gross floor area limitations of subsection  
6 21.04.030C.2.b., provided that an individual use occupies no more than 60,000 square feet of gross  
7 floor area.

8  
9 *(Note: Subsection 21.04.070C.5.a. is deleted.)*

10 [A. THE MINIMUM FRONT SETBACK IS REDUCED TO FIVE FEET FOR BUILDING  
11 ELEVATIONS THAT COMPLY WITH 21.04.030G.7., BUILDING PLACEMENT AND  
12 ORIENTATION.]

13 \*\*\* \*\*

14

1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 \*\*\* \*\*

3 **21.07.010 General Provisions.**

4 \*\*\* \*\*

5 **D. Alternative Equivalent Compliance.**

6 \*\*\* \*\*

7 2. *Applicability.* The alternative equivalent compliance procedure shall be available only for  
8 the following sections of this title:

9 \*\*\* \*\*

10 c. Subsection 21.07.060F., Pedestrian Frontage Standard;

11  
12 d. Subsection 21.07.060G., Pedestrian Amenities;

13  
14 (Note to code revisor: re-number subsections c. - i. as subsections d.- j.)

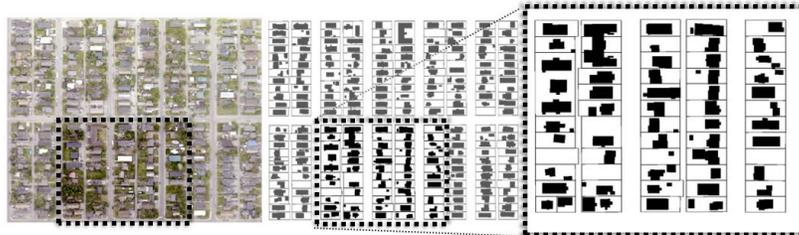
15 \*\*\* \*\*

16 **E. Urban Neighborhood Development Context Area.**

17  
18 **1. Purpose.**

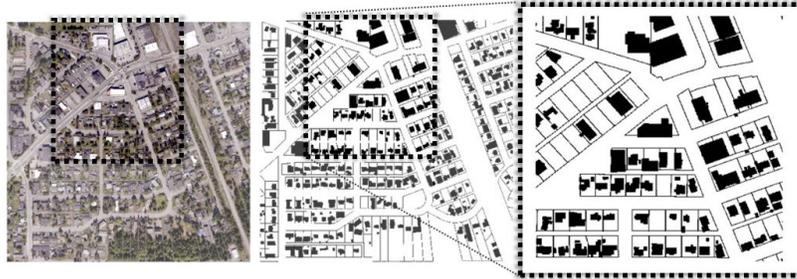
19 This section establishes an Urban Neighborhood Development Context Area that reflects  
20 the existing and desired characteristics of Anchorage’s older, urban neighborhoods  
21 identified by the Comprehensive Plan, Land Use Plan Map as Traditional Neighborhood  
22 Design areas. Some are also identified in neighborhood and district plans. This section  
23 provides a basis for development regulations tailored to the characteristics of these  
24 neighborhoods and fulfills their role as recommended in the Comprehensive Plan.

- 25
- 26 a. This area is intended to include Anchorage’s original urban neighborhoods of  
27 Government Hill, South Addition, and Fairview, and post-war era (1950s-1960s)  
28 neighborhoods such as Spenard, Airport Heights, and Russian Jack Park. It also  
29 includes transit-supportive development corridors and designated mixed-use centers.
- 30
- 31 b. This area recognizes urban neighborhood characteristics including a more highly  
32 interconnected street system, smaller lot and block sizes, and access to pedestrian  
33 facilities and public transit. Some neighborhoods have consistent or frequent sidewalks  
34 and alleys. Buildings and residences often have moderate front setbacks and orient to  
35 the street, with landscaping or walkways (instead of parking) comprising most of their  
36 street frontage. Transit-supportive development corridors and mixed-use centers  
37 contribute to a compact, accessible land use pattern.



38 *Regular street grid of city blocks and alleys in Mountain View.*

39  
40



*More relaxed street grid along a transit corridor in Spenard.*

**2. Applicability.**

The urban neighborhood development context area shall apply as a basis for area-specific development standards in this chapter 21.07. It shall not be used as a basis to change allowed uses or review and approval procedures of the underlying zoning district, district-specific dimensional standards, or any other standards elsewhere in this title.

**3. Urban Neighborhood Development Context Area Established.**

The urban neighborhood development context area is delineated in map 21.07-1, with the following clarifications:

- a. Birchwood Park, DeBarr Vista, and Green Acres Subdivisions (and Additions) are included (marked “A” on map).
- b. The Downtown (DT) zoning districts are not included. The university and medical campuses of the UMED District are also not included.
- c. The properties of Merrill Field Airport, Alaska Railroad Terminal Reserve, and Port of Alaska are not included.
- d. Broadmore Estates, Bentzen Addition, Creekside Park, Town Square, Tudor Park, University Park, and Windemere Subdivisions (and their Additions) are not included (marked “B” on map).



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1 **CHAPTER 21.07: DEVELOPMENT AND DESIGN STANDARDS**

2 \*\*\* \*\*

3 **21.07.060 Transportation and Connectivity**

4 \*\*\* \*\*

5 **E. Standards for Pedestrian Facilities**

6 \*\*\* \*\*

7 **2. Sidewalks**

8  
9 a. All sidewalks shall be designed to comply with the standards of the Design Criteria Manual (DCM) and Municipality of Anchorage Standard Specifications (MASS) and shall be improved in accordance with subsection 21.08.050H.

10  
11 b. Street improvement projects and new streets in subdivisions [I]in all class A zoning districts except for industrial districts[,] shall install sidewalks [SHALL BE INSTALLED ]on both sides of all streets (local, collector, arterial, public or private, including loop streets). Where indicated in the comprehensive plan, a pathway may replace a sidewalk on one side. Street improvement projects [I]in industrial zoning districts shall install[,] a sidewalk [SHALL BE INSTALLED ]on one side of all local streets, and on both sides of local streets if the new sidewalks would connect to existing sidewalks on both ends and the needed sidewalk length is no greater than one quarter mile.

12 \*\*\* \*\*

13  
14 f. Development on lots along existing streets in class A zoning districts shall install sidewalks on all lot frontages abutting streets in the following situations:

15 i. In the R-4, R-4A, and commercial zoning [DISTRICTS] districts.

16 ii. In the DT zoning districts as set forth in the provisions of chapter 21.11, *Downtown*.

17 \*\*\* \*\*

18  
19 **4. On-Site Pedestrian Walkways**

20 \*\*\* \*\*

21 **b. On-Site Pedestrian Connections**

22 \*\*\* \*\*

23 ii. All primary building entrances on a site shall be connected to the street by a convenient system of walkways. This includes multiple primary entrances into one building, and primary entrances in separate buildings on a site.

24 \*\*\* \*\*

25  
26 iv. The primary front entrance of a residential dwelling shall be connected to the street by a walkway as provided in i. through ii. above, or by the dwelling's individual driveway, or by a shared parking courtyard meeting 21.07.060G.21.

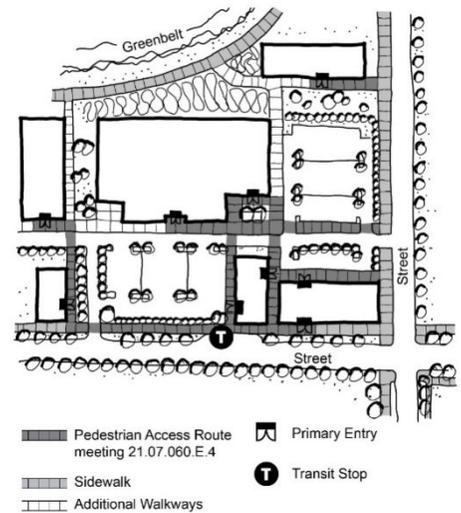
27 \*\*\* \*\*

28 **c. Walkway Clear Width and Improvements**

29 \*\*\* \*\*

30 i. The minimum width of a required pedestrian walkway shall be five feet of unobstructed clear width, [EXCLUDING VEHICULAR OVERHANG, ]except where otherwise stated in this title. A walkway that provides access to no more than four residential dwelling units may provide an unobstructed clear width of three feet.

31 ii. Walkways shall be improved in accordance with subsection 21.08.050H.



1 \*\*\* \*\*

2 **F. Pedestrian Frontage Standard**

3  
4 **1. Purpose**

5 The pedestrian frontage standard requires site planning and building orientation toward  
6 neighborhood streets and sidewalks to facilitate pedestrian access and reduce automobile parking  
7 congestion. Objectives include to:

- 8
- 9 **a.** Organize and orient buildings around public streets and associated frontages in a way that  
10 frames streets as positive public space, promotes pedestrian activity, and connects to  
11 multiple modes of transportation.
- 12
- 13 **b.** Provide clearly defined, safe pedestrian access to building entries that invites people of all  
14 abilities and minimizes conflicts with vehicles and parking.
- 15
- 16 **c.** Place active indoor spaces, entrances, and windows on street-facing building facades to  
17 improve the visual connection to the street and promote a safe, secure neighborhood.
- 18

19 **2. Applicability**

20 Subsection 4. applies to development in the urban neighborhood development context area  
21 established in 21.07.010E. Subsection 5. applies to development in the other areas of the  
22 municipality. The following are exempt:

- 23
- 24 **a.** Changes of use and other developments that comprise building modifications of less than  
25 50 percent of the total improvement value of the building(s) on the site.
- 26
- 27 **b.** Development in Girdwood, Downtown (DT), CE-DO, CE-EVO, PLI, PR, TA, or industrial  
28 zoning districts.
- 29
- 30 **c.** Single-family and two-family dwellings constructed prior to January 1, 2016, or on lots of  
31 20,000 square feet or larger, or in Class B zoning districts.
- 32
- 33 **d.** Uses without habitable floor area, such as utility substations.
- 34

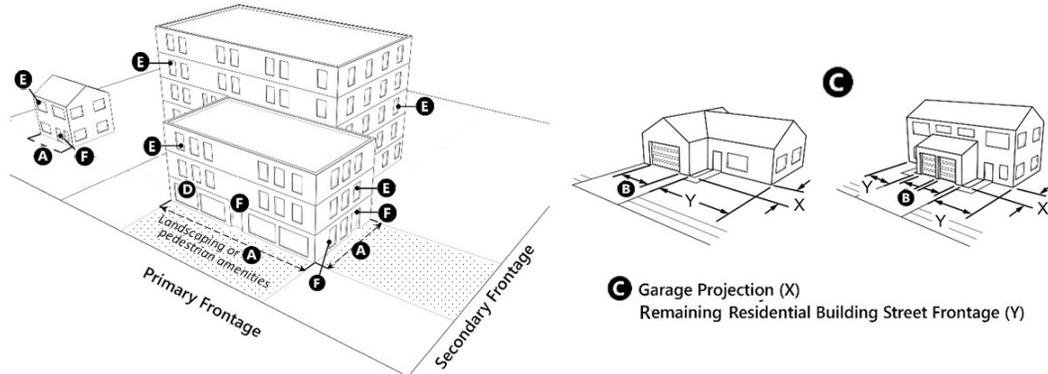
35 **3. Administrative Adjustments**

- 36
- 37 **a.** The Alternative Equivalent Compliance procedure in 21.07.010D. may be used to propose  
38 alternative means of complying with the intent of this section.
- 39
- 40 **b.** The director shall approve administrative relief if the applicant demonstrates the adjustment  
41 is necessary to compensate for some practical difficulty of the site. This includes reducing  
42 the window area requirement by the amount needed to comply with 5-star or affordable  
43 housing (21.15.040) energy rating requirements. The department shall keep record of the  
44 approved exception with written findings supporting the exception on file.
- 45
- 46

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4. **Standard or urban neighborhood contexts**

The standards of table 21.07-2 apply to the primary frontage and one secondary frontage.



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Table 21.07-2: Pedestrian Frontage Standard for Urban Neighborhood Contexts	
Site Elements for Pedestrian-Oriented Frontages	Standard
A. Minimum required ground-floor, street-facing building elevation with on-site walkways, pedestrian amenities, or landscaping in front—and no off-street automobile parking or circulation	50% of building elevation width <sup>1</sup> Lots with alley access: no parking allowed in front of building except in driveways for individual dwellings
B. Maximum allowed width of residential garage entrance(s) on ground-floor street-facing building elevation	40% of building elevation width <sup>1</sup>
C. Maximum allowed distance a garage may project out in front of the rest of the street-facing residential building elevation	No more than 50% of the width of the non-garage portion of the street-facing building, on other lots
D. Minimum required visual access windows or primary entrances on non-residential ground-floor street-facing building elevation <sup>2</sup>	25% of wall area on primary frontage 15% of wall area on second frontage
E. Minimum required windows or primary entrances on residential and upper-floor non-residential street-facing elevations <sup>2</sup>	15% of wall area on primary frontage 10% of wall area on second frontage
F. Requirement for primary entrance facing the street or visible from the street via an unobstructed line of sight	Residential: Meet 21.07.060G.14., Covered, Visible Residential Entrance Other uses: at least one entrance
G. Minimum required number of pedestrian amenities from 21.07.060G. (in addition to G.14.) in multifamily, mixed-use, townhouse, group housing, and non-residential developments	2 pedestrian amenities
<sup>1</sup> Exception: On lots without alley access in R-2A, R-2D, and R-2M zoning districts, two-family (duplex) developments may have a minimum of 20% in A. above and a maximum of 67% in B. above. <sup>2</sup> Visual access windows shall have a sill height of no more than four feet above finished grade. Rules for measuring window area as a percentage of building wall area are provided in 21.15.020P. Visual access windows ("Window, providing visual access"), primary entrance, and ground-floor wall area are defined in 21.15.040.	

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- 5. **Standard for areas outside of urban neighborhood contexts**  
The standards of table 21.07-3, apply to the primary frontage and one secondary frontage.

<b>Table 21.07-3: Pedestrian Frontage Standard – Outside of Urban Neighborhood Contexts</b>	
Site Elements for Pedestrian-Oriented Frontages	Standard
A. Minimum required ground-floor, street-facing building elevation with on-site walkways, pedestrian amenities, or landscaping in front—and no off-street automobile parking or circulation <sup>1</sup>	20% of building elevation width, on one frontage
B. Maximum allowed width of residential garage entrance(s) on ground-floor street-facing building elevation	67% of building elevation width
C. Maximum allowed distance a garage may project out in front of the rest of the street-facing residential building elevation	No more than the width of the non-garage portion of the street-facing building elevation
D. Minimum required visual access windows or primary entrances on non-residential ground-floor street-facing building elevation <sup>1,2</sup>	15% on primary frontage 10% on secondary frontage
E. Minimum required windows or primary entrances on residential and upper-floor non-residential street-facing elevations <sup>1,2</sup>	10% on primary frontage 5% on secondary frontage
F. Requirement for primary entrance facing the street or visible from the street via an unobstructed line of sight <sup>1</sup>	Residential: Meet 21.07.060G.14., Covered, Visible Residential Entrance. Other uses: at least one entrance.
G. Minimum required number of pedestrian amenities from 21.07.060G. (in addition to G.14.) in multifamily, mixed-use, townhouse, group housing, and non-residential developments.	1 pedestrian amenity
<sup>1</sup> Exception: Non-residential buildings located more than 100 feet from the applicable street rights-of-way are exempt from A., D., E., and F. above. <sup>2</sup> Visual access windows shall have a sill height of no more than four feet above finished grade. Rules for measuring window area as a percentage of building wall area are provided in 21.15.020P. Visual access windows (“window, providing visual access”), primary entrance, and ground-floor wall area are defined in 21.15.040.	

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**G[F]. Pedestrian Amenities Menu**

- 1. **Purpose**  
This section defines and provides standards for pedestrian amenities that are used in developments to satisfy a requirement, menu choice, or incentive in this title. The standards in this section give predictability for applicants and the public for the minimum acceptable standards for pedestrian amenities. It encourages amenities that will improve and enhance the community and respond to Anchorage’s northern latitude climate.
- 2. **Applicability**  
Pedestrian amenities shall meet the minimum standards of this section to be credited toward a requirement, menu choice, or [AS A SPECIAL FEATURE BONUS ]incentive of this title, except where specifically provided otherwise in this title. The standards of this section do not apply to amenities that are not counted toward a requirement, menu choice, or incentive under this title.

1  
2 **3. Administrative Adjustments**

3 The alternative equivalent compliance procedure set forth in subsection 21.07.010D. may be used  
4 to propose alternative means of complying with the standards of this subsection 21.07.060G.  
5

6 *(Note: Current subsection 3., Walkway, is deleted.)*

7 **[3. WALKWAY]**

8 [A WALKWAY IS A SURFACE THAT CONNECTS TWO POINTS FOR PEDESTRIAN USE, AS  
9 DEFINED IN CHAPTER 21.15. A WALKWAY MAY BE IN A PUBLICLY DEDICATED  
10 PEDESTRIAN EASEMENT. EXAMPLES INCLUDE PEDESTRIAN CONNECTIONS WITHIN ONE  
11 DEVELOPMENT SITE, MID-BLOCK, BETWEEN SUBDIVISIONS, OR LEADING FROM  
12 STREETS TO PUBLIC AMENITIES, SUCH AS SCHOOLS OR PARKS.]

13 [A. A WALKWAY SHALL HAVE A MINIMUM UNOBSTRUCTED CLEAR WIDTH OF FIVE  
14 FEET, EXCEPT WHERE OTHERWISE STATED IN THIS TITLE. A WALKWAY THAT  
15 PROVIDES ACCESS TO NO MORE THAN FOUR RESIDENTIAL DWELLING UNITS  
16 MAY HAVE AN UNOBSTRUCTED CLEAR WIDTH OF THREE FEET.]

17 [B. WALKWAYS SHALL BE IMPROVED IN ACCORDANCE WITH SUBSECTION  
18 21.08.050H.] **Chapter 21.07: Development and Design Standards**

19  
20 **4[4]. Enhanced On-Site[PRIMARY PEDESTRIAN] Walkway**

21 An enhanced on-site[PRIMARY PEDESTRIAN] walkway is intended to provide an option for  
22 applicants to receive credit for exceeding the minimum development standards for walkways and  
23 improving pedestrian convenience, comfort, and safety on the site. Enhanced on-site[PRIMARY  
24 PEDESTRIAN] walkways provide additional width [AN UNOBSTRUCTED CLEAR WIDTH OF AT  
25 LEAST EIGHT FEET] for pedestrian movement [WITH ADDITIONAL SPACE INCORPORATING  
26 FEATURES ALONG THE WALKWAY SUCH AS STOREFRONT SIDEWALK SPACE, ROOM FOR  
27 RESIDENTIAL STOOPS OR BUILDING FOUNDATION PLANTINGS,] and peripheral space that  
28 accommodates landscaping, furniture, and utilities. [AS ESTABLISHED GENERALLY IN  
29 SUBSECTION F.1 AND F. 2 ABOVE, THE STANDARDS OF THIS SUBSECTION APPLY ONLY  
30 WHERE THE SPECIFIC TERM "PRIMARY PEDESTRIAN WALKWAY" IS LISTED AS A  
31 REQUIREMENT, MENU CHOICE, OR SPECIAL FEATURE THAT COUNTS TOWARD A BONUS.  
32 THIS SUBSECTION IS NOT A GENERALLY APPLICABLE REQUIREMENT FOR OTHER LARGE  
33 WALKWAYS.]

34  
35 **a.** An enhanced on-site walkway shall have a pedestrian movement zone with a continuous,  
36 unobstructed walkway clear width of at least eight feet, or six feet where providing access  
37 only to four or fewer residential dwelling units. Where adjacent to a ground-floor building  
38 elevation it shall also have a building interface zone a minimum of two feet in width for  
39 building foundation landscaping or space for opening doors or seating and transition  
40 pedestrian spaces. In addition to the pedestrian movement zone and any building interface  
41 zone, the enhanced on-site walkway shall have a buffer space of at least two feet in width  
42 where abutting motor vehicle parking lots, circulation aisles, or driveways. The buffer space  
43 shall accommodate landscaping beds, fencing or bollards, light poles, utilities, benches,  
44 and other furnishings [OBJECTS TO BE KEPT CLEAR OF THE WALKWAY].

45  
46 **b.[C].** A minimum of two pedestrian features as defined by this title (21.15.040) shall be provided  
47 along the enhanced walkway, with at least one for every 50 feet of the walkway length. [AT  
48 LEAST ONE PEDESTRIAN FEATURE AS DEFINED BY THIS TITLE SHALL BE  
49 PROVIDED FOR EVERY 50 FEET OF LENGTH ALONG A PRIMARY PEDESTRIAN  
50 WALKWAY].

51 **[D.]** A PRIMARY PEDESTRIAN WALKWAY SHALL BE ILLUMINATED WITH PEDESTRIAN  
52 SCALE LIGHTING.]

53 **c[E].** [A]Enhanced on-site[PRIMARY PEDESTRIAN] walkways shall provide continuous, direct[LY]  
54 connections from building primary entrance(s) to surrounding public streets and sidewalks

and be publicly accessible or available to all residents of the development.**[AT ALL TIMES].**

\*\*\*      \*\*\*      \*\*\*

(Note: Pedestrian Amenities G.5. through G.12. are not shown because they are not being amended.)

**[15. BICYCLE PARKING FACILITIES.]**

**[A. REQUIRED BICYCLE PARKING OR A SIGN LEADING THERETO SHALL BE LOCATED IN AN AREA VISIBLE FROM A PRIMARY ENTRANCE AREA AND NO FARTHER FROM A PRIMARY ENTRANCE THAN THE CLOSEST MOTOR VEHICLE PARKING SPACE, NOT INCLUDING DESIGNATED ACCESSIBLE PARKING, CARPOOL, OR VANPOOL SPACES. IT MAY ALSO BE LOCATED INSIDE THE BUILDING SERVED, IN A LOCATION THAT IS EASILY ACCESSIBLE FOR BICYCLES.]**

**[B. A REQUIRED BICYCLE PARKING SPACE SHALL INCLUDE A SECURELY FIXED STRUCTURE THAT ALLOWS THE BICYCLE WHEEL AND FRAME TO BE LOCKED TO THE FACILITY, AND THAT SUPPORTS THE BICYCLE FRAME IN A STABLE POSITION WITHOUT DAMAGE TO THE BICYCLE, OR SHALL BE IN A BICYCLE LOCKER, LOCKABLE BICYCLE ENCLOSURE, OR LOCKABLE ROOM.]**

**[C. A REQUIRED BICYCLE PARKING SPACE SHALL BE A MINIMUM OF SIX FEET LONG AND TWO FEET WIDE.]**

**[D. THE SURFACING OF BICYCLE PARKING FACILITIES SHALL BE DESIGNED AND MAINTAINED TO BE CLEAR OF MUD AND SNOW.]**

**[E. BICYCLE PARKING SHALL NOT OBSTRUCT PEDESTRIAN WALKWAYS, BUILDING ACCESS, OR USE AREAS.]**

**13. Separated Walkway to the Street**

The development shall connect the building primary entrances to the street with a clear and direct walkway that is not routed through a parking facility or across vehicle driveways or circulation aisles. The minimum clear width portion of the walkway shall be separated from the parking facility by at least five feet.

**14. Covered, Visible Residential Entrance**

A porch, stoop, or landing sheltered by a roof is intended to give visual emphasis to the building entrance as an aid in wayfinding, and help provide safe, convenient access to residential buildings from the street. The entrance shall meet the following standards:

- a. The entrance shall incorporate a porch, stoop, or landing with an internal dimension of at least 16 square feet, and a permanent, sheltering roof covering at least 12 square feet.
- b. The porch, stoop, or landing shall be distinguished from adjoining areas and vehicle parking by vertical separation or a change in surfacing material.
- c. The building entrance shall also be visible (via an unobstructed line of sight) from a street or face a common private open space (21.07.030), a plaza or courtyard (21.07.060G.6.), a housing courtyard (21.07.060G.7.), or a shared parking courtyard (21.07.060G.21.) that is visible from a street.

**15. Enhanced Primary Entrance**

An enhanced building entry is intended to provide an option for applicants to receive credit for a more prominent and inviting primary pedestrian entrance. To receive credit, the enhanced primary entrance shall incorporate at least three of the following features:

- a. Outdoor sheltering roof feature projecting from the building façade such as an overhang, portico, canopy, marquee with an inside dimension of at least 16 square feet;

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- b. Recessed and/or projected entrance or other building wall modulation with projections or recesses in the building wall plane;
- c. Changes in the building's main roofline such as arches, peaked roof forms, or terracing parapets;
- d. Changes in siding material or exterior finishes, or façade detail features such as tilework that emphasize the entrance;
- e. Entrance plaza, patio, or similar common private open space;
- f. Landscaping not otherwise required by this title, such as integrated planters, landscape accent lighting, or special paving treatments; or
- g. One or more pedestrian features (21.15.040) such as pedestrian-scale lighting or seating.

**16. Enhanced Façade Transparency.**  
Increase the windows and/or primary entrances on street-facing building elevations which are subject to the window requirement in 21.07.060F., by an additional five (5) percent of the wall area.

**17. Pedestrian Frontage Free of Parking in Front.**  
In urban neighborhood development contexts (21.07.010E.), provide a street frontage without driveways, parking, or loading facilities in front of at least 75% of the ground-floor, street-facing building elevation. In other areas, provide such a frontage in front of at least 33% of the street-facing building elevation.

**18[16]. Site Entry Feature[USE]**  
Highlight and define a pedestrian entrance to a development site using three or more of the following elements:

[A. A PEDESTRIAN-INTERACTIVE USE SHALL BE ANY OF THE FOLLOWING USES THAT ARE PERMITTED IN THE DISTRICT: RETAIL AND PET SERVICES; FINANCIAL INSTITUTION PROVIDING BANKING SERVICES OPEN TO THE PUBLIC WITH AT LEAST ONE EMPLOYEE ON SITE; FOOD OR BEVERAGE SERVICE; PERSONAL SERVICE; CULTURAL FACILITY; OR THE FRONTAGE OF ENTRYWAYS OR STAIRWAYS THROUGH WHICH SUCH USES ARE PRINCIPALLY ACCESSED;]

[B. RETAIL SALES USES THAT ARE PERMITTED IN THE DISTRICT SHALL BE CONSIDERED PEDESTRIAN-INTERACTIVE USES, EXCEPT FOR THE FOLLOWING TYPES OF RETAIL SALES USES: FUELING STATION; BUILDING MATERIALS STORE.]

[C. THE FOLLOWING USES SUPPORTING RESIDENTIAL NEIGHBORHOOD AND HOUSING DEVELOPMENT ARE ALSO CONSIDERED PEDESTRIAN-INTERACTIVE USES WHEN PERMITTED IN THE DISTRICT: RESIDENTIAL DWELLINGS WITH INDIVIDUAL FRONT ENTRIES ALONG THE STREET; ELEMENTARY SCHOOL; MIDDLE OR HIGH SCHOOL; HEALTH SERVICES; CHILD CARE CENTER.]

a[D]. [USE]Landscape treatment with seasonal color and trees, which clearly distinguishes and highlights the site entry.

b[E]. Plaza or courtyard as described in subsection 21.07.060G.6.

[F. A PEDESTRIAN-INTERACTIVE USE SHALL COMPLY WITH SUBSECTION 21.06.030C.5., MAXIMUM SETBACKS, BUT THE EXCEPTIONS OF SUBSECTION 21.06.030C.5.D. SHALL NOT BE AVAILABLE.] c[G]. Identifying building primary entrance form including a covered entry, when the primary entrance is within 50 feet of the site entrance. [, EXCEPT THAT SUCH]

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2 d. Special paving, pedestrian walkway area lighting, ornamental lighting, and/or bollards.

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4 e[H]. Ornamental gate and/or fence.

5  
6 **19[16]. Pedestrian-Interactive Building[USE]**

7 A pedestrian-interactive building[USE] is intended to provide interior[GROUND-FLOOR] spaces  
8 that [STRONGLY]engage the sidewalk with street-facing windows and entrances and [THAT  
9 FEATURE] activities and services that support neighborhood residents, and [GENERALLY  
10 ]contribute to the activity level and quality of the pedestrian[-ORIENTED] environment of the  
11 neighborhood or district.

12  
13 [A. A PEDESTRIAN-INTERACTIVE USE SHALL BE ANY OF THE FOLLOWING USES THAT  
14 ARE PERMITTED IN THE DISTRICT: RETAIL AND PET SERVICES; FINANCIAL  
15 INSTITUTION PROVIDING BANKING SERVICES OPEN TO THE PUBLIC WITH AT LEAST  
16 ONE EMPLOYEE ON SITE; FOOD OR BEVERAGE SERVICE; PERSONAL SERVICE;  
17 CULTURAL FACILITY; OR THE FRONTAGE OF ENTRYWAYS OR STAIRWAYS THROUGH  
18 WHICH SUCH USES ARE PRINCIPALLY ACCESSED;]

19 [B. RETAIL SALES USES THAT ARE PERMITTED IN THE DISTRICT SHALL BE CONSIDERED  
20 PEDESTRIAN-INTERACTIVE USES, EXCEPT FOR THE FOLLOWING TYPES OF RETAIL  
21 SALES USES: FUELING STATION; BUILDING MATERIALS STORE.]

22 [C. THE FOLLOWING USES SUPPORTING RESIDENTIAL NEIGHBORHOOD AND HOUSING  
23 DEVELOPMENT ARE ALSO CONSIDERED PEDESTRIAN-INTERACTIVE USES WHEN  
24 PERMITTED IN THE DISTRICT: RESIDENTIAL DWELLINGS WITH INDIVIDUAL FRONT  
25 ENTRIES ALONG THE STREET; ELEMENTARY SCHOOL; MIDDLE OR HIGH SCHOOL;  
26 HEALTH SERVICES; CHILD CARE CENTER.]

27 a[D]. A pedestrian-interactive building[USE] shall provide a primary entrance facing the street.  
28 Entrances at building corners facing a street may be used to satisfy this requirement.

29  
30 b[E]. A pedestrian-interactive building[USE] shall contain habitable floor area at least 24 feet  
31 deep extending along a minimum of 50 percent of the [FULL ]length of the ground-floor,  
32 street-facing building elevation in Urban Neighborhood Contexts, and 30 percent  
33 elsewhere. The habitable floor area may include[ , ALLOWING FOR] pedestrian [AND  
34 VEHICLE]entrances, entry lobbies or atriums, and stairwells.

35  
36 [F. A PEDESTRIAN-INTERACTIVE USE SHALL COMPLY WITH SUBSECTION  
37 21.06.030C.5., MAXIMUM SETBACKS, BUT THE EXCEPTIONS OF SUBSECTION  
38 21.06.030C.5.D. SHALL NOT BE AVAILABLE.] c[G]. At least 50 percent of the street-  
39 facing building elevation width of a pedestrian-interactive building shall have no parking  
40 facilities in front.[, EXCEPT THAT SUCH]

41  
42 d. Street-facing wall areas of a non-residential use shall be 50 percent visual access windows  
43 or primary entrances on the ground floor, and 20 percent windows above the ground floor.

44  
45 e[H]. Street-facing wall areas for residential uses shall be at least 20 percent windows or primary  
46 entrances.

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48 f[H]. Where a building has three or more street frontages, these criteria apply along only two of  
49 the frontages.

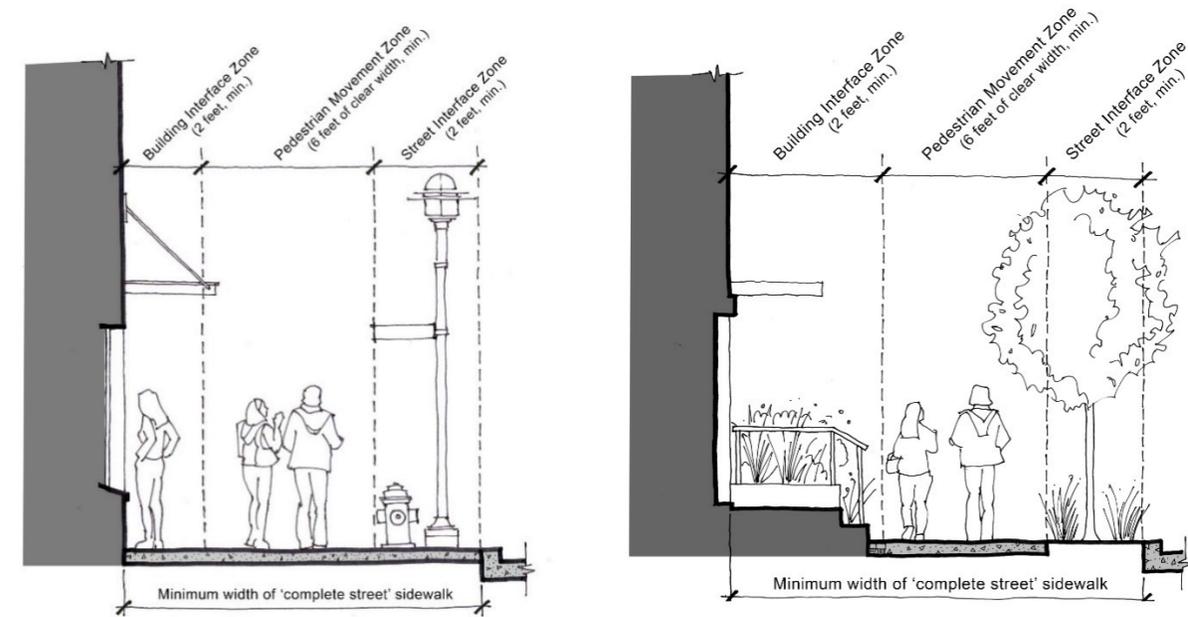
50  
51 **20[17]. Enhanced Street Sidewalk[ OPTION]**

52 An enhanced street sidewalk is intended to provide an option for applicants to receive credit for  
53 exceeding the minimum development standards for sidewalks. An enhanced street sidewalk  
54 promotes sidewalk widening and streetscape enhancements to support higher levels of pedestrian  
55 activity, comfort, and safety in the district[ AND ACCESS IN MIXED-USE DEVELOPMENTS]. An

1 enhanced street sidewalk with “complete[MAIN] street” (21.15.040) style amenities may be  
2 provided in lieu of required site perimeter landscaping [WHERE IT IS LOGICAL TO SUPPORT A  
3 PEDESTRIAN ZONE ]as determined through an administrative site plan review, and subject to the  
4 following:

- 5  
6 a. The enhanced street sidewalk cross-section[WIDTH] shall be at least 12 feet wide, and  
7 include a pedestrian movement zone, building interface zone, and street interface zone  
8 (21.15.040). The pedestrian movement zone shall have a walkway clear width of at least  
9 six feet. The street interface zone shall be at least two feet wide from back-of-curb, and  
10 four feet wide along major arterials. Where a building adjoins the sidewalk, there shall be  
11 a building interface zone at least two feet wide..[FEATURE A PRINCIPAL BUILDING WITH  
12 A 20-FOOT MAXIMUM SETBACK IN COMPLIANCE WITH SUBSECTION 21.06.030C.5.]  
13

14 (Note: Existing illustration replaced with the two illustrations below.)



Enhanced Street Sidewalks in [A]Commercial and Residential Settings

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18  
19 [I. A PUBLIC USE EASEMENT SHALL BE RECORDED FOR ANY PART OF THE  
20 DESIGNATED SIDEWALK TO BE LOCATED WITHIN THE SUBJECT PARCEL.]  
21 [II. PHYSICAL OBSTRUCTIONS WITHIN THE SIDEWALK'S BUILDING  
22 INTERFACE ZONE, SUCH AS LANDSCAPING, ENTRY STOOPS, OR  
23 SEATING, SHALL EXTEND NO MORE THAN TWO FEET INTO THE MINIMUM  
24 REQUIRED 12 FOOT WIDTH, SO THAT AT LEAST TEN FEET REMAIN.]

- 25 b. The enhanced street sidewalk shall provide at least half the number of trees and shrubs  
26 that would otherwise have been required for site perimeter landscaping. Shrubs are not  
27 required if perennials are substituted for shrubs on a three to one basis or if the tree planting  
28 bed is provided as a suspended pavement system with a minimum of 300 cubic yards of  
29 soil per tree.  
30  
31 c. The enhanced street sidewalk may be placed wholly or in part within a right-of-way, subject  
32 to approval of the traffic engineer and municipal engineer.  
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i. The enhanced street sidewalk shall be subject to the applicable requirements of title 24, including sections 24.30.020., *Permit to use Public Places*, and 24.90, *Encroachment Permit*.

ii. IMPROVEMENTS WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSISTENT WITH THE DCM AND MASS. Existing improvements that meet the standards of the enhanced street sidewalk may be counted towards the requirements of this section, subject to approval by the director.

iii. The owner shall maintain landscaping and amenities for the enhanced street sidewalk within the right-of-way. AND COMPLY WITH THE PROVISIONS FOR REMOVAL OF SNOW AND ICE IN AMC 24.80.090, 100, AND 110.

iv. Where the right-of-way is not adequate or cannot be configured to accommodate the enhanced street sidewalk, then the development shall be set back from the street frontage as necessary to accommodate part of the improvements within the property. A public use easement shall be recorded for any part of the designated sidewalk width to be located within the subject parcel.

**20[17]. Shared Parking Courtyard[ OPTION]**

\*\*\* \*\*

1 **21.07.090 Off-Street Parking and Loading**

2 \*\*\* \*\*

3 **A. Purpose**

4 \*\*\* \*\*

- 5 5. Manage the width and frequency of driveways along street frontages to reduce conflicts with traffic
- 6 and provide space for street snow clearing and storage, on-street parking (where appropriate), and
- 7 pedestrian facilities.

8 \*\*\* \*\*

9  
10 **H. Parking and Loading Facility Design Standards**

11 \*\*\* \*\*

- 12 2. *Applicability.* These standards apply to all areas used for off-street parking, and any parking facility
- 13 or loading facility in a development, except where stated otherwise. A temporary parking lot shall
- 14 comply with all applicable development requirements of this title for surface parking lots and parking
- 15 lot landscaping, except when associated with another temporary use permitted pursuant to section
- 16 21.05.080.

17 \*\*\* \*\*

18 **8. Vehicular Access and Circulation – General**

19 Parking lots and structures access, layout, and dimensions shall be designed for a safe and orderly  
20 flow of traffic throughout the site, as provided in [THE] subsections 9. through 12. that follow.  
21 Vehicular access and circulation patterns and the location and traffic direction of all circulation  
22 aisles, driveways, and queuing lanes shall be designed and maintained in accordance with  
23 accepted principles of traffic engineering and safety as determined by the traffic engineer.

24  
25 *(Note: Existing subsection 8.a. Key Elements, 8.b. Circulation Patterns, and 8.c., Parking Spaces*  
26 *Along Major Entrance Drives, are deleted.)*

27  
28 **[C. PARKING SPACES ALONG MAJOR SITE ENTRANCE DRIVES]**

29 [THE PROVISION, LOCATION, DESIGN, AND DIMENSIONS OF PARKING SPACES ON  
30 A MAJOR ACCESS DRIVEWAY THAT SERVES AS AN ENTRY OR EXIT FOR A LARGE  
31 ESTABLISHMENT WITH MULTIPLE LOTS, TRACTS, OR BUSINESSES, SHALL  
32 CONFORM TO MUNICIPAL STANDARDS FOR ON-STREET PARKING AND BE  
33 SUBJECT TO REVIEW AND APPROVAL BY THE TRAFFIC ENGINEER.]

34 **9. Vehicular Access Location**

35  
36 **a. Street Access Location**

37 The number and spacing of driveways, including minimum distance from street  
38 intersections, shall be as provided in the *Municipal Driveways Standards* and this section.  
39 Access to streets owned by the state of Alaska requires compliance with state driveway  
40 standards, department of transportation and public facilities approval and driveway permit.

41  
42 **b. Alley Access Requirement**

43 Where a residential use is served by an alley in the Urban Neighborhood Development  
44 Context Area (21.07.010E.), vehicle access shall take place from the alley, except for direct  
45 vehicle access to the street that is limited to the following:

- 46  
47 i. One driveway no wider than 12 feet at any point, except that lot frontages 100 feet
- 48 or wider on a primary street frontage may have one driveway per 50 feet of lot
- 49 frontage on that street.
- 50  
51 ii. One additional driveway no wider than 12 feet providing access to the secondary
- 52 street frontage on corner lots for multifamily, townhouse, or two-family
- 53 developments.

1                   iii.       The director and traffic engineer may approve an administrative variance from the  
2                   requirements of this subsection, as provided in 21.03.240J.7.  
3

4                   c.       **Cross-Access and Shared Access with Adjacent Sites**

5                   Parking facilities serving a site, whether located on that same lot or on an adjacent lot, may  
6                   be connected by means of a common access driveway within or between the interior of  
7                   such lots. Where a property receives its access to the street through a shared driveway  
8                   with another lot, a shared access easement shall be provided on the plat, or a shared  
9                   access agreement running with the land shall be recorded by the municipality, as approved  
10                  and executed by the director, guaranteeing the continued availability of the shared access  
11                  between the properties.  
12

13                  d.       **Paired Residential Driveways Allowed**

14                  Driveways for two single-family, two-family, or townhouse units may be attached across a  
15                  property line, provided the maximum combined width of the driveway is 28 feet and other  
16                  standards of this section are met.  
17

18                  e.       **Garage Door Setbacks**

19                  Street-facing garage bay doors that are proposed to be set back from the public right-of-  
20                  way by less than the length of a standard parking space (as defined in table 21.07-8,  
21                  *Parking Angle, Stall, and Aisle Dimensions*) shall be reviewed and approved by the traffic  
22                  engineer, unless there is a sign posted for “no parking”. Garages shall be set back from  
23                  public rights-of-way including alleys by at least four feet unless the traffic engineer  
24                  approves otherwise, to ensure adequate sight distance for turning and maneuvering.  
25

26                  10.       **Access to Parking Spaces**  
27

28                  a.       **Access to Parking Spaces**

29                  Each parking space shall open to a parking aisle or driveway of such width and design as  
30                  provided in section 21.07.090H.12. to provide safe and efficient means of vehicular access  
31                  with no more than a standard two-movement entrance or exit from the parking space and  
32                  without having to move another vehicle. Stacked and tandem parking spaces are allowed  
33                  exceptions as provided in 21.07.090H.12.  
34

35                  b.       **Backing Distances**

36                  Adequate ingress and egress to each parking space shall be provided without backing  
37                  more than 25 feet.  
38

39                  c.       **Dead-End Parking Aisles**

40                  Any parking bay that does not provide two means of vehicle egress shall provide, at the  
41                  closed end, an extension of the parking aisle at least nine feet in depth, designated and  
42                  signed as a “No Parking” turn-around area. Dead-end parking bays longer than 100 feet  
43                  shall be subject to discretionary review and approval by the traffic engineer.  
44

45                  d.       **Vehicle Maneuvering**

46                  Off-street parking facilities shall be designed so that all vehicle maneuvers occur on the  
47                  development site and not in the public right-of-way, and vehicles enter and exit the right-  
48                  of-way in a forward motion, except the following are exempt:  
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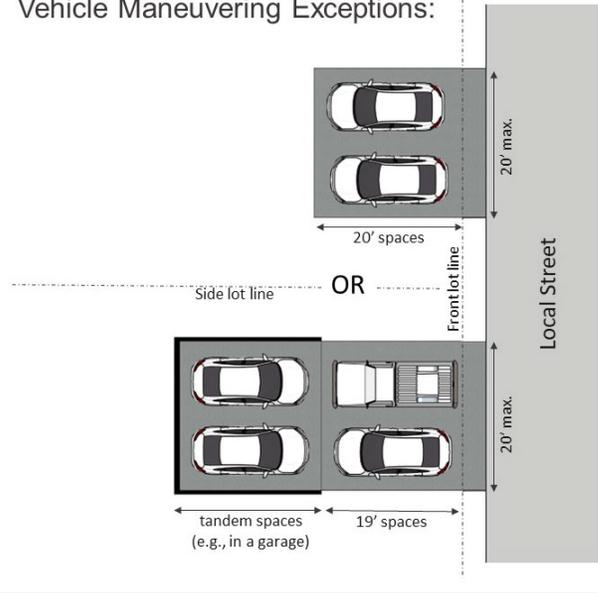
50                  i.        Parking for single-family, two-family, and mobile home dwellings on individual lots,  
51                  accessing a local street.  
52

53                  ii.       For other developments, parking comprising only one or two parking spaces whose  
54                  only access is to a local street, provided that the vehicle area occupies no more  
55                  than 20 feet of the lot frontage in the front setback. The number of spaces may be  
56                  increased to four spaces if arranged in tandem for residential dwellings as provided

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in 21.07.090K.12.k. Additional spaces for multifamily developments with up to four dwelling units may be approved by the traffic engineer in appropriate circumstances such as lots on dead-end streets, cul-de-sacs, or other local streets with low traffic volumes.

Vehicle Maneuvering Exceptions:



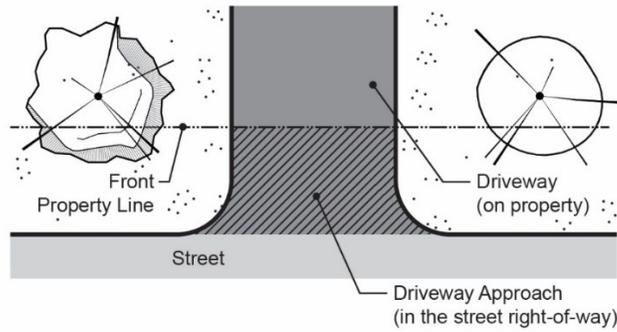
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- iii. The usable portion of an alley may be credited as circulation and/or parking aisle space for parking areas. The maneuvering area between the end of the parking space and the opposite side of the improved alley shall meet the parking aisle width in table 21.07-8, *Parking Angle, Stall, and Aisle Dimensions*.

11. Driveway Design and Dimensions

a[D]. Parking Lot Entries/Driveway Approaches

Entries and driveway approaches providing access from the street edge to the front property line[TO PARKING LOTS] shall conform to the municipal driveway standards [CURRENTLY ESTABLISHED BY THE TRAFFIC ENGINEER]and this section 21.07.090H.11. Access to streets owned by the state of Alaska requires compliance with state driveway standards as provided in 21.07.090H.9.[DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES APPROVAL, AND A CURRENT VALID STATE OF ALASKA DRIVEWAY PERMIT. INGRESS AND EGRESS TO PARKING FACILITIES SHALL BE DESIGNED TO MAINTAIN ADEQUATE SIGHT DISTANCE AND SAFETY AND AS PRESCRIBED IN THE MUNICIPAL DRIVEWAY STANDARDS.]



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**b. Curb Openings and Public Walkway Crossings**

- i. Curb cut and curb returns at driveway openings to the street shall be provided as prescribed in the municipal driveway standards.
- ii. Public walkways shall be maintained or restored to the maximum running slope and cross-slope prescribed by M.A.S.S. and A.D.A., except that in the urban neighborhood context areas (21.07.010E.), public walkways on local streets shall be restored to a level running grade to the extent reasonably feasible.

**c. Driveway Approach (in ROW) as a Percentage of Lot Frontage Width**

**[I. RESIDENTIAL USES]**

[RESIDENTIAL DRIVEWAY ENTRANCES SHALL COMPLY WITH SUBSECTION 21.07.110F.3., DRIVEWAY WIDTH.]

**[II. NONRESIDENTIAL USES]**

The total width of the driveway approach [ENTRANCES TO A NONRESIDENTIAL LOT] from a street shall not exceed 40 percent of the frontage of the lot, or 33 percent of the frontage if the platting authority or traffic engineer finds that conditions warrant it, provided the following:

[, UNLESS THE APPLICANT PROVIDES FOR SNOW STORAGE IN A MANNER APPROVED BY THE DECISION-MAKING BODY.]i. The driveway approach is always allowed to have the minimum driveway width provided by subsection d. The traffic engineer may approve more than the minimum driveway width, provided the traffic engineer determines that snow storage, traffic flow and safety, and the neighborhood context are addressed.

- ii. The driveway approach shall not exceed the maximum driveway width established in the municipal driveway standards.
- iii. Flag lots are exempt from the percentage limitations but shall have a maximum driveway approach width of 20 feet. Abutting flag lots may share a driveway approach up to 24 feet wide (12 feet per lot).

**d. Minimum Driveway Width**

The minimum required width of driveways including both the driveway approach within the street ROW and the portion of the driveway on the development property is as follows:

- i. The minimum width of parking aisles shall be as set forth in 21.07.090H.12.

- 1                   ii.       The minimum width of the driveway approach shall be as set forth in the municipal
- 2                               driveway standards, except as modified or clarified by subsections iii. through vi.
- 3
- 4                   iii.       The minimum width of a driveway providing access to a single-family, two-family,
- 5                               townhouse, mobile home, or other individual dwelling from a local street or alley is
- 6                               12 feet.
- 7
- 8                   iv.       The minimum width of a driveway providing access for up to 10 parking spaces
- 9                               serving a townhouse or multifamily residential use from a local street or alley is 12
- 10                              feet, except that straight (non-curving) driveway segments on the development
- 11                              property (i.e., not in the driveway approach) may be 10 feet.
- 12
- 13                   v.       The minimum width of a driveway providing access for more than 10 parking
- 14                              spaces serving a townhouse or multifamily use from a local street or alley is 12
- 15                              feet for one-way traffic and 20 feet for two-way traffic.
- 16
- 17                   vi.       The traffic engineer may reduce the driveway width to relieve hardship on small or
- 18                              narrow lots or increase the required width to meet vehicle turning and maneuvering
- 19                              needs, in accordance with accepted principles of traffic engineering and safety as
- 20                              determined by the traffic engineer. Minimum driveway widths are also subject to
- 21                              other applicable codes, including the Fire Code.
- 22

23                   e.       **Driveway Throat Length**

24                              The driveway throat shall be of sufficient length to enable the street and walkways in the

25                              ROW and the circulation, parking, and walkways in the development site to function without

26                              interfering with each other, as provided in the municipal driveway standards.

27

28                   f.       **Sight Distance**

29                              Ingress and egress to parking facilities shall be designed as prescribed in the municipal

30                              driveway standards to maintain adequate sight distance and safety.

31

32                   g.       **Circulation Definition**

33                              Circulation patterns within parking facilities shall be well defined with pavement marking

34                              and signage, curbs, landscaping, and/or other similar features. Curbed end islands shall

35                              be provided at the end of each row of parking spaces to define circulation and provide sight

36                              distance at internal intersections of parking aisles, driveways, and/or on-site roadways.

37                              Shared parking courtyards meeting 21.07.060G.21. may depart from this requirement

38

39                   *(Note: Existing subsections 21.07.090H.8.e., Parking and Maneuvering, through 21.07.090H.8.h., Cross*

40                   *Access and Joint Access with Adjacent Sites, are deleted.)*

41                   [E.       PARKING AND MANEUVERING]

42                              [ALL CIRCULATION AISLES, DRIVEWAYS, AND VEHICLE MANEUVERING AREAS

43                              REQUIRED BY THIS SECTION SHALL BE LOCATED ENTIRELY OFF-STREET AND ON

44                              THE PROPERTY UNLESS SPECIFICALLY PROVIDED OTHERWISE BY THIS

45                              SECTION.]

46                   [I.       ACCESS TO PARKING SPACES]

47                              [TO ENSURE SAFE AND EFFICIENT VEHICULAR ACCESS TO PARKING

48                              SPACES, EACH OFF-STREET PARKING SPACE SHALL OPEN DIRECTLY ON

49                              A PARKING AISLE OR DRIVEWAY OF SUCH WIDTH AND DESIGN AS

50                              PROVIDED IN TABLE 21.07-7 AND THE ILLUSTRATIONS THAT FOLLOW THE

51                              TABLE. ADEQUATE INGRESS AND EGRESS TO EACH PARKING SPACE

52                              SHALL BE PROVIDED WITHOUT BACKING MORE THAN 25 FEET.]

53                   [II.     MANEUVERING AREA]

54                              [OFF-STREET PARKING FACILITIES SHALL BE DESIGNED WITH

55                              SUFFICIENT MANEUVERING ROOM SO THAT ALL MANEUVERS

1 ASSOCIATED WITH THE PARKING SHALL OCCUR IN THE OFF-STREET  
2 PARKING FACILITY, AND THAT ALL VEHICLES ENTER THE ABUTTING  
3 STREET IN A FORWARD MOTION.]

4 [III. SOME DWELLINGS EXEMPTED]

5 [SINGLE-FAMILY, TWO-FAMILY, TOWNHOUSE, AND MOBILE HOME  
6 DWELLINGS ON INDIVIDUAL LOTS SHALL BE EXEMPTED FROM THIS  
7 SUBSECTION. MULTIFAMILY DWELLINGS WITH UP TO FOUR UNITS SHALL  
8 BE EXEMPTED FROM THIS SUBSECTION IN APPROPRIATE  
9 CIRCUMSTANCES IF APPROVED BY THE TRAFFIC ENGINEER.  
10 APPROPRIATE CIRCUMSTANCES MAY INCLUDE LOTS WITH ALLEY  
11 ACCESS, LOTS LOCATED ON LOW-VOLUME STREETS, AND LOTS  
12 LOCATED ON DEAD-END STREETS OR CUL-DE-SACS.]

13 [F. DEAD-END PARKING AISLES]

14 [DEAD-END PARKING AISLES MAY BE ALLOWED ONLY WITH THE APPROVAL OF  
15 THE TRAFFIC ENGINEER.]

16 [G. ALLEYS]

17 [SUBJECT TO SAFETY APPROVAL BY THE TRAFFIC ENGINEER, THE USABLE  
18 PORTION OF AN ALLEY MAY BE CREDITED AS CIRCULATION AND/OR PARKING  
19 AISLE SPACE.]

20 [H. CROSS ACCESS AND JOINT ACCESS WITH ADJACENT SITES]

21 [THE PLAN SHALL SHOW EXISTING PARKING AND CIRCULATION PATTERNS ON  
22 ADJACENT PROPERTIES AND POTENTIAL CONNECTIONS. REQUIRED PARKING  
23 LOTS SERVING A SITE, WHETHER LOCATED ON THAT SAME LOT OR ON AN  
24 ADJACENT LOT, MAY BE CONNECTED BY MEANS OF A COMMON ACCESS  
25 DRIVEWAY WITHIN OR BETWEEN THE INTERIOR OF SUCH LOTS. APPLICANTS  
26 ARE ENCOURAGED TO PROVIDE SHARED VEHICLE AND PEDESTRIAN ACCESS TO  
27 ADJACENT PROPERTIES FOR CONVENIENCE, SAFETY, AND EFFICIENT  
28 CIRCULATION. AN ACCESS EASEMENT SHALL BE PROVIDED ON THE PLAT, OR A  
29 SHARED ACCESS AGREEMENT RUNNING WITH THE LAND SHALL BE RECORDED  
30 BY THE MUNICIPALITY, AS APPROVED AND EXECUTED BY THE DIRECTOR,  
31 GUARANTEEING THE CONTINUED AVAILABILITY OF THE SHARED ACCESS  
32 BETWEEN THE PROPERTIES.]

33  
34 **12[9]. Dimensions of Parking Spaces and Aisles**

35  
36 **a. *Minimum Dimensions for Parking***

37 Parking shall meet or exceed the STANDARD parking space and aisle dimensions set forth  
38 in table 21.07-8, except as follows:

- 39  
40 i. Parking facilities in the Downtown (DT) zoning districts, and in the Urban  
41 Neighborhood Contexts delineated in subsection 21.07.010E., may meet the  
42 SMALL parking space and aisle dimensions in table 21.07-8. Up to 30 percent of  
43 parking in the Downtown (DT) zoning districts may meet the COMPACT parking  
44 dimensions, if signed for compact vehicles only.  
45  
46 ii. In all other areas of the municipality, up to 50 percent of employee-only parking,  
47 structured parking facilities, or parking for uses with a low parking turnover rate  
48 may meet the SMALL parking space and aisle dimensions in table 21.07-13. Uses  
49 with a low parking turnover rate comprise multifamily and mixed-use household  
50 living uses, office uses, and educational facility uses other than instructional

1  
2  
3

services. Required parking shared between low-turnover and non-low-turnover uses shall meet STANDARD dimensions.

TABLE 21.07-8 PARKING ANGLE, STALL, AND AISLE DIMENSIONS									
Parking Angle (Degrees)	Parking Space Size	Space Width	Curb Length (Width Projection)	Space Depth (Vehicle Projection)	Aisle Width 1-way	Aisle Width 2-way	Typical Parking Bay Width (Module)	Interlock Reduction	Overhang Allowance
***	***	***							
<sup>1</sup> Developments in the Urban Neighborhood Development Context Areas delineated in 21.07.010E. may use the following alternative STANDARD parking space depth (Vehicle Projection): 19' 0" for 90-degree parking angle; 20' 8" for 75-degree parking angle; 20' 11" for 60-degree parking angle, and 19' 6" for 45-degree parking angle. Parking bay width may decrease as a result.									

4  
5  
6

\*\*\*      \*\*\*      \*\*\*

1 **21.07.110 Residential Design Standards**

2  
3 **A. Purpose**

4 \*\*\* \*\*

- 5 5. Manage the width and frequency of driveways along street frontages to reduce conflicts with traffic  
6 and provide space for street snow clearing and storage, on-street parking (where appropriate), and  
7 pedestrian facilities.

8  
9 *(Note: Subsections 4. and 5. of the purpose statement for the residential design standards are deleted,  
10 and the remaining subsections re-numbered.)*

11 \*\*\* \*\*

12 [4. ENHANCE PUBLIC SAFETY BY PROMOTING LINES OF SIGHT TO RESIDENTIAL ENTRIES,  
13 CLEARLY DEFINING TRANSITIONS FROM PUBLIC TO MORE PRIVATE RESIDENTIAL  
14 OUTDOOR SPACES, AND DESIGNING ENTRANCES TO BE PROMINENT. ]

15 [5. LOCATE ACTIVE LIVING SPACES, ENTRANCES, AND WINDOWS TO IMPROVE THE  
16 PHYSICAL AND VISUAL CONNECTION FROM RESIDENCES TO THE STREET, AND FOSTER  
17 OPPORTUNITIES FOR CASUAL SURVEILLANCE OF THE STREET AND OUTWARDLY  
18 EXPRESSED PROPRIETORSHIP OF THE NEIGHBORHOOD.]

19 **C. Standards for Multifamily and Townhouse Residential**

20  
21 **1. Purpose**

22 \*\*\* \*\*

23 *(Note: Subsections c. and g. of the purpose statement for Multifamily and Townhouse  
24 Residential design standards are deleted, and the remaining subsections re-numbered.)*

25 [C. PROMOTE BUILDING PLACEMENT AND ORIENTATION THAT INTERFACES WITH THE  
26 NEIGHBORHOOD STREET AND ENGAGES THE PEDESTRIAN, TO CONTRIBUTE TO PUBLIC  
27 SAFETY, ATTRACTIVE STREET FRONTAGES, PEDESTRIAN ACCESS, AND A SENSE OF  
28 NEIGHBORHOOD AND COMMUNITY.]

29 \*\*\* \*\*

30 **3. Pedestrian-Oriented Street Frontage Standards.**

31 Provisions for site planning and building orientation in relationship to street frontages and  
32 pedestrian access are provided in subsection 21.07.060F., *Pedestrian Frontage Standards*.

33  
34 *(Note: The following subsections of the Multifamily and Townhouse Residential design standards are  
35 deleted: 21.07.110C.3., Windows Facing the Street; 21.07.110C.5., Pedestrian Access; 21.07.110C.6.,  
36 Building and Site Orientation Menu; and 21.07.110C.9., Entryway Treatment. The remaining subsections  
37 of 21.07.110C. are re-numbered accordingly.)*

38  
39 **D. Standards for Some Single-Family and Two-Family Residential Structures**

40  
41 **2. Pedestrian-Oriented Street Frontage Standards.**

42 Provisions for site planning and building orientation in relationship to street frontages and  
43 pedestrian access are provided in subsection 21.07.060F., *Pedestrian Frontage Standards*.

44  
45 *(Note: Subsections 3., 4., and 5. are deleted.)*

46 \*\*\* \*\*

47 *(Note: Subsection 21.07.110E., Prohibited Structures, is deleted, and Site Design (below) is re-numbered.)*

48  
49 **E. Site Design**

50  
51 *(Note: Site Design subsections 3., Driveway Width, and 4., Alleys, are deleted.)*

1 \*\*\* \*\*

2

1 **21.15.020 Rules of Construction and Interpretation**

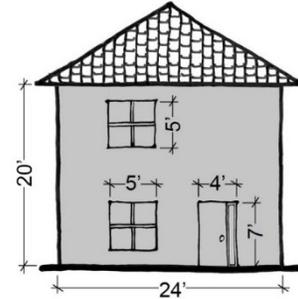
2 \*\*\* \*\*

3 **P. Measurement of Windows and Entrances as a Percentage of Building Wall Area**

4 The percentage of building elevation wall area that is window and entrance openings is determined by  
5 dividing the total square footage of the windows and primary entrances on the building elevation by the  
6 gross square footage of the building elevation wall area (except for parts of the building exempted in 3.),  
7 and then multiplying the resulting quotient by 100.

8  
9 **1. Illustrated Example**

10 In the illustration, the building elevation wall area is 20 feet of  
11 height x 24 feet of length = 480 square feet. The window and  
12 primary entrance area is the area of the two windows (5 x 5  
13 feet x 2) plus the area of the primary entrance and its sidelight  
14 (4 x 7 feet) = 78 square feet. The percentage of the building  
15 elevation wall area is found by dividing 78 by 480 then  
16 multiplying by 100, to yield 16%.



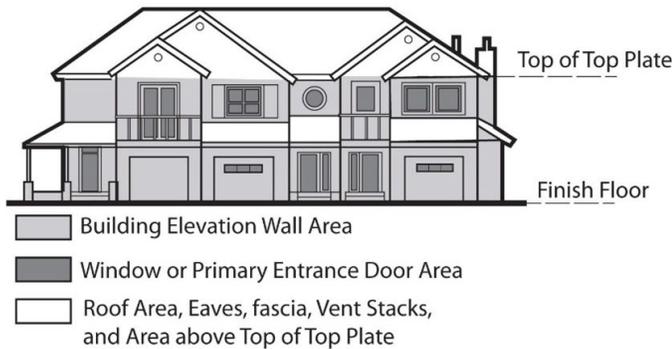
17  
18 **2. Measuring Window and Primary Entrance Area**

19 "Window area" is defined in section 21.15.040, Definitions. Primary entrance area includes the  
20 entrance opening and door frame but excludes trim, molding, and other features beyond the door  
21 frame. "Primary entrance" is defined in 21.15.040, Definitions.

22  
23 **3. Exempt Parts of the Building Elevation Wall Area**

24 The building elevation wall area is measured as the exterior wall plane surface area from finished  
25 grade to the top of the wall on the building except that, for the purposes of measuring windows and  
26 entrances as a percentage of building wall area, the following parts of the building are not included  
27 in the wall area calculation:

- 28
- 29 a. Wall area above the topmost story's top plate, such as gable ends of a roof enclosing a cold attic, or parapet walls.
- 30
- 31
- 32 b. Roof surfaces, eaves, fascia, vent stacks, mechanical equipment and rooms, rooftop access enclosures, and other roof appurtenances.
- 33
- 34
- 35 c. Wall area of stories below grade plane, such as below grade parking, up to the finished floor of the story above grade plane.
- 36
- 37
- 38 d. Parts of a street-facing building elevation below the grade plane of the abutting sidewalk (or edge of street pavement where there is no sidewalk).
- 39



40 \*\*\* \*\*

1 **21.15.040 Definitions**

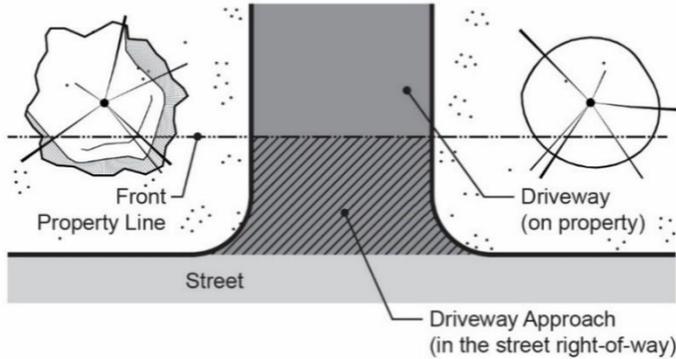
2 \*\*\* \*\*

3 **Complete Street.** See *Street, Complete*.

4 \*\*\* \*\*

5 **Driveway Approach**

6 The portion of a driveway providing direct vehicle ingress and egress over public right-of-way to a property. The  
7 driveway approach extends from the street edge to the front property line.



8 \*\*\* \*\*

10 **Pedestrian Feature**

11 A permanent object that provides pedestrians with increased convenience, comfort, and utility, and which is publicly  
12 accessible and not limited to a tenant or establishment such as seating for a restaurant. Pedestrian features include:

- 13 • Seating such as benches accommodating several people;
- 14 • Secondary/informal seating opportunities such as steps, pedestals, low walls, or edges of fountains,  
15 accommodating several people;
- 16 • A space for standing with objects to lean against, such as bollards, short fences, or irregular building  
17 facades, accommodating several people;
- 18 • A tree or raised planter;
- 19 • A work of art such as a water feature, sculpture, cultural exhibit, or clock feature;
- 20 • Pedestrian-scale area lighting designed to light outdoor walkways or pedestrian spaces;
- 21 • Ornamental lighting designed to illuminate architecture and/or landscaping for decorative and aesthetic  
22 effect;
- 23 • A cross-country ski rack that can accommodate a ski lock and is located at building entrances within 500  
24 feet of a ski trailhead;
- 25 • A winter city urban design feature such as a wind screen, or outdoor stove or space heater; or
- 26 • Other object supporting pedestrian utility, such as a gazebo or kiosk.

28 **Pedestrian Movement Zone**

29 The middle portion of an enhanced street sidewalk (21.07.060G.19.), comprising its walkway clear width located  
30 between the sidewalk’s street interface and building interface zones. The pedestrian movement zone provides for  
31 the primary function of sidewalks and is kept clear of any obstructions to pedestrian movement.

32 \*\*\* \*\*

33 *(Note: The definition of Primary Pedestrian Walkway is deleted.)*

34 [PRIMARY PEDESTRIAN WALKWAY]

35 [A WALKWAY MEETING THE STANDARDS OF SUBSECTION 21.07.060F.4.]

36 \*\*\* \*\*

37 **Primary Entrance**

38 See **Entrance, Primary**.

39 \*\*\* \*\*

40 **Street, Complete**

1 A street that is designed, used, and operated to enable safe access for all users, including pedestrians, bicyclists,  
2 motorists, and public transportation users of all ages and abilities, and for all users to safely move through the  
3 transportation network. For example, Anchorage Metropolitan Area Transportation Solutions (AMATS) adopted a  
4 complete streets policy in 2018 for its urbanized planning area. The AMATS Complete Streets Policy is available  
5 on the municipal website.

6 \*\*\* \*\*

7 **Walkway Clear Width**

8 That portion of the total width of a walkway, trail, pathway, or sidewalk cross-section which is unobstructed,  
9 continuous (without interruption or being split by obstructions), and kept clear for pedestrian movement.

10  
11 *(Note: The definition reference "Walkway, Primary Pedestrian" is deleted.)*

12 [WALKWAY, PRIMARY PEDESTRIAN]

13 [SEE PRIMARY PEDESTRIAN WALKWAY.]

14 \*\*\* \*\*





Planning Department  
Long-Range Planning Division  
PO Box 196650  
Anchorage, Alaska 99519-6650

[www.muni.org/Planning/2040Actions.aspx](http://www.muni.org/Planning/2040Actions.aspx)