



MUNICIPALITY OF ANCHORAGE PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

MP 1.0 to 3.5

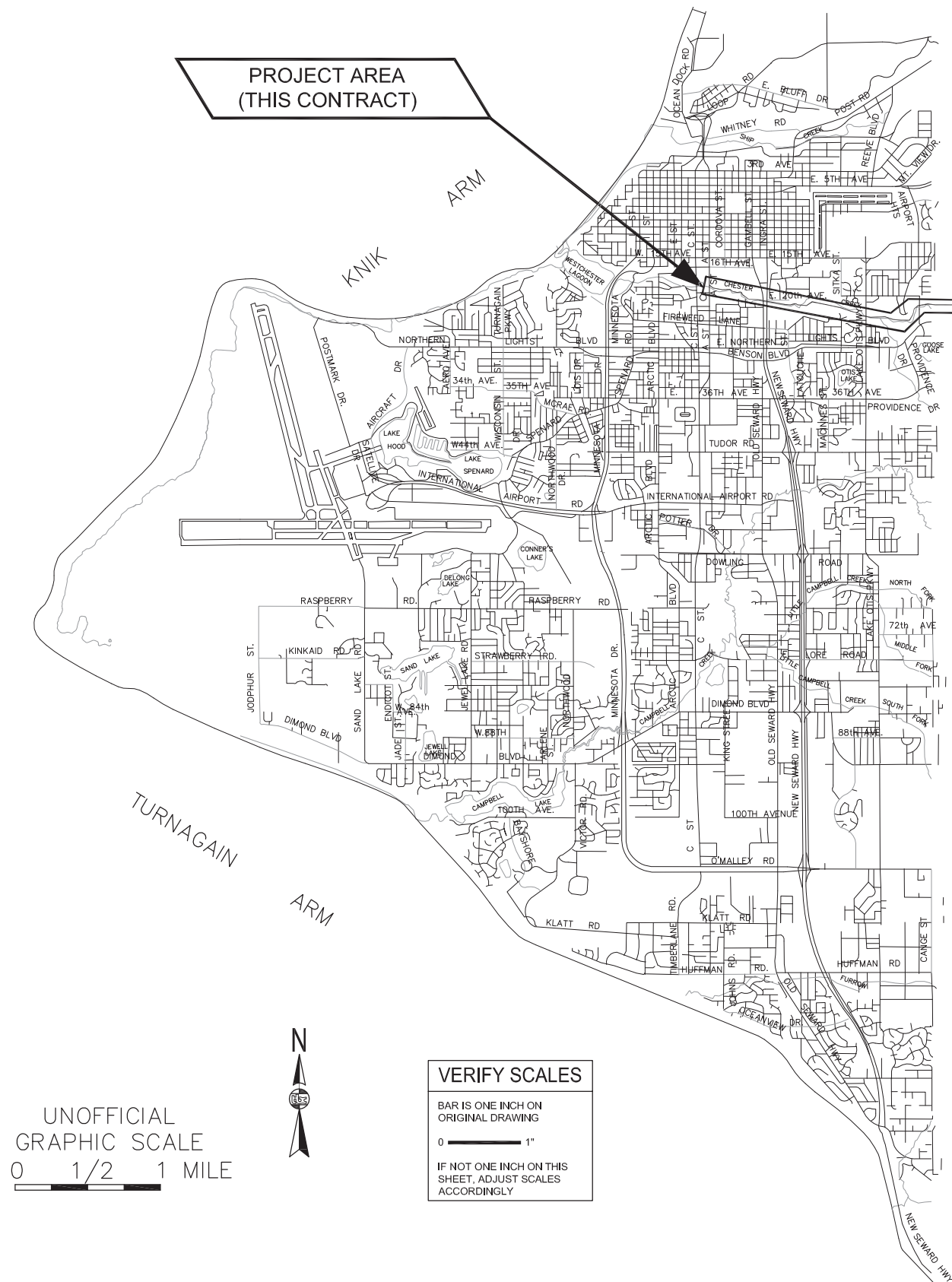
PROJECT NO. ##-##

65% SUBMITTAL

APPROVED BY:

DANIEL A. SULLIVAN
MAYOR

JOHN RODDA
DIRECTOR PARKS AND RECREATION



CONSTRUCTION NOTES

1. CONSTRUCTION SHALL BE COMPLETED IN ACCORDANCE WITH THE MUNICIPALITY OF ANCHORAGE STANDARD SPECIFICATIONS, STREETS-DRAINAGE-UTILITIES-PARKS, DATED 2009 AND "REVISION 3" HEREAFTER REFERRED TO AS MASS, AS CURRENTLY AMENDED BY THE SPECIAL PROVISIONS AND THESE CONSTRUCTION DRAWINGS.
2. SCOPE OF PROJECT INCLUDES THE RECLAMATION, REPLACEMENT AND NEW CONSTRUCTION OF THE LANIE FLEISCHER CHESTER CREEK TRAIL FROM MILE POST 0.0 TO MILEPOST 1.0 (WESTCHESTER LAGOON TO ARCTIC BLVD TUNNEL) AS SHOWN HEREIN.
3. DRAWING SCALES ON SHEETS WITHIN THESE PLANS MAY VARY AND SHOULD BE NOTED PRIOR TO USE. THESE PLANS WERE CREATED FOR 22X34 PLAN SET AND AT A SPECIFIC DRAWING SCALE. ANY REPRODUCTION OR PUBLISHING OF THESE PLANS MAY RESULT IN DISTORTION OF SCALE AND SHALL BE VERIFIED PRIOR TO USE.
4. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO BEGINNING CONSTRUCTION. THE PERMITS SHALL BE MAINTAINED AT THE JOB SITE.
5. CONTRACTOR SHALL MAINTAIN "REDLINE" RECORD DRAWINGS ON A CLEAN SET OF CONSTRUCTION DRAWINGS IN ACCORDANCE WITH MASS DIVISION 65.00 CONSTRUCTION SPECIFICATIONS FOR CONSTRUCTION SURVEY. THE "REDLINES" SHALL BE KEPT CURRENT ON A DAILY BASIS AND SHALL BE AVAILABLE TO THE ENGINEER FOR INSPECTION ON THE JOBSITE.
6. CONTRACTOR SHALL RECORD SURVEY NOTES FOR SUBMITTAL WITH AS-BUILT PLANS, INCLUDING HORIZONTAL AND VERTICAL LOCATIONS OF ALL UTILITIES ENCOUNTERED IN THE FIELD. CONTRACTOR SHALL RECORD ALL DEVIATIONS FROM THE PLANS.
7. THESE NOTES CONTAIN INFORMATION NECESSARY FOR THE PROPER EXECUTION OF THE WORK CONTAINED ON THESE IMPROVEMENT PLANS. THESE NOTES APPLY TO ALL PLAN SHEETS. ADDITIONAL CONSTRUCTION NOTES MAY ALSO BE SHOWN ON INDIVIDUAL PLAN SHEETS. THE CONTRACTOR IS RESPONSIBLE TO READ AND COMPLY WITH ALL NOTES SHOWN ON THIS SET OF PLANS. THE TERM "CONTRACTOR", AS USED IN THESE NOTES AND ELSEWHERE IN THIS PLAN SET, MEANS THE GENERAL CONTRACTOR AND ALL SUBCONTRACTORS AND INDIVIDUALS AUTHORIZED TO PERFORM WORK SHOWN ON THESE IMPROVEMENT PLANS. THE CONTRACTOR IS RESPONSIBLE TO COMPLY WITH ALL NOTES APPLICABLE TO HIS/HER WORK. ALL CONTRACTORS ARE DIRECTED TO CONTACT THIS ENGINEER FOR ANY QUESTIONS REGARDING THE STATED OR IMPLIED MEANING OF ANY NOTE OR OTHER INFORMATION CONTAINED ON THESE IMPROVEMENT PLANS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO INSURE THAT HIS/HER CONTRACT FOR SERVICES INCLUDES THE RESPONSIBILITIES DEFINED BY THE APPLICABLE NOTES.
8. RE-STRIPING OF TRAIL TO BE DONE BY MOA PARKS & RECREATION MAINTENANCE. CONTRACTOR TO COORDINATE WITH MOA PARKS AND RECREATION PRIOR TO REMOVAL OF PAVEMENT TO INSURE PROPER DOCUMENTATION AND SURVEY OF STRIPING LOCATIONS HAS BEEN ACCOMPLISHED. DOCUMENTATION AND SURVEY OF STRIPING TO BE DONE BY MOA PARKS & RECREATION OR REPRESENTATIVE.
9. EXISTING SIGNAGE ALONG THE COASTAL TRAIL MAY REQUIRE REMOVAL AND REINSTALLATION AS A RESULT OF WORK TO BE PERFORMED. REINSTALLATION OF SIGNAGE SHALL BE PER MASS, ALL ASSOCIATED WORK SHALL BE INCIDENTAL TO THE CONTRACT, AND NO SEPARATE PAYMENT SHALL BE MADE.
10. TREE REMOVAL WILL NOT BE A PART OF THIS CONTRACT.
11. ALL HAUL ROUTES AND CONSTRUCTION TRAFFIC DESIGNATIONS SHOWN HEREIN ARE BY RECOMMENDATION OF THE ENGINEER AND AS SUCH, ARE RECOMMENDATIONS ONLY. TRAFFIC CONTROL PLAN (TCP) TO BE COMPLETED BY CONTRACTOR PER MASS, COORDINATED WITH AND APPROVED BY THE MOA TRAFFIC ENGINEER, AS WELL AS ADOT. SEASONAL WEIGHT RESTRICTIONS MAY EXIST DURING PROPOSED CONSTRUCTION SCHEDULE. SUCH RESTRICTIONS MUST BE ADHERED TO BY CONTRACTOR, AND INCLUDED IN TCP.
12. ALL QUANTITIES SHOWN HEREIN ARE APPROXIMATE. CONTRACTOR SHALL VERIFY ALL QUANTITIES.
13. EXACT LOCATION OF EXCAVATION AND BACKFILL SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
14. ORGANIC MATERIAL SHALL BE REMOVED FROM THE SUBGRADE TO A DEPTH TO BE DETERMINED BY THE ENGINEER. NO ORGANIC MATERIAL OR OTHER DELETERIOUS MATERIAL SHALL BE UTILIZED FOR BACKFILL.
15. DEWATERING IS NOT ANTICIPATED FOR THIS PROJECT. IF DEWATERING IS REQUIRED, IT SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE
16. COMPACT NATIVE SOILS TO 95% M.D.D. PRIOR TO FILL PLACEMENT.
17. CLASSIFIED FILL SHALL BE PLACED IN LIFTS NOT TO EXCEED 12-INCHES IN DEPTH AND SHALL BE COMPACTED TO 95% M.D.D.
18. FINISH GRADE (FG) REPRESENTS THE ELEVATION OF THE FINISHED SURFACE. THIS INCLUDES LANDSCAPE AREAS, PAVED OR CONCRETE SURFACES, ROCK RIP-RAP SURFACE AND ELEVATION AT EXTERIOR OF STRUCTURE FOUNDATION, UNLESS OTHERWISE DENOTED ON DETAIL OR SPECIAL LABEL. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ADJUST SUBGRADE OR TOPSOIL TO ALLOW FOR FINISHED SURFACE MATERIAL DIMENSIONS. IF DETAIL IS PROVIDED FOR SPECIAL AREA, DETAIL SHALL DENOTE FINISH GRADES.
19. CUT AND FILL SLOPES SHALL NOT EXCEED 3 HORIZONTAL UNITS TO 1 VERTICAL UNIT (3:1).

CLEANUP AND TOPSOIL:

20. WORK AND MATERIALS REQUIRED FOR REMOVING LITTER OR DEBRIS THAT EXISTS WITHIN THE PROJECT LIMITS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "DEMOLITION" (OR INCIDENTAL TO THE PROJECT) AND NO SEPARATE PAYMENT SHALL BE MADE.
21. CONTRACTOR SHALL RESTORE DISTURBED PROPERTY TO PRE-CONSTRUCTION CONDITION(S), UNLESS OTHERWISE DIRECTED BY THE ENGINEER. PAYMENT FOR RESTORING DISTURBED PROPERTY SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND AND NO SEPARATE PAYMENT SHALL BE MADE, UNLESS BID ITEMS ARE PROVIDED.

RECLAMATION AND REPLACEMENT:

22. NO SOILS REPORT OR INVESTIGATION HAS BEEN CONDUCTED FOR THIS PROJECT.
23. TRAIL REPLACEMENT AREAS ARE BASED ON EXISTING FAILURES WITHIN TRAIL PAVEMENT. ADDITIONAL TRAIL REPLACEMENT MAY BE REQUIRED.
24. CONTRACTOR TO WORK WITH ENGINEER AND PROVIDE POTHOLING AT INTERMEDIATE LOCATIONS WITHIN TRAIL DURING CONSTRUCTION. LOCATIONS AND EXTENT TO BE DETERMINED ONSITE. CONTRACTOR SHALL POTHOLE AS DIRECTED BY THE ENGINEER.
25. DEPTH OF EXISTING PAVEMENT IS ASSUMED TO RANGE BETWEEN 2-4 INCHES. IF DEEPER PAVEMENT SECTIONS ARE ENCOUNTERED DURING RECLAMATION, NOTIFY ENGINEER.
26. PORTIONS OF TRAIL APPROACHES, PULLOUTS, LANDINGS AND TRANSITIONS MAY NOT BE ACCESSIBLE TO RECLAMATION EQUIPMENT. FOR THESE AREAS, YIELD TO REMOVE AND REPLACE PAVEMENT ITEM IN THE CONTRACT.

GRADE TO DRAIN:

27. MINOR CLEARING & GRUBBING AND EARTHWORK IS REQUIRED AROUND EXISTING AND PROPOSED CULVERTS TO PERPETUATE DRAINAGE UNDER TRAIL.
28. MINOR CLEARING & GRUBBING AND EARTHWORK IS REQUIRED AROUND EXISTING TUNNEL ENTRANCES TO PERPETUATE DRAINAGE OFF OF TRAIL AND TO MEDIATE KNOWN BIRDBATHS.

EXISTING UTILITIES

1. LOCATIONS DEPICTED FOR THE UTILITIES AND OTHER EXISTING FEATURES ARE APPROXIMATE. SOME UTILITIES HAVE BEEN LOCATED FROM AS-BUILT DRAWINGS AND SOME FROM UTILITY COMPANY LOCATES, AND THEREFORE MAY NOT BE VISIBLE.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND VERIFYING ALL UTILITIES AND PERFORMING ANY NECESSARY VERIFICATION PRIOR TO CONSTRUCTION.
3. UNDERGROUND AND OVERHEAD ELECTRICAL, TELECOMMUNICATION LINES, AND POLES OCCUR WITHIN THE PROJECT AREA; CONTRACTOR SHALL COORDINATE WORK ACCORDINGLY. ALL WORK IN CLOSE PROXIMITY TO EXISTING OVERHEAD AND/OR UNDERGROUND LINES, AND POLES SHALL COMPLY WITH APPLICABLE FEDERAL, STATE, AND LOCAL STATUTES, CODES AND GUIDELINES, AND THE ELECTRICAL FACILITY CLEARANCE REQUIREMENTS OF THE GOVERNING UTILITY.
4. HAND DIGGING IS REQUIRED WITHIN THREE FEET OF BURIED ELECTRICAL CABLE AND FIBER OPTIC CABLE.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATION OF ALL EXISTING UTILITIES WITHIN THE LIMITS OF CONSTRUCTION, WHETHER OR NOT SAID UTILITIES ARE SHOWN ON THE PLANS. THIS RESPONSIBILITY INCLUDES CONTACTING UTILITY COMPANIES FOR LOCATIONS OR POTHOLING PRIOR TO CONSTRUCTION. ANY DAMAGE TO UTILITIES DURING CONSTRUCTION IS THE RESPONSIBILITY OF THE CONTRACTOR.
6. ACCESS TO MANHOLES MUST BE MAINTAINED AT ALL TIMES. PROPER HEIGHT AND RING ADJUSTMENTS PER MASS SECTION 50.03 DETAIL 50-5 AND SECTION 50.20 DETAIL 50-25.



Know what's below.
Call before you dig.

LEGEND

| | |
|--|------------------------------------|
| | TRAIL RECLAMATION |
| | TRAIL REPLACEMENT |
| | REMOVE AND REPLACE PAVEMENT |
| | JOGGING SHOULDER |
| | EXISTING WETLANDS |
| | PARCEL LINE (MOA GIS) |
| | EXISTING CURB & GUTTER |
| | EXISTING EDGE OF PAVEMENT |
| | EXISTING FENCE |
| | EXISTING EDGE OF WATER |
| | OPEN DITCH |
| | PERIMETER CONTROL BMP |
| | EXISTING GUARDRAIL |
| | EXISTING SANITARY SEWER |
| | EXISTING SANITARY SEWER MANHOLE |
| | EXISTING STORM DRAIN |
| | EXISTING STORM DRAIN MANHOLE |
| | EXISTING UNDERGROUND ELECTRIC |
| | EXISTING TRAIL LIGHT AND J-BOX |
| | EXISTING OVERHEAD UTILITY POLE |
| | EXISTING VEGETATION (CANOPY) |
| | WOOD BOLLARD |
| | REMOVABLE BOLLARD |
| | EXISTING BOLLARD SIGN |
| | EXISTING SIGN |
| | EXISTING TRASH RECEPTACLE |
| | EXISTING MONITORING WELL TO RESET |
| | EXISTING MONITORING WELL TO REMAIN |

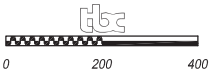
| | |
|-------|--|
| ADA | AMERICAN WITH DISABILITIES ACT OF 1990 |
| ADOT | STATE OF ALASKA DEPARTMENT OF TRANSPORTATION |
| AC | ASPHALTIC CONCRETE |
| BOP | BEGINNING OF PROJECT |
| BC | BEGINNING OF CURVE |
| BF | BOTTOM OF FOOTING |
| BM | BENCH MARK |
| BMP | BEST MANAGEMENT PRACTICE |
| BVC | BEGIN VERTICAL CURVE |
| BW | BACK OF WALK ELEVATION |
| C&G | CURB AND GUTTER |
| CL | CENTERLINE |
| CO | CLEANOUT |
| CC | CURB CUT |
| CONST | CONSTRUCT |
| CR | CURB RETURN |
| DET | DETAIL |
| DI | DROP INLET |
| DIP | DUCTILE IRON PIPE |
| EOP | END OF PROJECT |
| E | ELECTRICAL |
| EP | EDGE OF PAVEMENT |
| EL | ELEVATION |
| EA | EASEMENT LINE |
| EC | END OF CURVE |
| EST | ESTIMATED |
| EX | EXISTING |
| EVC | END OF VERTICAL CURVE |
| FI | FURNISH AND INSTALL |
| FF | FINISH FLOOR ELEVATION |
| FG | FINISHED GRADE |
| FH | FIRE HYDRANT |
| FL | FLOW LINE |
| FLG | FLANGE |
| G | GAS |
| GB | GRADE BREAK |
| GV | GATE VALVE |
| HDPE | HIGH DENSITY POLYETHYLENE PIPE |
| HORZ | HORIZONTAL |
| HP | HIGH POINT |
| IE | INVERT ELEVATION |
| INT | INTERSECTION |
| IAW | IN ACCORDANCE WITH |
| L | LENGTH |
| LC | LIP OF CURB |
| LF | LINEAR FEET |
| LOC | LOCATION |
| LP | LOW POINT |
| LT | LEFT |
| MASS | MUN. OF ANCHORAGE STANDARD SPECIFICATIONS |
| MAX | MAXIMUM |
| ME | MATCH EXISTING |
| MH | MANHOLE |
| MIN | MINIMUM |
| MON | MONUMENT |
| MSL | MEAN SEA LEVEL |
| NTS | NOT TO SCALE |
| OGS | OIL/GREASE SEPARATOR |
| PAD | PAD ELEVATION |
| PCC | PORTLAND CEMENT CONCRETE |
| PC | POINT OF CURVATURE |
| PED | PEDESTRIAN |
| PL | PROPERTY LINE |
| PI | POINT OF INTERSECTION |
| PRC | POINT OF REVERSE CURVE |
| PO | PUSH ON |
| PUE | PUBLIC UTILITY EASEMENT |
| PVC | POINT OF VERTICAL CURVE |
| PVI | POINT OF VERTICAL INTERSECTION |
| PVT | POINT OF VERTICAL TANGENT |
| PT | POINT OF TANGENT |
| R | RADIUS |
| RAP | RECYCLED ASPHALT PAVEMENT |
| RCB | REINFORCED CONCRETE BOX |
| RCP | REINFORCED CONCRETE PIPE |
| RR | REMOVE AND REPLACE |
| ROW | RIGHT-OF-WAY |
| RP | RADIUS POINT |
| RT | RIGHT |
| REF | REFERENCE |
| RET | RETURN |
| S | SLOPE |
| SG | SUBGRADE |
| SS | SANITARY SEWER |
| STA | STATION |
| STD | STANDARD |
| SW | SIDEWALK |
| ST | STREET |
| SHT | SHEET |
| SD | STORM DRAIN |
| TAN | TANGENT |
| TEL | TELEPHONE |
| TC | TOP OF CURB |
| TBM | TEMPORARY BENCH MARK |
| TB | THRUST BLOCK |
| TOW | TOP OF WALL |
| TOE | TOE OF SLOPE |
| TOP | TOP OF SLOPE |
| TYP | TYPICAL |
| VB | VALVE BOX |
| VC | VERTICAL CURVE |
| W | WATER |
| Δ | CURVE DELTA |

| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---------------------|--|-----------------|----------|-------|----------------|----------|------------|------------------|----------|------------|-----|------|-------------|------------------|-----------|--|--|--|--|------------|--|---|--|----------------|------|------------------|--|-----------|--|-------|--|----------|--|
| | | | | | | | | | | | | | | 65% SUBMITTAL | | Department of Parks and Recreation | | | | | | PARKS AND RECREATION DEPARTMENT | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | Municipality of Anchorage | | | | | | 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL NOTES & ABBREVIATIONS | | | | | | | | | | | |
| FIELD BOOKS | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | | | | | | SCALE | | HOR. PER SHEET | | DATE: 12/15/2014 | | GRID: N/A | | SHEET | | G3 of G4 | |
| DESIGN: | | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | | | | | | | | | | | | | | | | | | |
| STAKING: | | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | | | | | | | | | | | | | | | | | | |
| ASBUILT: | | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | | | | | | | | | | | | | | | | | | |
| CONTRACTOR: | | BASIS OF DATUM: | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | | | | | | | | | | | | | | | | | | |
| INSPECTOR: | | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | | | | VERTICAL DATUM | | | | | PLAN CHECK | | | | | REVISIONS | | | | | CONSULTANT | | | | | SEAL | | | | | | | | |

SURVEY CONTROL NOTES:

1. BASIS OF COORDINATES ARE PUBLISHED NAD83 COORDINATES FROM THE NGS CORS (NATIONAL GEODETIC SURVEY CONTINUOUS OPERATING SYSTEM) STATION TSEA (NGS PID A10952). THE PUBLISHED NAD83 COORDINATE FOR THAT STATION IS:

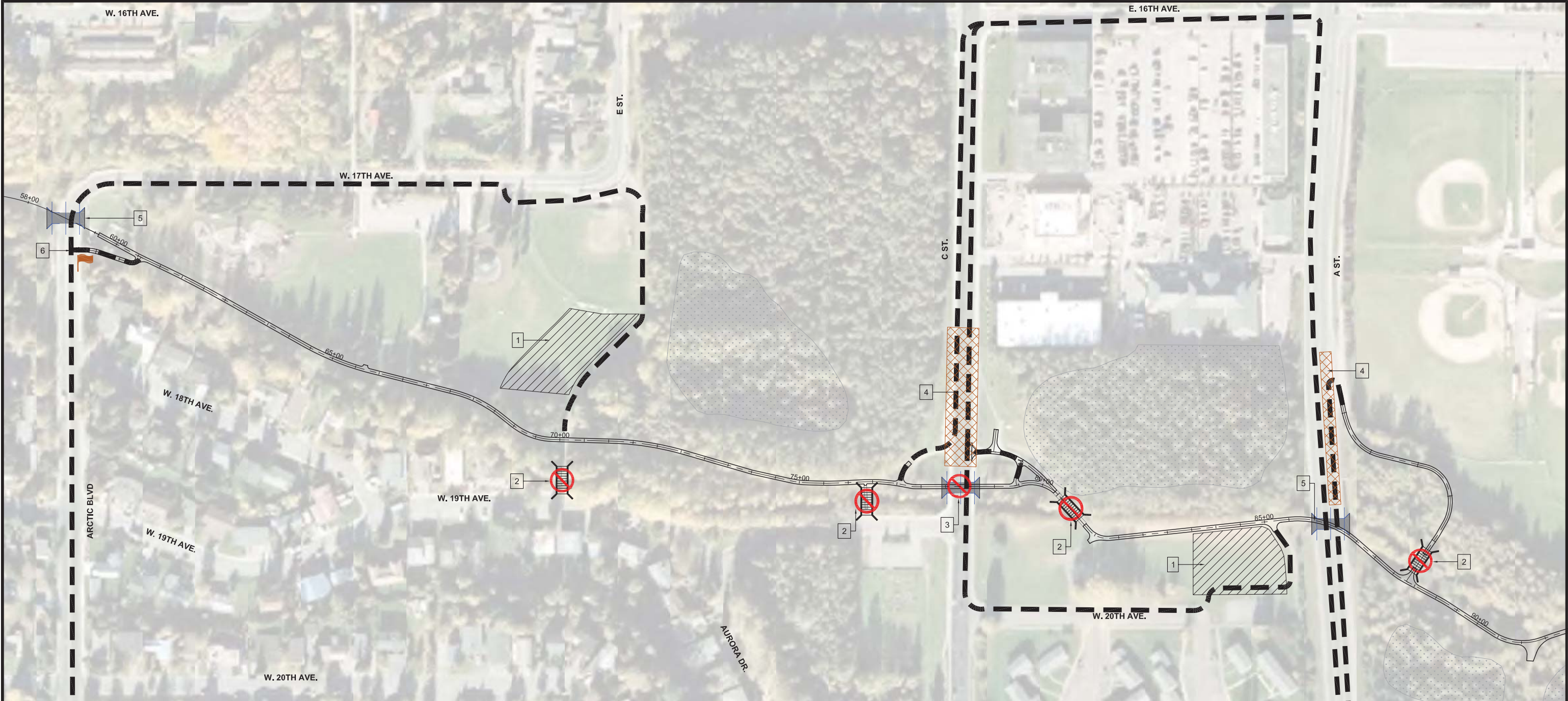
61°11'14.37631" N
149°53'41.82239" W
2. BEARINGS ARE NAD83. DISTANCES ARE EXPRESSED IN U.S. SURVEY FEET.
3. BASIS OF ELEVATION – ELEVATIONS ARE BASED ON THE PUBLISHED ELLIPSOID ELEVATION OF THE NGS CORS (NATIONAL GEODETIC SURVEY CONTINUOUS OPERATING SYSTEM) STATION TSEA (NGS PID A10952). THE PUBLISHED NAD83 ELLIPSOID ELEVATION FOR THAT STATION IS 139.56 FEE
4. THE CONTROL SURVEY WAS PERFORMED IN NOVEMBER, 2013. FIELD SURVEY INFORMATION FOR THIS PROJECT IS LOCATED FIELD BOOK XXXX-X, PAGES X-X.
5. NO TITLE SEARCH WAS PERFORMED FOR THIS PROPERTY.
6. ALL DISTURBED PROPERTY CORNERS SHALL BE REPLACED BY THE CONTRACTOR.



Surveyor's Certificate

I, Anthony P. Hoffman, professional land surveyor, do hereby certify that this plat is a true and correct representation of lands actually surveyed, and that all distances and bearings are shown correctly and that all permanent exterior control monuments, all other monuments, and lot corners have been set and staked, or if final completion is assured by subdivision agreement, they will be set as specified in said subdivision agreement.

| | | | | | | | | | | | | | | | | | | | | | | | |
|---------------------|--|-----------------|----------|-------|----------------|----------|------------------|--|----------|------------|-----|-----------|-------------|------------|-----|------------------|--|----------------|------------------|---------------------------------|-------|----------|--|
| | | | | | | | | | | | | | | | | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage | | | PARKS AND RECREATION DEPARTMENT | | | |
| | | | | | | | | 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL SURVEY CONTROL | | | | | | | | | | | | | | | |
| FIELD BOOKS | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | SCALE | | HOR. PER SHEET | DATE: 12/15/2014 | GRID: NA | SHEET | G4 of G4 | |
| DESIGN: | | | | | BASE | TBC | | TELEPHONE | -- | | | | | | | | VER. PER SHEET | | STATUS: 65% PS&E | | | | |
| STAKING: | | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | | | | | | | | | |
| | | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | | | | | | | | | |
| ASBUILT: | | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | | | | | | | | | |
| CONTRACTOR: | | BASIS OF DATUM: | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | | | | | | | | | |
| INSPECTOR: | | | | | WATER | -- | | QUANTITIES | TBC | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | VERTICAL DATUM | | | PLAN CHECK | | MUN. FINAL CHECK | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | REVISIONS | | CONSULTANT | | SEAL | | | | | | | |



CONSTRUCTION ACCESS NOTES

LEGEND

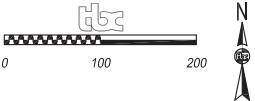
SPECIFIC

- 1 → APPROVED CONSTRUCTION STORAGE/STAGING AREAS. AREAS SHALL BE PROTECTED AND MAINTAINED IN ACCORDANCE WITH MASS AND THE APPROVED PROJECT SWPPP.
- 2 → BRIDGE HAS LOW LOAD LIMIT. NO EQUIPMENT OR VEHICULAR TRAFFIC SHALL CROSS THE BRIDGE.
- 3 → NO VEHICLE OR EQUIPMENT ACCESS THROUGH THE C ST. UNDERPASS.
- 4 → LANE CLOSURE REQUIRED FOR CONSTRUCTION ACCESS. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS FOR LANE CLOSURES. NO MORE THAN ONE LANE MAY BE CLOSED AT ANY TIME. LANES MAY BE CLOSED FROM 9:00 AM TO 2:00 PM UNLESS OTHERWISE PERMITTED BY AKDOT. ACCESS TO BUS STOPS MUST BE MAINTAINED DURING CLOSURES.
- 5 → ACCESS IS LIMITED AT EXISTING TUNNELS/UNDERPASSES. SEE TUNNEL SHEETS
- 6 → FLAGGER REQUIRED FOR CONSTRUCTION ACCESS

GENERAL

- 1. A TRAFFIC CONTROL PLAN (TCP) IS REQUIRED FOR THIS PROJECT AND SHALL COMPLY WITH MASS SECTION 70.12 AND PROJECT SPECIAL PROVISIONS.
- 2. THIS PROJECT WILL HAVE SIGNIFICANT IMPACTS ON PERMITTED SPECIAL ACTIVITIES. CONTRACTOR TO PREPARE A PEDESTRIAN ACCESS PLAN IN ACCORDANCE WITH MASS SECTION 70.12. SEE PEDESTRIAN DETOUR SHEET. CONTRACTOR SHALL COORDINATE WITH SHANE LOCKE AT MOA TRAFFIC ENGINEER TO ACCOMMODATE SPECIAL ACTIVITIES. SEE SPECIAL PROVISIONS.
- 3. CONTRACTOR SHALL PREPARE A WORK PLAN IN CONJUNCTION WITH TCP AND SHALL SUBMIT WORK PLAN AT THE PRE-CONSTRUCTION MEETING. WORK PLAN SHALL ADDRESS STORAGE/STAGING AND CONSTRUCTION ACCESS.
- 4. ANY COST ASSOCIATED WITH ACCESS IMPROVEMENTS/REQUIREMENTS SHALL BE INCIDENTAL TO THIS CONTRACT. ALL ROUTES WILL REQUIRE RESTORATION TO EXISTING CONDITIONS. ANY COST FOR REESTABLISHMENT OF EXISTING CONDITIONS SHALL BE INCIDENTAL TO THIS CONTRACT. NO SEPARATE PAYMENT SHALL BE MADE.

- NO VEHICULAR ACCESS
- FLAGGER
- LANE CLOSURE REQUIRED FOR ACCESS
- STAGING AREA
- ACCESS/HAUL ROUTE
- BRIDGE
- TUNNEL/UNDERPASS
- WETLAND/SWAMP



| FIELD BOOKS | | | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
|---------------------|--|--|--|----------------|----------|-------|----------------|----------|------------|------------------|----------|------------|-----|------|-------------|----|-----|
| DESIGN: | | | | | | | BASE | TBC | | TELEPHONE | --- | | | | | | |
| STAKING: | | | | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | |
| ASBUILT: | | | | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | |
| CONTRACTOR: | | | | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | |
| INSPECTOR: | | | | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | |
| | | | | | | | WATER | --- | | QUANTITIES | TBC | | | | | | |
| | | | | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | PLAN CHECK | | | | | REVISIONS | | | | | |

The Boutet Co.

65% SUBMITTAL

Department of Parks and Recreation

Healthy Parks Healthy People

Municipality of Anchorage

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CONSTRUCTION ACCESS

ARTIC BLVD. TO A ST.

SCALE

HOR. PER SHEET

VER. PER SHEET

DATE: 12/12/2014

GRID: N/A

STATUS: 65% SUBMITTAL

SHEET

H1 of H6



CONSTRUCTION ACCESS NOTES


LEGEND

SPECIFIC


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NO VEHICULAR ACCESS




FLAGGER




LANE CLOSURE REQUIRED FOR ACCESS




STAGING AREA




ACCESS/HAUL ROUTE



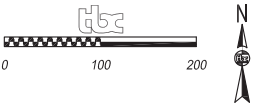
BRIDGE



TUNNEL/UNDERPASS



WETLAND/SWAMP



| FIELD BOOKS | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | |
|---------------------|--|-----------------|----------|-------|----------------|----------|------------|------------------|----------|------------|-----------|------|-------------|----|-----|--|--|
| DESIGN: | | | | | BASE | TBC | | TELEPHONE | -- | | | | | | | | |
| STAKING: | | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | | | |
| | | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | | | |
| ASBUILT: | | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | | | |
| CONTRACTOR: | | BASIS OF DATUM: | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | | | |
| INSPECTOR: | | | | | WATER | -- | | QUANTITIES | TBC | | | | | | | | |
| | | | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | | | |
| CONSTRUCTION RECORD | | VERTICAL DATUM | | | PLAN CHECK | | | | | | REVISIONS | | | | | | |



The Boutet Co.

65% SUBMITTAL

Department of Parks and Recreation

Municipality of Anchorage



Healthy Parks
Healthy People



MUNICIPALITY OF ANCHORAGE

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CONSTRUCTION ACCESS

A ST. TO SEWARD HWY.

SCALE

HOR. PER SHEET

VER. PER SHEET

DATE: 12/12/2014

GRID: N/A

STATUS: 65% SUBMITTAL

SHEET

H2 of H6



CONSTRUCTION ACCESS NOTES

LEGEND

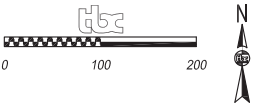
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GENERAL

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- LANE CLOSURE REQUIRED FOR ACCESS
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- ACCESS/HAUL ROUTE
- BRIDGE
- TUNNEL/UNDERPASS
- WETLAND/SWAMP



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| | | | | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | |
| ASBUILT: | | | | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | |
| CONTRACTOR: | | | | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | |
| INSPECTOR: | | | | | | | WATER | --- | | QUANTITIES | TBC | | | | | | |
| | | | | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | PLAN CHECK | | | | | REVISIONS | | | | | |



65%
SUBMITTAL

Department of
Parks and
Recreation

Municipality of
Anchorage

Healthy
Parks



Healthy
People



PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS
CONSTRUCTION ACCESS

SEWARD HWY. TO SITKA ST. PARK

SCALE

HOR. PER SHEET
VER. PER SHEET

DATE: 12/12/2014

GRID: N/A

STATUS: 65% SUBMITTAL

SHEET

H3
of
H6



CONSTRUCTION ACCESS NOTES

LEGEND

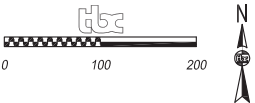
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| | | | | | | | | | | | | | | |
|---------------------|-----------------|----------|-------|----------------|----------|------------|------------------|------------|------------|-----|------|-------------|----|-----|
| | | | | | | | | | | | | | | |
| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
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The Boutet Co.

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Department of Parks and Recreation

Healthy Parks

Municipality of Anchorage

Healthy People

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CONSTRUCTION ACCESS

SITKA ST. PARK TO DAVENPORT FIELDS

SCALE

HOR. PER SHEET

VER. PER SHEET

DATE: 12/12/2014

GRID: N/A

STATUS: 65% SUBMITTAL

SHEET

H4 of H6



CONSTRUCTION ACCESS NOTES

LEGEND

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GENERAL

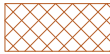
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FLAGGER



LANE CLOSURE REQUIRED FOR ACCESS



STAGING AREA



ACCESS/HAUL ROUTE



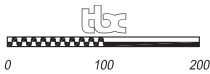
BRIDGE



TUNNEL/UNDERPASS



WETLAND/SWAMP



| FIELD BOOKS | | | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
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The Boutet Co.

65%
SUBMITTAL

Department of
Parks and
Recreation

Municipality of
Anchorage



Healthy
Parks

Healthy
People



MUNICIPALITY OF ANCHORAGE

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS
CONSTRUCTION ACCESS
DAVENPORT FIELDS TO ARCA DR.

SCALE

HOR. PER SHEET
VER. PER SHEET

DATE: 12/12/2014

GRID: N/A

STATUS: 65% SUBMITTAL

SHEET

H5
of
H6



CONSTRUCTION ACCESS NOTES

LEGEND

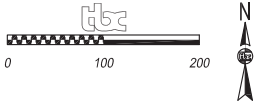
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Department of Parks and Recreation

Municipality of Anchorage



Healthy Parks Healthy People



MUNICIPALITY OF ANCHORAGE

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CONSTRUCTION ACCESS

TIKISHLA PARK TO NICHOLS ST.

SCALE

HOR. PER SHEET

VER. PER SHEET

DATE: 12/12/2014

GRID: N/A

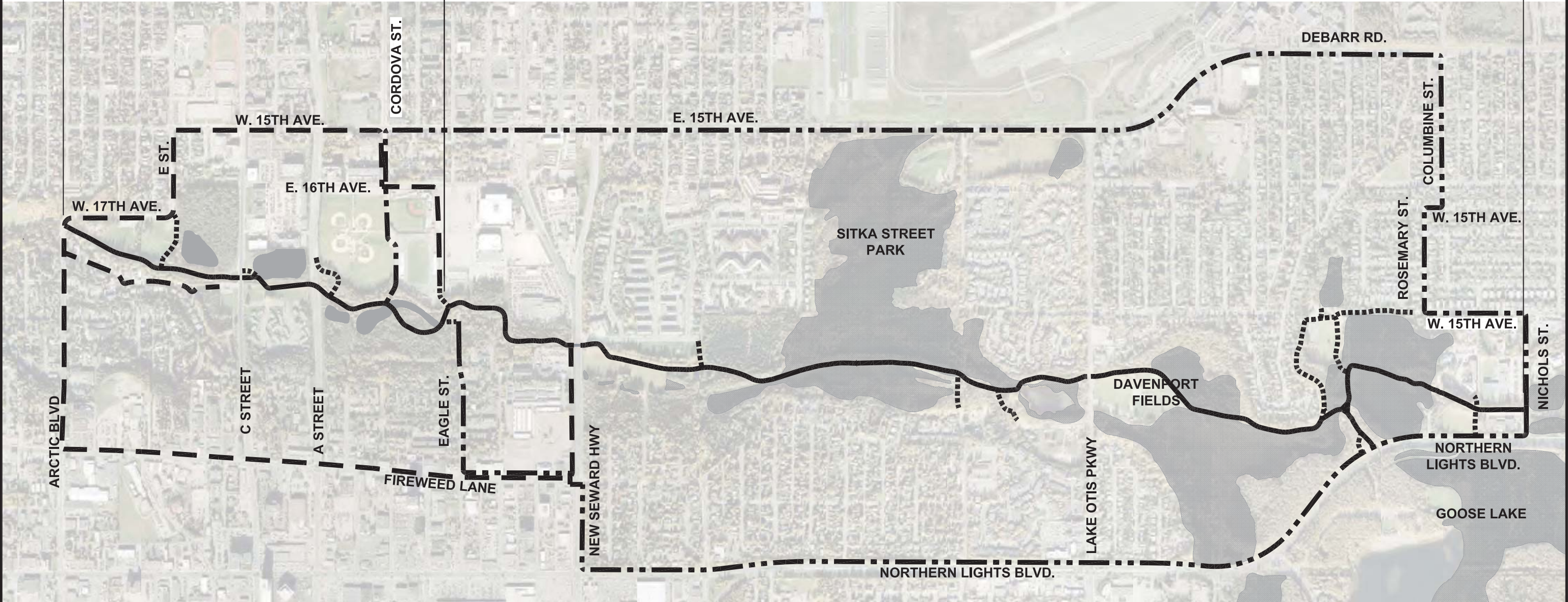
STATUS: 65% SUBMITTAL

SHEET

H6 of H6

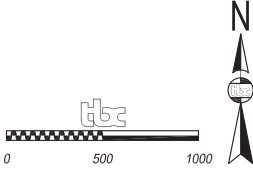
Phase#1
Arctic Blvd to Eagle St

Phase #2
Eagle St to Nichols St



LEGEND

- BASE BID
- ADDITIVE ALTERNATE
- - - - PHASE 1 DETOUR ROUTE
- . - . PHASE 2 DETOUR ROUTE



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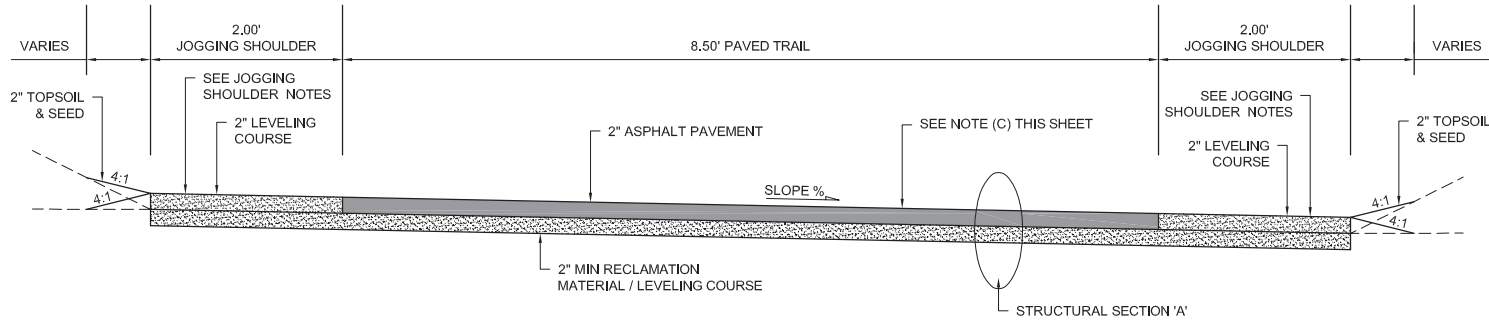
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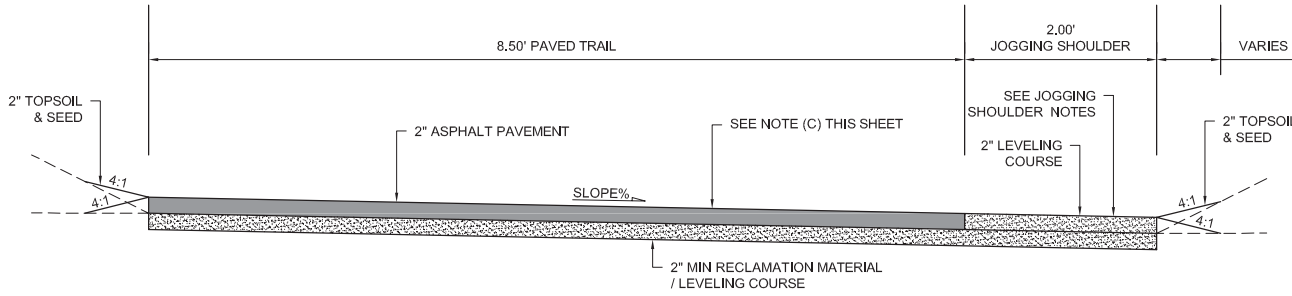
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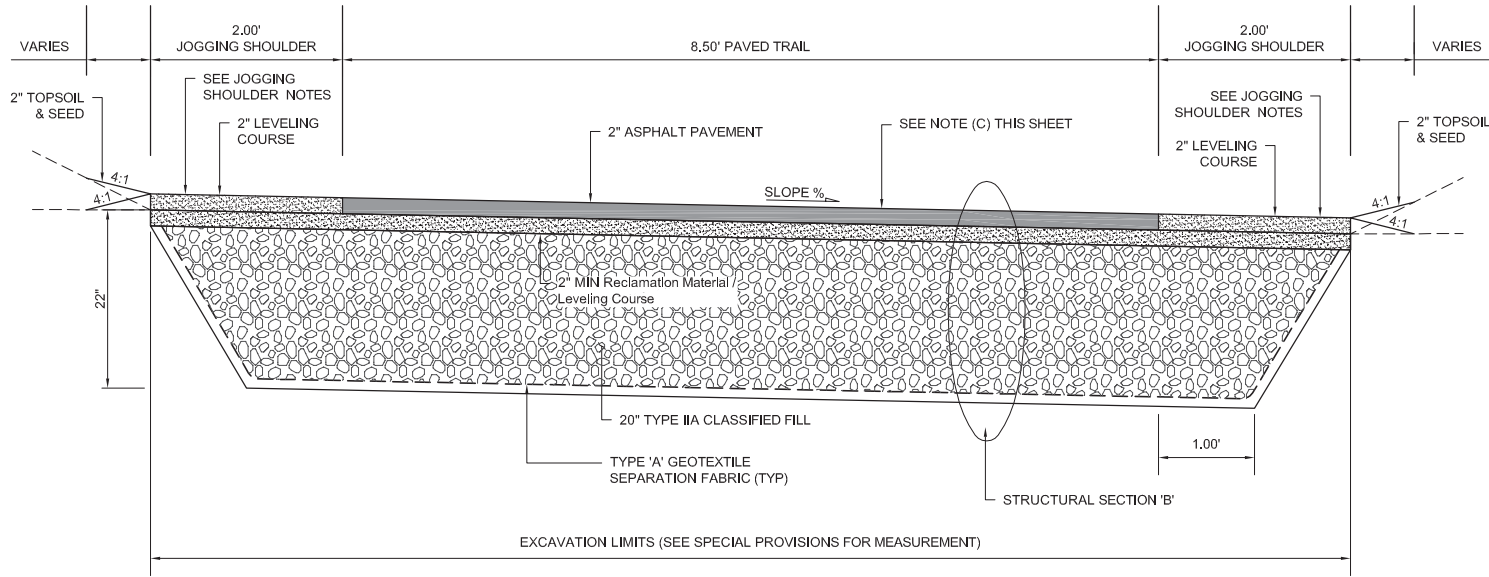
PARKS AND RECREATION DEPARTMENT
2015 CHESTER CREEK TRAIL IMPROVEMENTS
PEDESTRIAN DETOUR ROUTES



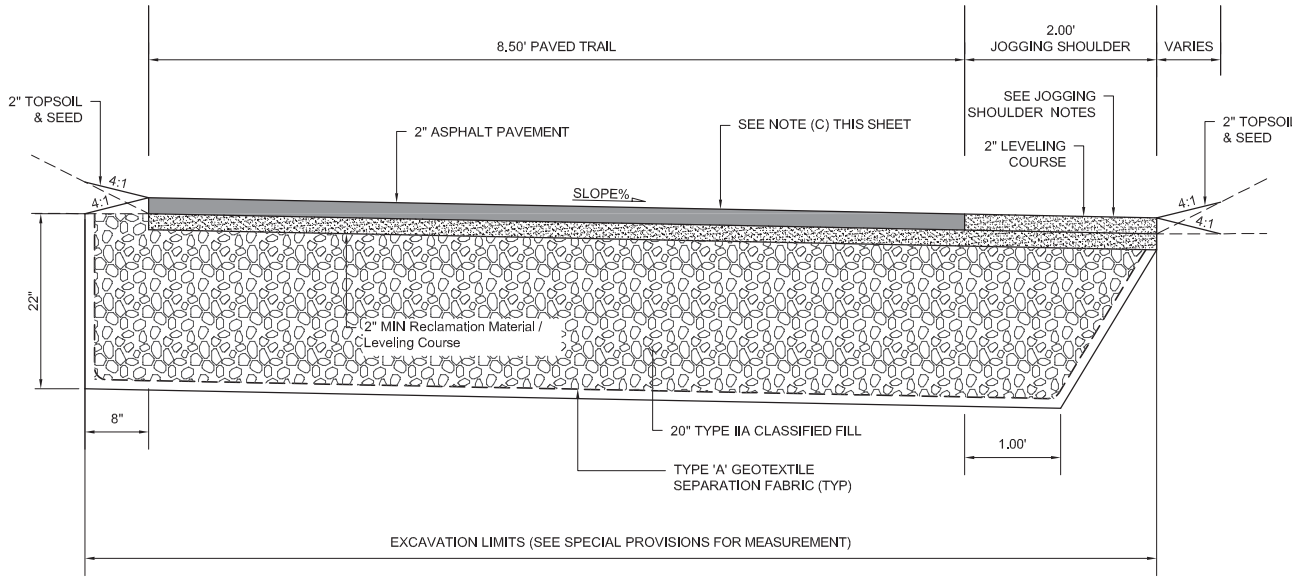
TRAIL RECLAMATION TYPICAL SECTION - JOGGING SHOULDER BOTH SIDES
(CHESTER CREEK TRAIL "CC")



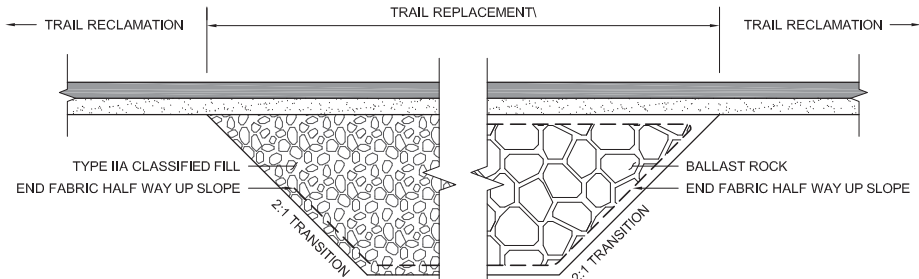
TRAIL RECLAMATION TYPICAL SECTION - ONE JOGGING SHOULDER
(CHESTER CREEK TRAIL "CC")



TRAIL REPLACEMENT TYPICAL SECTION - JOGGING SHOULDER BOTH SIDES
(CHESTER CREEK TRAIL "CC")



TRAIL REPLACEMENT TYPICAL SECTION - JOGGING SHOULDER ONE SIDE
(CHESTER CREEK TRAIL "CC")



TRAIL RECLAMATION TRANSITION TO REPLACEMENT (LONGITUDINAL
TRAIL SECTION VIEW)

TYPICAL SECTION NOTES:

- FOR RECLAMATION AND REMOVE AND REPLACE PAVEMENT AREAS NOT WITHIN TYPICAL SECTION, I.E. TRANSITIONS, TRAIL RETURNS, TIE-INS, REFER TO STRUCTURAL SECTION 'A'.
- FOR REPLACEMENT AREAS NOT WITHIN TYPICAL SECTION, I.E. TRANSITIONS, TRAIL RETURNS, TIE-INS, REFER TO STRUCTURAL SECTION 'B'.
- CROSS SLOPES SHALL BE MINIMUM OF 1% AND A MAXIMUM OF 2%. ENGINEER TO VERIFY PRIOR TO PAVING. FOR SLOPE TRANSITIONS AT TIE-INS, INTERSECTIONS AND NON-TYPICAL AREAS, ADJUST LEVELING COURSE GRADE LONGITUDINALLY TO PERPETUATE DRAINAGE. FINISHED CROSS SLOPES EXCEEDING THE TOLERANCES SHALL BE REMOVED AT THE EXPENSE OF THE CONTRACTOR.
- TRAIL JOINTS - CONTRACTOR TO LIMIT JOINTS DURING PAVING. NO MORE THAN TWO (2) JOINTS AS MEASURED IN ANY DIRECTION FOR A DISTANCE OF THREE HUNDRED (300) LINEAR FEET OF TRAIL SHALL EXIST POST PAVING. IF SECTIONS OF PAVEMENT ARE REQUIRED TO BE REMOVED (SEE NOTE C ABOVE) PROVISIONS OF JOINT SEPARATION SHALL APPLY.
- SEAL ALL JOINTS IN CONFORMANCE WITH SECTION 40.05.

JOGGING SHOULDER NOTES:

- GRUBBING FOR SHOULDERS SHALL OCCUR PRIOR TO TRAIL RECLAMATION.
- IN RECLAMATION SEGMENTS, IF EXISTING LEVELING COURSE MATERIAL IS PRESENT WITHIN SHOULDER AREA, NO GRUBBING SHALL BE REQUIRED. ADD LEVELING COURSE AS NEEDED TO ACHIEVE TYPICAL SECTION.
- GRUBBING IN EXISTING GRASS AREAS SHALL BE CONDUCTED IN SUCH A MANNER TO ENSURE STRAIGHT AND TRUE OUTSIDE EDGES. CUT WITH SHARP MECHANIZED EQUIPMENT DESIGNED FOR CUTTING SOD.
- THE TOP 2-INCHES OF SHOULDER AREA SHALL BE LEVELING COURSE. RECLAMATION MATERIAL IS NOT PERMITTED AT THE TOP 2-INCHES.
- CONTRACTOR SHALL SUBMIT WORK PLAN FOR SHOULDER WORK TO ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
- ADJUST TRAIL LIGHT JUNCTION BOXES LOCATED WITHIN SHOULDER AREA.

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| STAKING | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | |
| | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | |
| ASBUILT: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | |
| CONTRACTOR: | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | |
| INSPECTOR: | | | | WATER | --- | | QUANTITIES | TBC | | | | | | |
| | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | | | | | | | | | | | | | | |

65%
SUBMITTAL

Department of
Parks and
Recreation

Municipality of
Anchorage

Healthy
People

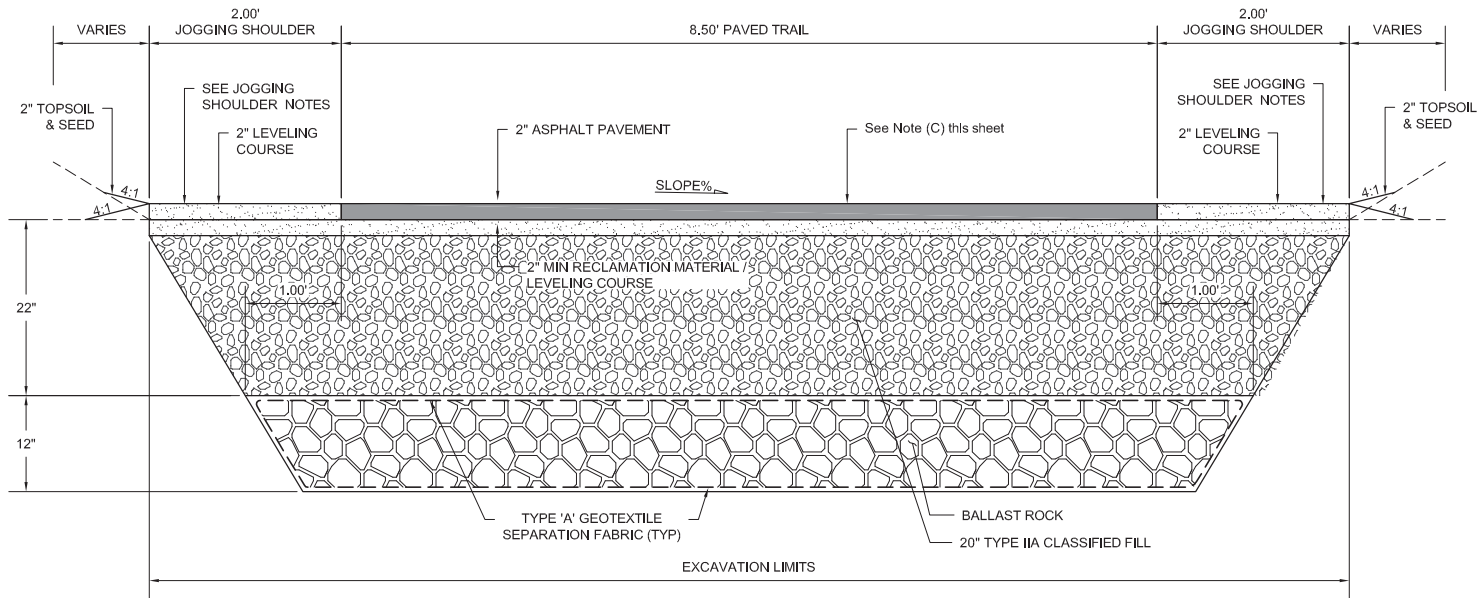
PARKS AND RECREATION DEPARTMENT
2015 CHESTER CREEK TRAIL IMPROVEMENTS
CHESTER CREEK TRAIL
TYPICAL SECTIONS

SCALE: HOR. PER SHEET
VER. PER SHEET

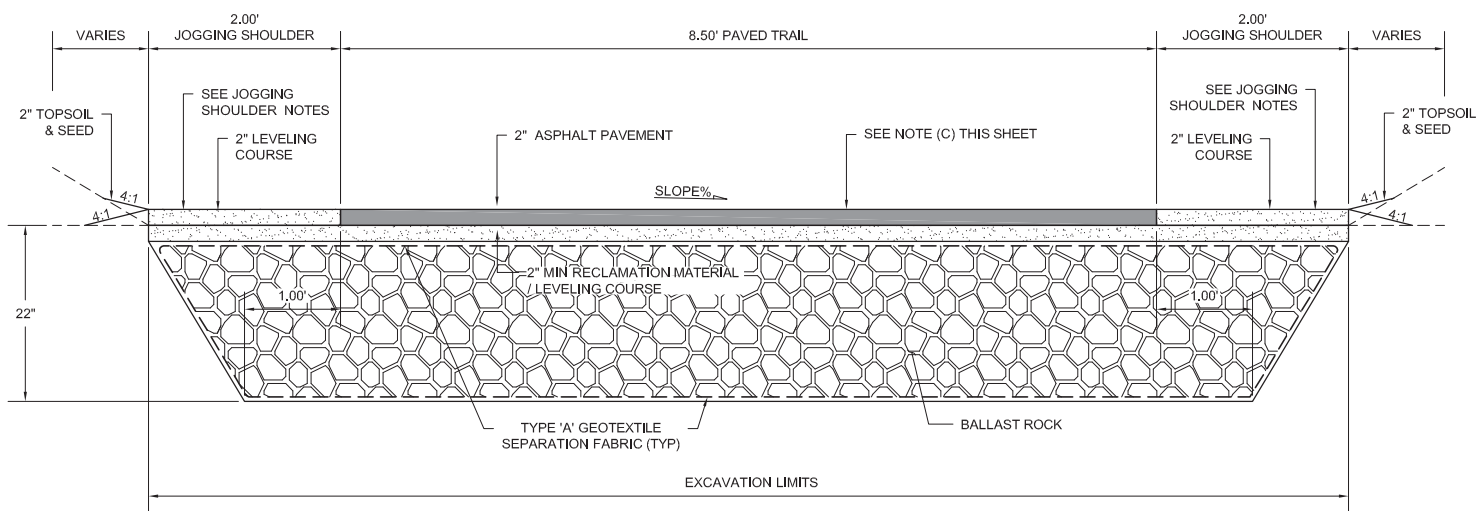
DATE: 12/15/2014
STATUS: 65% PS&E

GRID: N/A

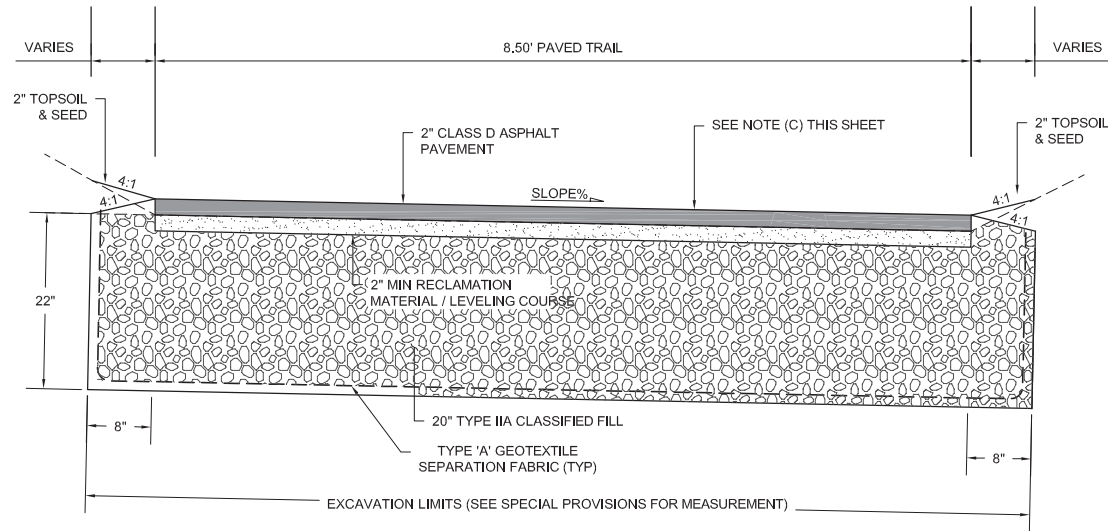
SHEET D1 of D4



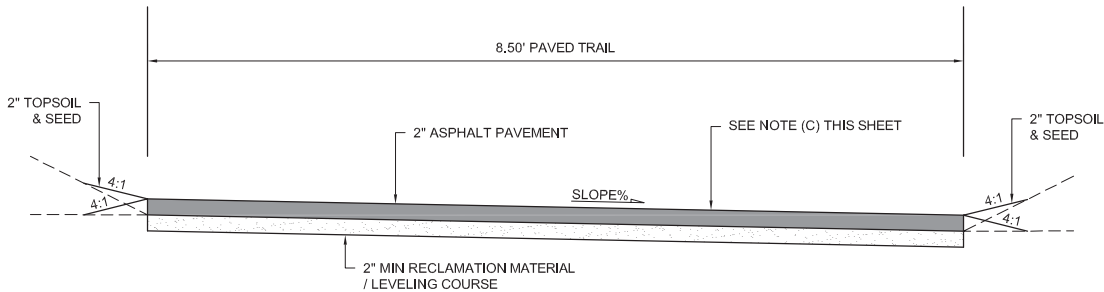
OVER-EXCAVATION SECTION
(CHESTER CREEK "CC")



ALTERNATE TRAIL REPLACEMENT SECTION (BRIDGING SECTION)
(CHESTER CREEK "CC")



SPUR TRAIL REPLACEMENT TYPICAL SECTION



SPUR TRAIL RECLAMATION TYPICAL SECTION

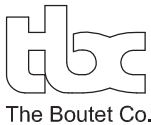
TYPICAL SECTION NOTES:

- FOR RECLAMATION AND REMOVE AND REPLACE PAVEMENT AREAS NOT WITHIN TYPICAL SECTION, I.E. TRANSITIONS, TRAIL RETURNS, TIE-INS, REFER TO STRUCTURAL SECTION 'A'.
- FOR REPLACEMENT AREAS NOT WITHIN TYPICAL SECTION, I.E. TRANSITIONS, TRAIL RETURNS, TIE-INS, REFER TO STRUCTURAL SECTION 'B'.
- CROSS SLOPES SHALL BE MINIMUM OF 1% AND A MAXIMUM OF 2%. ENGINEER TO VERIFY PRIOR TO PAVING. FOR SLOPE TRANSITIONS AT TIE-INS, INTERSECTIONS AND NON-TYPICAL AREAS, ADJUST LEVELING COURSE GRADE LONGITUDINALLY TO PERPETUATE DRAINAGE. FINISHED CROSS SLOPES EXCEEDING THE TOLERANCES SHALL BE REMOVED AT THE EXPENSE OF THE CONTRACTOR.
- TRAIL JOINTS - CONTRACTOR TO LIMIT JOINTS DURING PAVING, NO MORE THAN TWO (2) JOINTS AS MEASURED IN ANY DIRECTION FOR A DISTANCE OF THREE HUNDRED (300) LINEAR FEET OF TRAIL SHALL EXIST POST PAVING. IF SECTIONS OF PAVEMENT ARE REQUIRED TO BE REMOVED (SEE NOTE C ABOVE) PROVISIONS OF JOINT SEPARATION SHALL APPLY.
- SEAL ALL JOINTS IN CONFORMANCE WITH SECTION 40.05.

JOGGING SHOULDER NOTES:

- GRUBBING FOR SHOULDERS SHALL OCCUR PRIOR TO TRAIL RECLAMATION.
- IN RECLAMATION SEGMENTS, IF EXISTING LEVELING COURSE MATERIAL IS PRESENT WITHIN SHOULDER AREA, NO GRUBBING SHALL BE REQUIRED, ADD LEVELING COURSE AS NEEDED TO ACHIEVE TYPICAL SECTION.
- GRUBBING IN EXISTING GRASS AREAS SHALL BE CONDUCTED IN SUCH A MANNER TO ENSURE STRAIGHT AND TRUE OUTSIDE EDGES. CUT WITH SHARP MECHANIZED EQUIPMENT DESIGNED FOR CUTTING SOD.
- THE TOP 2-INCHES OF SHOULDER AREA SHALL BE LEVELING COURSE. RECLAMATION MATERIAL IS NOT PERMITTED AT THE TOP 2-INCHES.
- CONTRACTOR SHALL SUBMIT WORK PLAN FOR SHOULDER WORK TO ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
- ADJUST TRAIL LIGHT JUNCTION BOXES LOCATED WITHIN SHOULDER AREA.

| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
|---------------------|--------|----------------|-------|----------------|----------|------------|------------------|----------|------------|-----|------|-------------|----|-----|
| DESIGN: | | | | BASE | TBC | | TELEPHONE | --- | | | | | | |
| STAKING: | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | |
| ASBUILT: | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | |
| CONTRACTOR: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | |
| INSPECTOR: | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | |
| | | | | WATER | --- | | QUANTITIES | TBC | | | | | | |
| | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | | VERTICAL DATUM | | PLAN CHECK | | | REVISIONS | | | | | | | |



The Boutet Co.

65%
SUBMITTAL

Department of
Parks and
Recreation

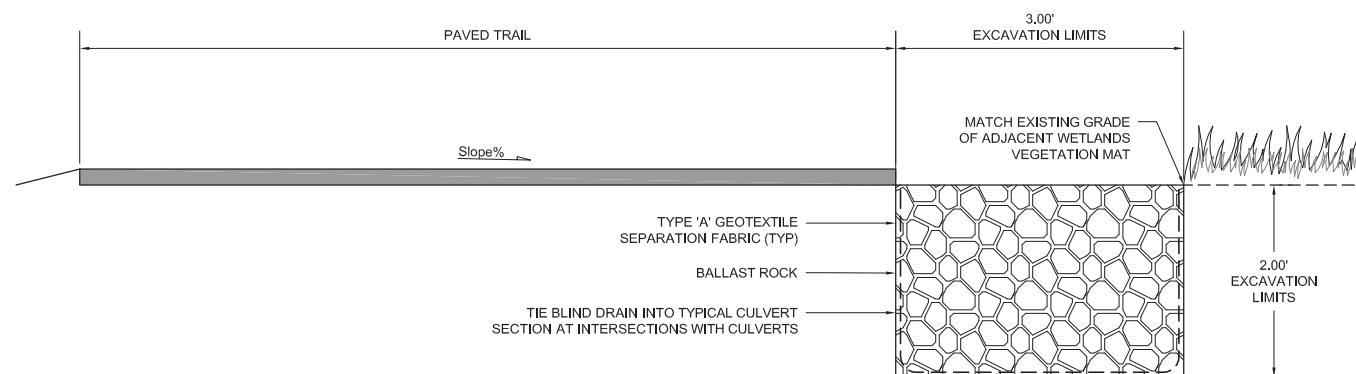
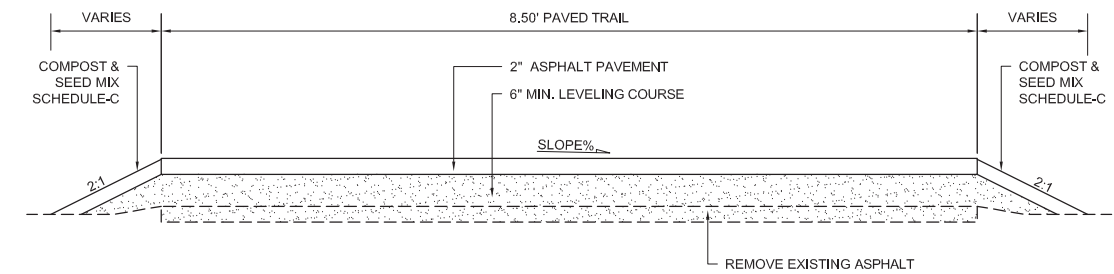
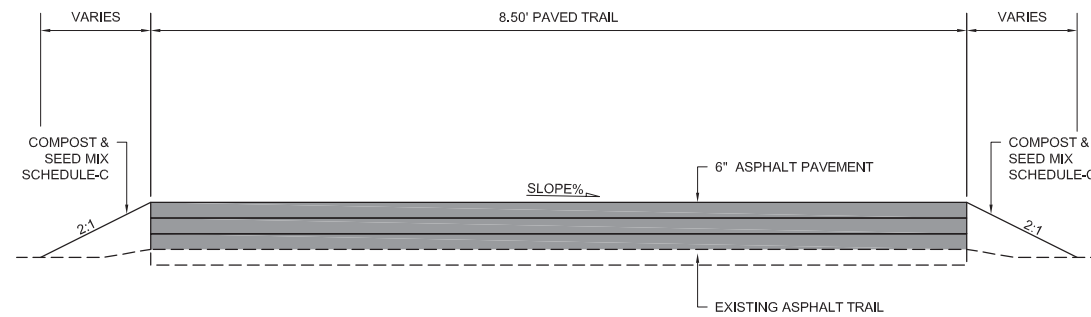
Municipality of
Anchorage



PARKS AND RECREATION DEPARTMENT




2015 CHESTER CREEK TRAIL IMPROVEMENTS
CHESTER CREEK TRAIL
TYPICAL SECTIONS

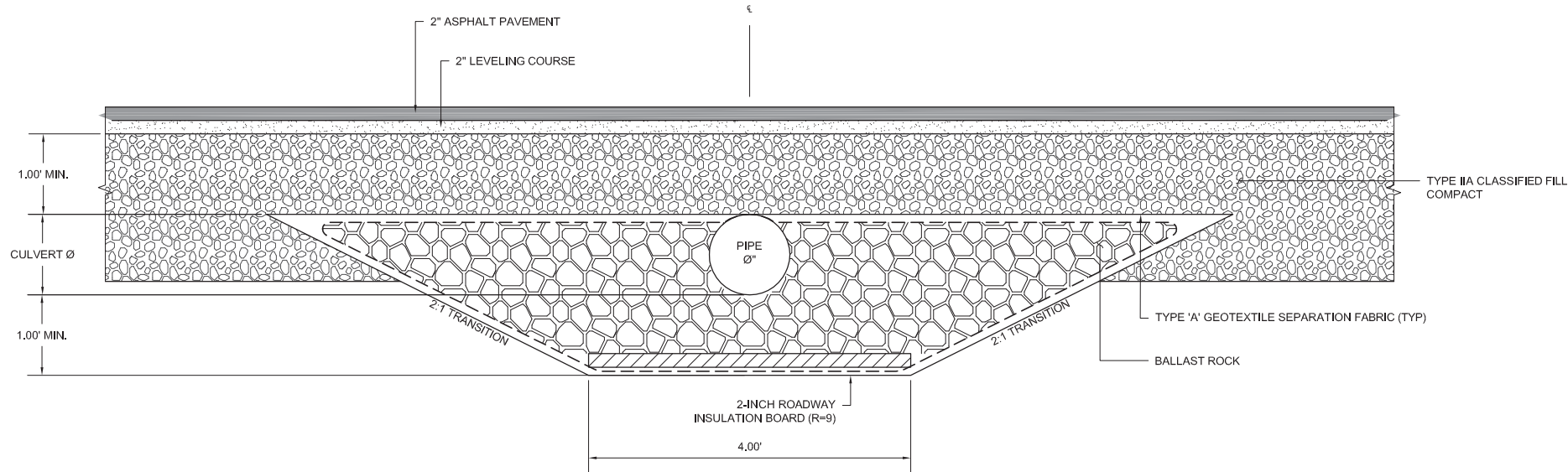
SCALE HOR. PER SHEET VER. PER SHEET DATE: 12/15/2014 STATUS: 65% PS&E GRID: N/A SHEET D2 of D4



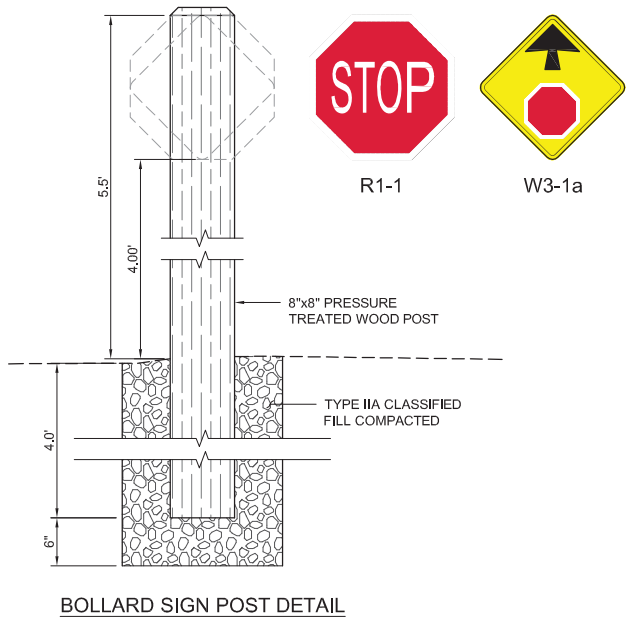
TYPICAL SECTION NOTES:

- A. CROSS SLOPES SHALL BE MINIMUM OF 1% AND A MAXIMUM OF 2%. ENGINEER TO VERIFY PRIOR TO PAVING. FOR SLOPE TRANSITIONS AT TIE-INS, INTERSECTIONS AND NON-TYPICAL AREAS, ADJUST LEVELING COURSE GRADE LONGITUDINALLY TO PERPETUATE DRAINAGE. FINISHED CROSS SLOPES EXCEEDING THE TOLERANCES SHALL BE REMOVED AT THE EXPENSE OF THE CONTRACTOR.
- B. TRAIL JOINTS - CONTRACTOR TO LIMIT JOINTS DURING PAVING, NO MORE THAN TWO (2) JOINTS AS MEASURED IN ANY DIRECTION FOR A DISTANCE OF THREE HUNDRED (300) LINEAR FEET OF TRAIL SHALL EXIST POST PAVING. IF SECTIONS OF PAVEMENT ARE REQUIRED TO BE REMOVED (SEE NOTE C ABOVE) PROVISIONS OF JOINT SEPARATION SHALL APPLY.
- C. TREE ROOTS IN SPUR TRAIL REMOVE & REPLACE ASPHALT TRAIL SECTIONS SHALL REMAIN IN PLACE WITH MINIMAL DISTURBANCE.
- D. SEAL ALL JOINTS IN CONFORMANCE WITH SECTION 40.05.

| | | | | | | | | | | | | | | | | | | | | | | |
|---------------------|--|-----------------|----------|--|----------------|----------|------------|------------------|----------|------------|-----|-----------|-------------|---|------------------|--|---|---|---------------------------------|-------|----------------|--|
| | | | | | | | | | | | | | |  The Boutet Co. | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage |  |  | PARKS AND RECREATION DEPARTMENT | | | |
| | | | | 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL TYPICAL SECTIONS | | | | | | | | | | | | | | | | | | |
| FIELD BOOKS | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | SCALE | HOR. PER SHEET VER. PER SHEET | DATE: 12/15/2014 STATUS: 65% PS&E | GRID: N/A | SHEET | D3 of D4 | |
| DESIGN: | | | | | BASE | TBC | | TELEPHONE | -- | | | | | | | | | | | | | |
| | | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | | | | | | | | |
| STAKING: | | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | | | | | | | | |
| | | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | | | | | | | | |
| ASBUILT: | | | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | | | | | | | | |
| CONTRACTOR: | | BASIS OF DATUM: | | | WATER | -- | | QUANTITIES | TBC | | | | | | | | | | | | | |
| INSPECTOR: | | | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | | PLAN CHECK | | | | REVISIONS | | | | CONSULTANT | | SEAL | | | | |



CULVERT REPLACEMENT TYPICAL SECTION



CULVERT SUMMARY TABLE

| CULVERT # | PIPE Ø | LENGTH | CL STATION | RIGHT STATION | RIGHT OFFSET | RIGHT INVERT | LEFT STATION | LEFT OFFSET | LEFT INVERT | ALIGNMENT | REMARKS |
|-----------|--------|--------|------------|---------------|--------------|--------------|--------------|-------------|-------------|-----------|--|
| CV-1 | 12" | 22 | 125+04.37 | 125+04.84 | 11.21' | 87.50 | 125+03.95 | 9.58' | 87.37 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-2 | 12" | 28 | 132+80.86 | 132+79.42 | 11.42' | 89.82 | 132+82.74 | 14.65' | 88.65 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-3 | 12" | 25 | 137+53.00 | 137+55.10 | 9.36' | 89.82 | 137+49.75 | 15.05' | 88.45 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-4 | 12" | 22 | 140+01.57 | 140+00.70 | 11.44' | 98.65 | 140+02.27 | 9.2' | 97.94 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-5 | 12" | 30 | 142+37.53 | 142+39.72 | 13.28' | 97.03 | 142+35.07 | 15.02' | 96.04 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-6 | 12" | 28 | 143+34.86 | 143+34.85 | 11.48' | 97.46 | 143+34.88 | 15.75' | 96.52 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-7 | 12" | 30 | 144+35.34 | 144+35.98 | 17.26' | 97.49 | 144+34.90 | 11.76' | 96.96 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-8 | 12" | 52 | 146+53.00 | 146+52.14 | 11.48' | 101.6 | 146+54.03 | 13.66' | 100.07 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-9 | 12" | 50 | 146+80.10 | 146+79.66 | 11.19' | 101.23 | 146+80.62 | 13.19' | 100.03 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-10 | 12" | 48 | 147+47.64 | 147+47.88 | 11.52' | 102.16 | 147+47.41 | 110.1' | 101.33 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-11 | 12" | 50 | 147+93.33 | 147+90.28 | 7.84' | 103.22 | 147+98.95 | 14.9' | 101.38 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-12 | 12" | 46 | 148+95.36 | 148+95.51 | 11.08' | 101.34 | 148+95.21 | 10.98' | 101.05 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-13 | 12" | 68 | 149+60.66 | 149+60.84 | 16.74' | 102.23 | 149+60.47 | 16.67' | 101.14 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-14 | 12" | 56 | 149+96.37 | 149+96.02 | 10.54' | 101.59 | 149+96.90 | 16.23' | 101.28 | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-15 | 12" | 24 | 171+90.04 | 171+89.65 | 11.05' | 117.47 | 171+90.58 | 12.42' | ??? | CC | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-16 | 24 | 22 | 11+18.59 | 11+16.65 | 10.22' | 141.95 | 11+20.90 | 10.27' | 141.06 | ARCA | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-17 | 12" | 20 | 12+02.22 | 12+02.22 | 10.25' | 141.26 | 12+02.22 | 9.75' | 141.06 | ARCA | PROVIDE NEW CULVERT |
| CV-18 | 12" | 16 | 12+68.11 | 12+64.31 | 7.78' | 141.36 | 12+71.33 | 6.6' | 141.2 | ARCA | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-19 | 12" | 20 | 13+45.02 | 13+45.02 | 8.5' | 140.84 | 13+45.02 | 10.' | 140.64 | ARCA | PROVIDE NEW CULVERT |
| CV-20 | 12" | 28 | 14+33.09 | 14+21.21 | 7.8' | 140.77 | 14+43.72 | 8.74' | 140.49 | ARCA | PROVIDE NEW CULVERT |
| CV-21 | 12" | 30 | 15+21.08 | 15+32.26 | 10.' | 140.29 | 15+09.90 | 10.' | 139.29 | ARCA | PROVIDE NEW CULVERT |
| CV-22 | 12" | 20 | 16+13.84 | 16+14.16 | 8.53' | 142.36 | 16+13.50 | 9.36' | 142.29 | ARCA | REMOVE & DISPOSE OF PIPE AND STRUCTURE |
| CV-23 | 12" | 20 | 19+37.91 | 19+38.07 | 8.63' | 143.9 | 19+37.70 | 11.15' | 144.17 | ARCA | REMOVE & DISPOSE OF PIPE AND STRUCTURE |

FIELD BOOKS

BM NO.

LOCATION

ELEV.

DESIGN

STAKING

ASBUILT:

CONTRACTOR:

INSPECTOR:

BASIS OF DATUM:

VERTICAL DATUM

DATA

BASE

TOPOGRAPHY

PROFILE

SANITARY SEWER

STORM SEWER

WATER

GAS

DRAWN BY

TBC

TBC

TBC

CHECKED BY

DATA

TELEPHONE

ELECTRIC

CABLE TV

TRAFFIC SIGNAL

DESIGN

QUANTITIES

MUN. FINAL CHECK

DRAWN BY

TBC

TBC

CHECKED BY

REV

DATE

DESCRIPTION

BY

REV

CONSTRUCTION RECORD

PLAN CHECK

REVISIONS

The Boutet Co.

CONSULTANT

65%
SUBMITTAL

SEAL

Department of
Parks and
Recreation

Municipality of
Anchorage

Healthy
Parks

Healthy
People

MUNICIPALITY OF ANCHORAGE

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CHESTER CREEK TRAIL

TYPICAL SECTIONS & CULVERT SUMMARY

SCALE

HOR. PER SHEET

VER. PER SHEET

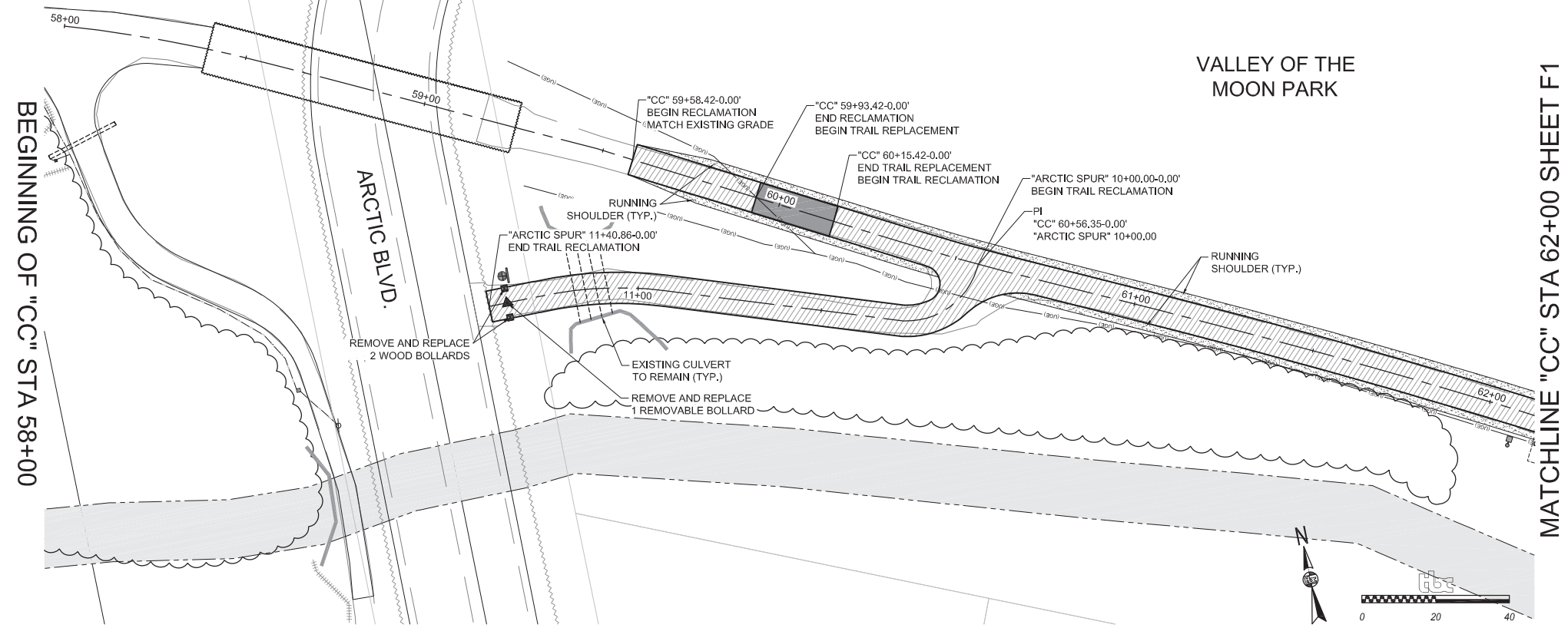
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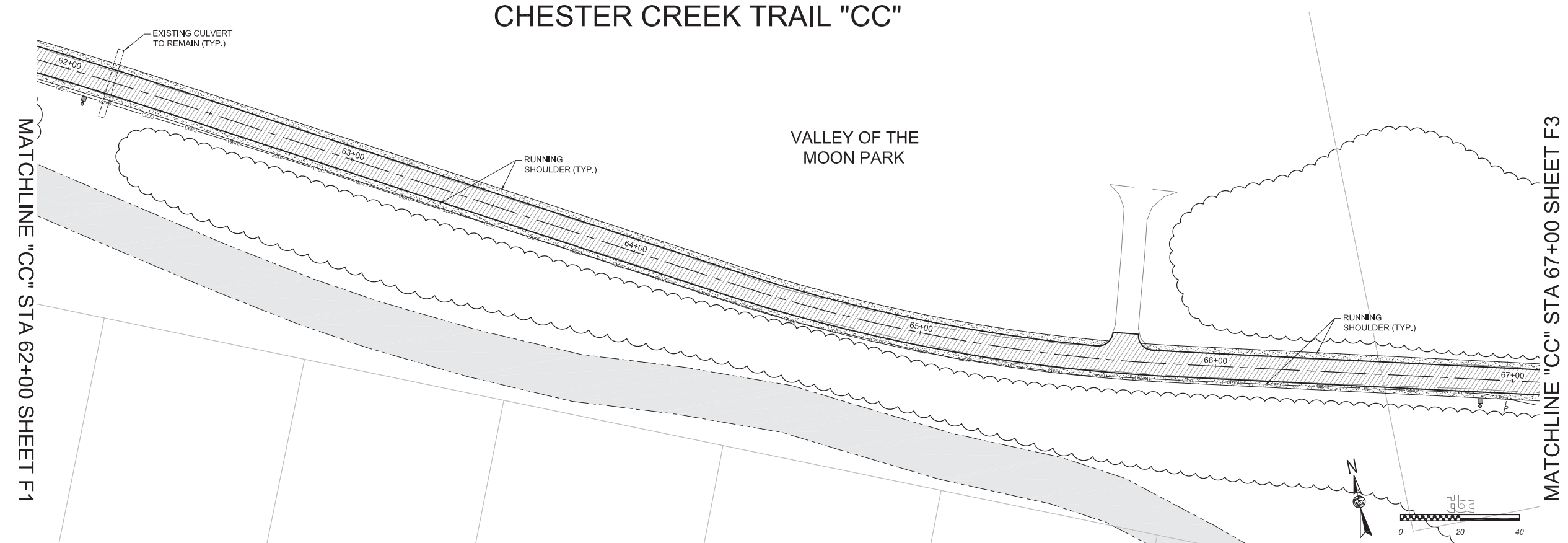
SHEET

D4
of
D4

CHESTER CREEK TRAIL "CC"



CHESTER CREEK TRAIL "CC"



| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
|---------------------|--------|----------|-------|----------------|----------|------------|------------------|-----------|------------|-----|------|-------------|----|-----|
| DESIGN: | | | | BASE | TBC | | TELEPHONE | --- | | | | | | |
| STAKING: | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | |
| | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | |
| ASBUILT: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | |
| CONTRACTOR: | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | |
| INSPECTOR: | | | | WATER | --- | | QUANTITIES | TBC | | | | | | |
| CONSTRUCTION RECORD | | | | GAS | --- | | MUN. FINAL CHECK | --- | | | | | | |
| BASIS OF DATUM: | | | | PLAN CHECK | | | | REVISIONS | | | | CONSULTANT | | |
| VERTICAL DATUM | | | | | | | | | | | | SEAL | | |



The Boutet Co.

65%
SUBMITTAL

Department of
Parks and
Recreation

Healthy
Parks



Healthy
People



MUNICIPALITY OF ANCHORAGE

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CHESTER CREEK TRAIL

"CC" 58+00 TO 67+00

SCALE

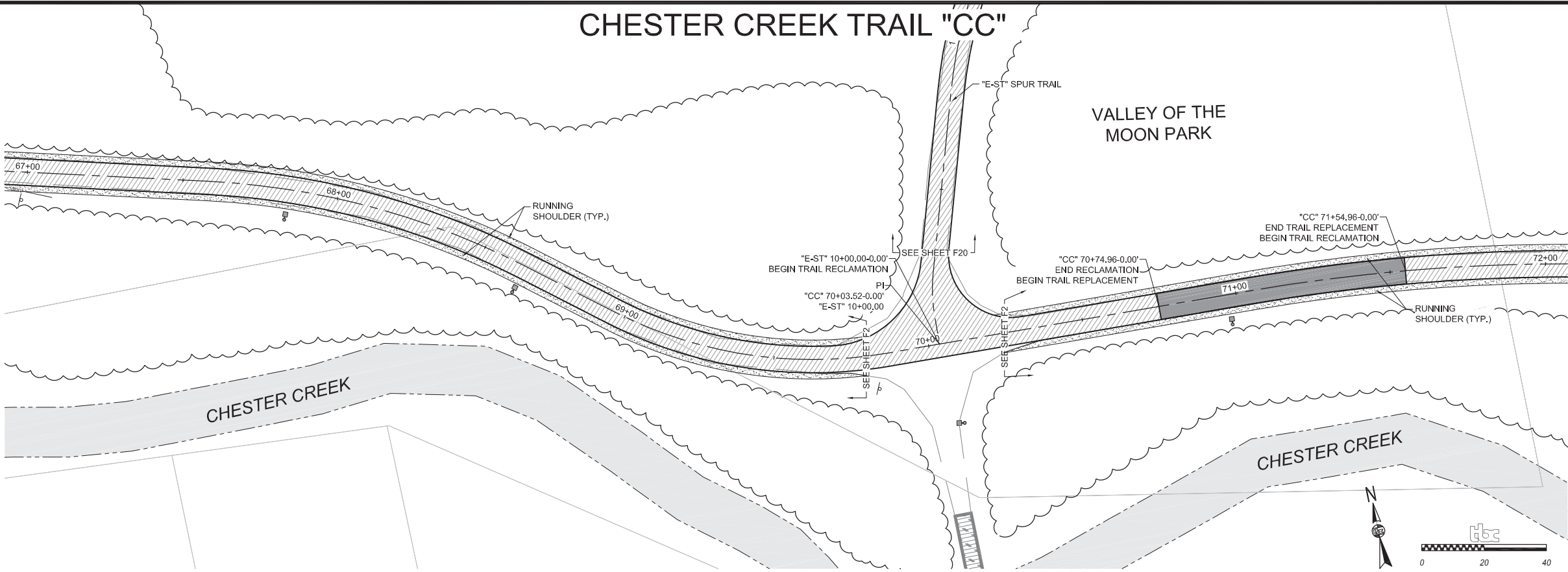
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| HOR. PER SHEET | DATE: 12/15/2014 | GRID: N/A |
| VER. PER SHEET | STATUS: 65% PS&E | |

SHEET

F1 of F25

CHESTER CREEK TRAIL "CC"

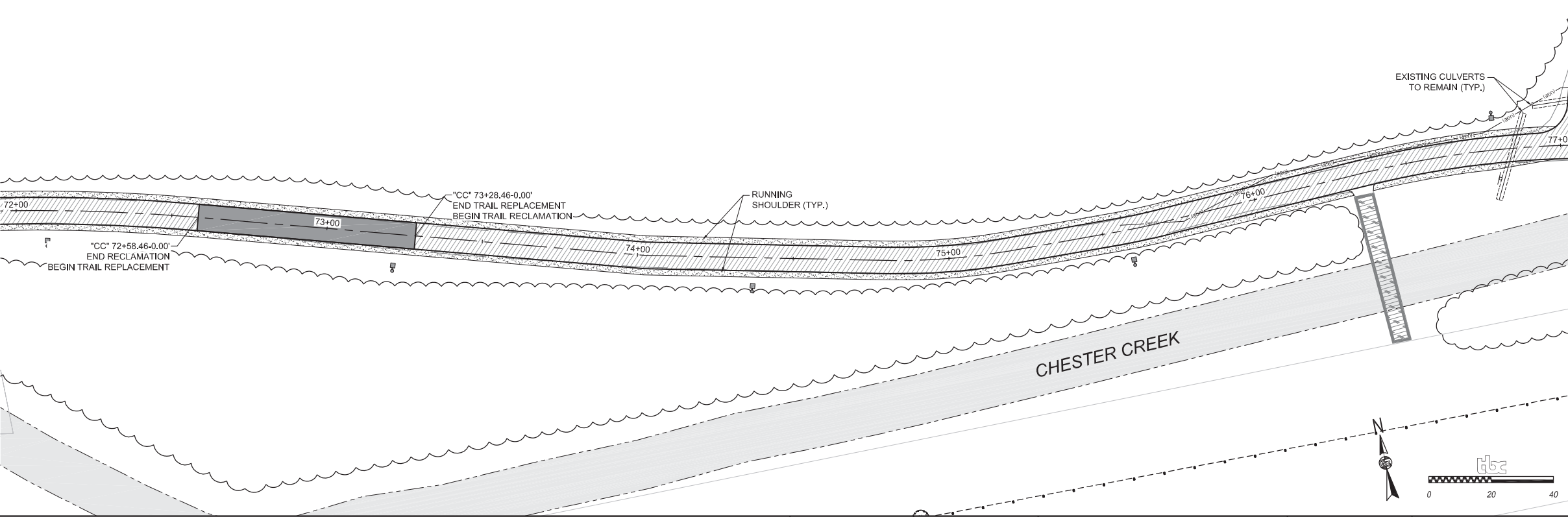
MATCHLINE "CC" STA 67+00 SHEET F2






MATCHLINE "CC" STA 72+00 SHEET F3

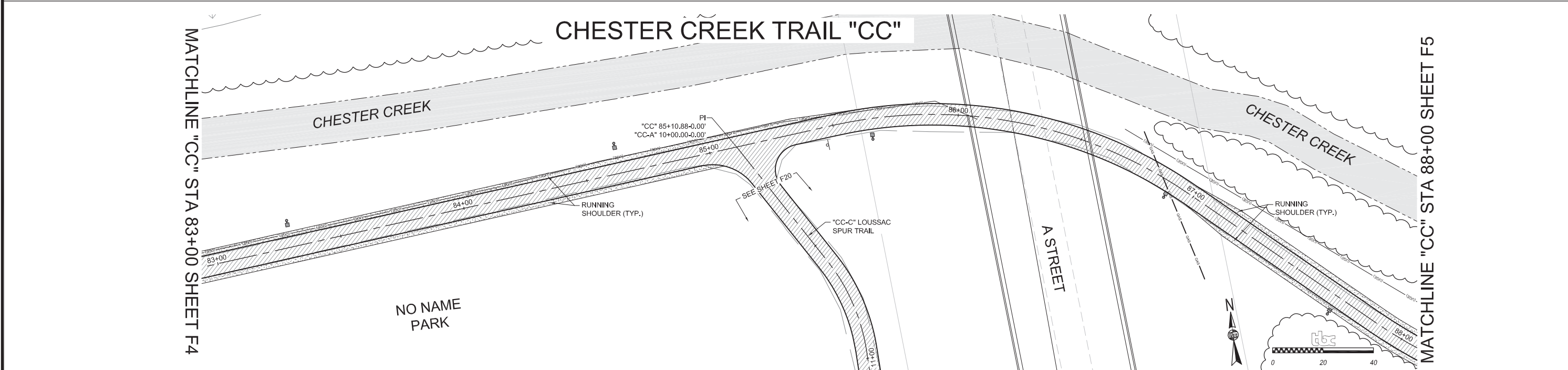
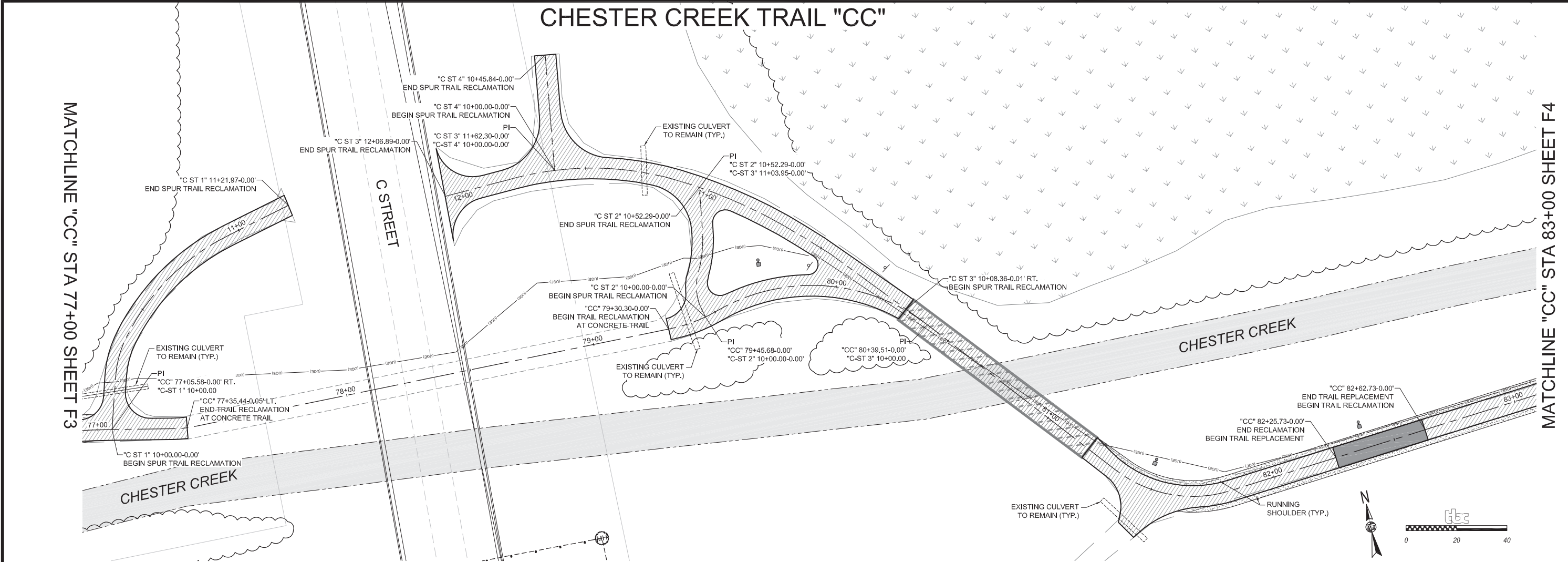
CHESTER CREEK TRAIL "CC"




MATCHLINE "CC" STA 72+00 SHEET F3

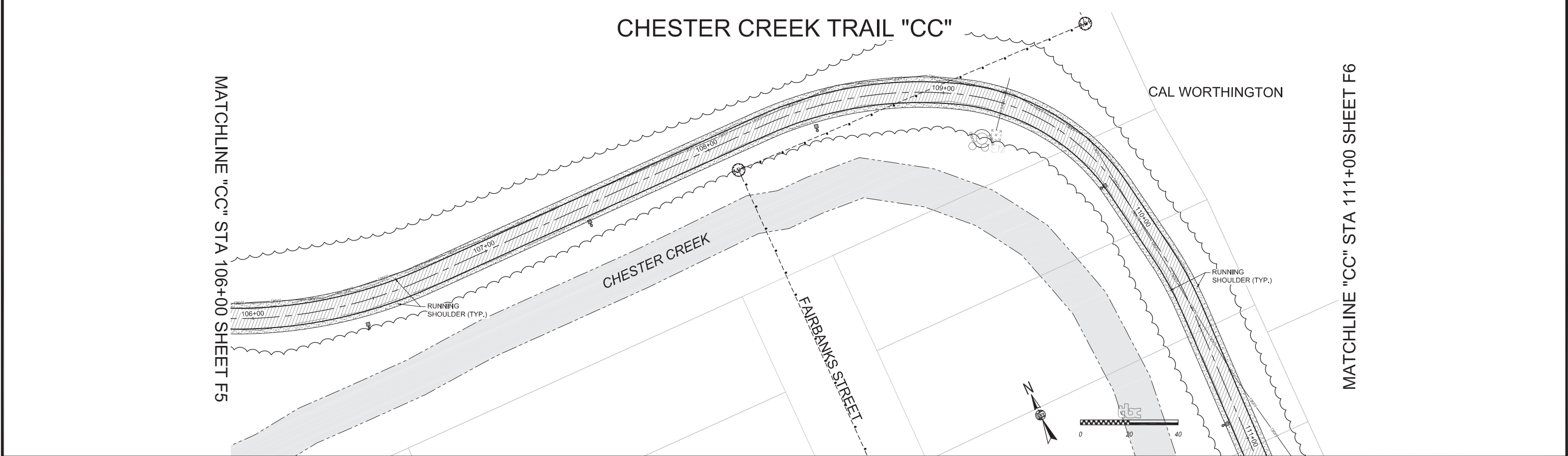
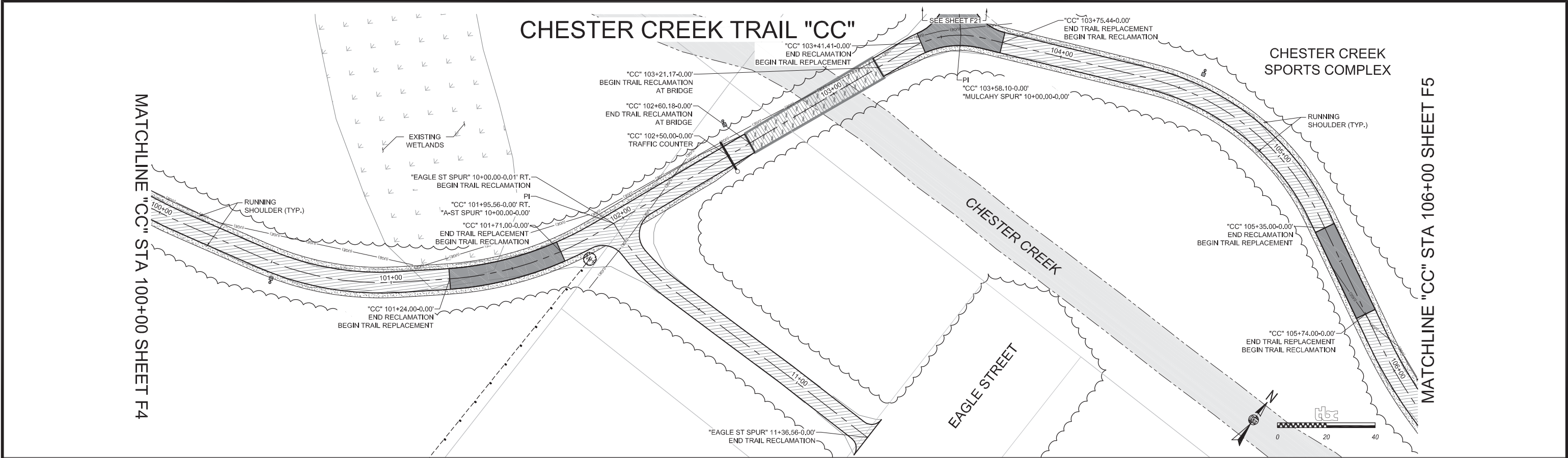





MATCHLINE "CC" STA 77+00 SHEET F4

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|---|-----------------|----------------------------------|--------------------------------------|----------------|----------|------------|------------------|----------|------------|-------------|--------|-------------|-------|------|----------|------------|------|----------|------------|-----|------|-------------|----|-----|--------|--|--|--|------|-----|--|-----------|-----|--|--|--|--|--|--|---------|--|--|--|------------|-----|--|----------|-----|--|--|--|--|--|--|--|--|--|--|---------|-----|--|----------|-----|--|--|--|--|--|--|----------|--|--|--|----------------|-----|--|----------------|-----|--|--|--|--|--|--|-------------|-----------------|--|--|-------------|-----|--|--------|-----|--|--|--|--|--|--|------------|--|--|--|-------|-----|--|------------|-----|--|--|--|--|--|--|---------------------|----------------|--|--|-----|-----|--|------------------|--|--|--|--|--|--|--|---|------------------|--|---|---|---------------------------------|--|
| <table border="1"><tr><td>FIELD BOOKS</td><td>BM NO.</td><td>LOCATION</td><td>ELEV.</td><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td><td>REV</td><td>DATE</td><td>DESCRIPTION</td><td>BY</td><td>REV</td></tr><tr><td>DESIGN</td><td></td><td></td><td></td><td>BASE</td><td>TBC</td><td></td><td>TELEPHONE</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>STAKING</td><td></td><td></td><td></td><td>TOPOGRAPHY</td><td>TBC</td><td></td><td>ELECTRIC</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td>PROFILE</td><td>TBC</td><td></td><td>CABLE TV</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>ASBUILT:</td><td></td><td></td><td></td><td>SANITARY SEWER</td><td>---</td><td></td><td>TRAFFIC SIGNAL</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>CONTRACTOR:</td><td colspan="3">BASIS OF DATUM:</td><td>STORM SEWER</td><td>---</td><td></td><td>DESIGN</td><td>TBC</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>INSPECTOR:</td><td colspan="3"></td><td>WATER</td><td>---</td><td></td><td>QUANTITIES</td><td>TBC</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>CONSTRUCTION RECORD</td><td colspan="3">VERTICAL DATUM</td><td>GAS</td><td>---</td><td></td><td>MUN. FINAL CHECK</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr></table> | | | | | | | | | | FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | DESIGN | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | STAKING | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | ASBUILT: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | CONTRACTOR: | BASIS OF DATUM: | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | INSPECTOR: | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | CONSTRUCTION RECORD | VERTICAL DATUM | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | |  The Boutet Co. | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage |  |  | PARKS AND RECREATION DEPARTMENT | |
| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| DESIGN | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STAKING | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| ASBUILT: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONTRACTOR: | BASIS OF DATUM: | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| INSPECTOR: | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | VERTICAL DATUM | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "CC" 67+00 TO 77+00 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SCALE | | HOR. PER SHEET VER. PER SHEET | DATE: 12/15/2014 STATUS: 65% PS&E | GRID: N/A | SHEET | F2 of F25 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |



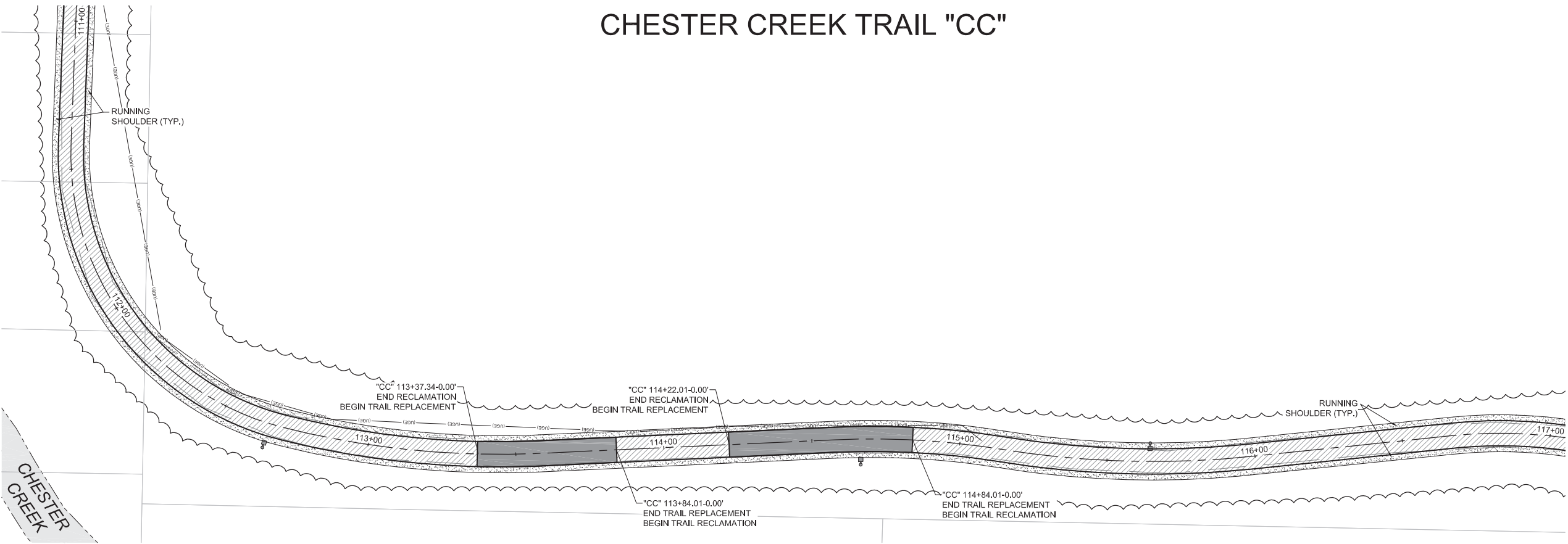
| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|-----------------|----------------------------------|-------|--------------------------------------|----------|------------|----------------|-----------------|------------|-------------|--------|-------------|-------|------|----------|------------|------|----------|------------|-----|------|-------------|----|-----|--------|--|--|--|------|-----|--|-----------|-----|--|--|--|--|--|--|---------|--|--|--|------------|-----|--|----------|-----|--|--|--|--|--|--|--|--|--|--|---------|-----|--|----------|-----|--|--|--|--|--|--|---------|--|--|--|----------------|-----|--|----------------|-----|--|--|--|--|--|--|------------|-----------------|--|--|-------------|-----|--|--------|-----|--|--|--|--|--|--|-----------|--|--|--|-------|-----|--|------------|-----|--|--|--|--|--|--|---------------------|--|--|--|----------------|--|--|--|------------|--|--|--|-----------|--|--|--|---|------------------|--|---|---|---------------------------------|--|--|--|
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| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| DESIGN | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STAKING | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| ASBUILT | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONTRACTOR | BASIS OF DATUM: | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| INSPECTOR | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | | PLAN CHECK | | | | REVISIONS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "CC" 77+00 TO 88+00 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SCALE | | HOR. PER SHEET VER. PER SHEET | | DATE: 12/15/2014 STATUS: 65% PS&E | | GRID: N/A | | SHEET F3 of F25 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |



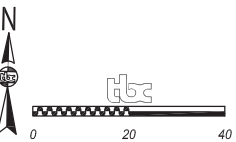
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|---|----------------------------------|--------------------------------------|----------|-------|-----------------|----------|------------|------------------|----------|------------|-----------|------|-------------|----|------------|---|------------------|--|---|---|---------------------------------|--|--|--|
| | | | | | | | | | | | | | | | |  The Boutet Co. | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage |  |  | PARKS AND RECREATION DEPARTMENT | | | |
| 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "CC" 100+00 TO 111+00 | | | | | | | | | | | | | | | | | | | | | | | | |
| SCALE | HOR. PER SHEET VER. PER SHEET | DATE: 12/15/2014 STATUS: 65% PS&E | GRID: NA | SHEET | F5 of F25 | | | | | | | | | | | | | | | | | | | |
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| FIELD BOOKS | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | | | | | | | | |
| DESIGN: | | | | | BASE | TBC | | TELEPHONE | -- | | | | | | | | | | | | | | | |
| | | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | | | | | | | | | | |
| STAKING: | | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | | | | | | | | | | |
| | | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | | | | | | | | | | |
| ASBUILT: | | | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | | | | | | | | | | |
| CONTRACTOR: | | BASIS OF DATUM: | | | WATER | -- | | QUANTITIES | TBC | | | | | | | | | | | | | | | |
| INSPECTOR: | | | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | VERTICAL DATUM | | | PLAN CHECK | | | | | | REVISIONS | | | | CONSULTANT | | SEAL | | | | | | | |

CHESTER CREEK TRAIL "CC"

MATCHLINE "CC" STA 111+50 SHEET F5

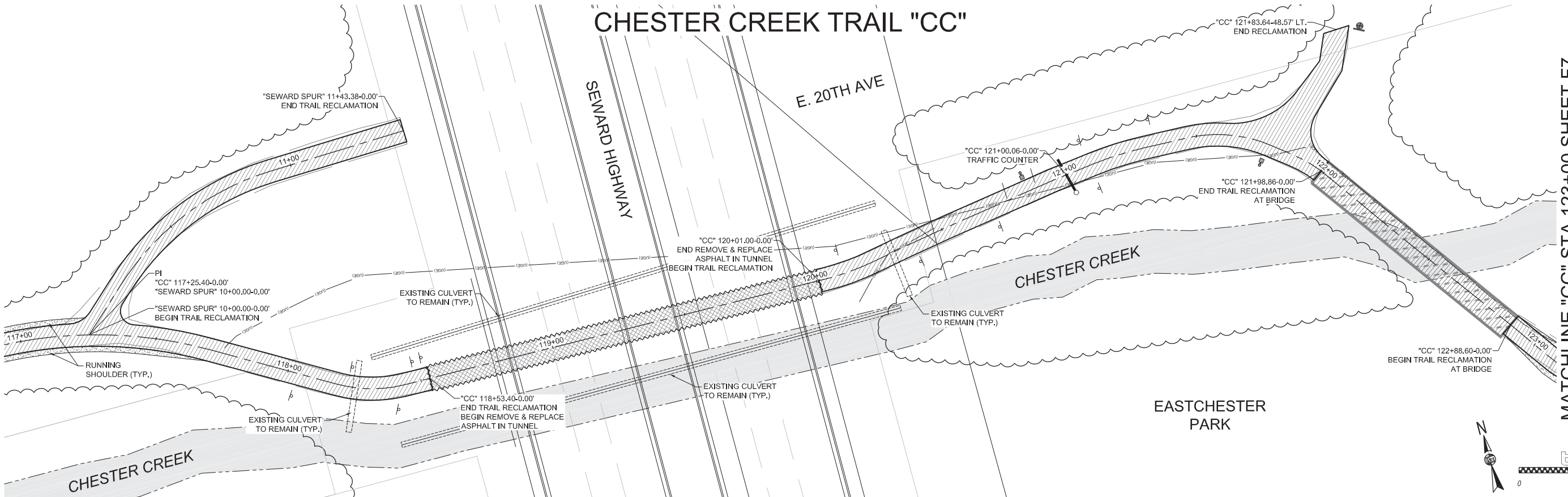


MATCHLINE "CC" STA 117+00 SHEET F6

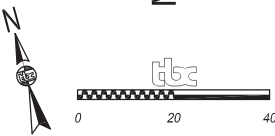


CHESTER CREEK TRAIL "CC"

MATCHLINE "CC" STA 117+00 SHEET F6



MATCHLINE "CC" STA 123+00 SHEET F7



| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
|---------------------|--------|----------------|-------|----------------|----------|------------|------------------|----------|------------|-----|------|-------------|----|-----|
| DESIGN: | | | | BASE | TBC | | TELEPHONE | --- | | | | | | |
| STAKING: | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | |
| ASBUILT: | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | |
| CONTRACTOR: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | |
| INSPECTOR: | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | |
| | | | | WATER | --- | | QUANTITIES | TBC | | | | | | |
| | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | | VERTICAL DATUM | | PLAN CHECK | | | REVISIONS | | | | | | | |



The Boutet Co.

65%
SUBMITTAL

Department of
Parks and
Recreation

Healthy
Parks



Healthy
People



MUNICIPALITY OF ANCHORAGE

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CHESTER CREEK TRAIL

"CC" 111+00 TO 123+00

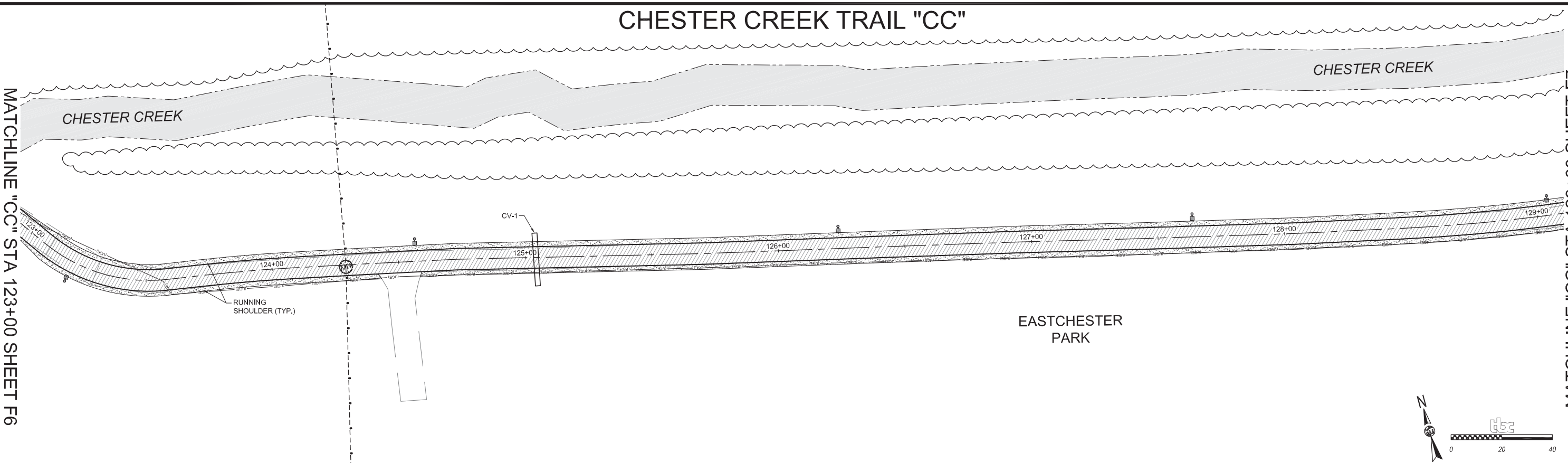
SCALE

| | | | |
|----------------|------------------|-----------|-----------------|
| HOR. PER SHEET | DATE: 12/15/2014 | GRID: N/A | SHEET F6 of F25 |
| VER. PER SHEET | STATUS: 65% PS&E | | |

CHESTER CREEK TRAIL "CC"

MATCHLINE "CC" STA 123+00 SHEET F6

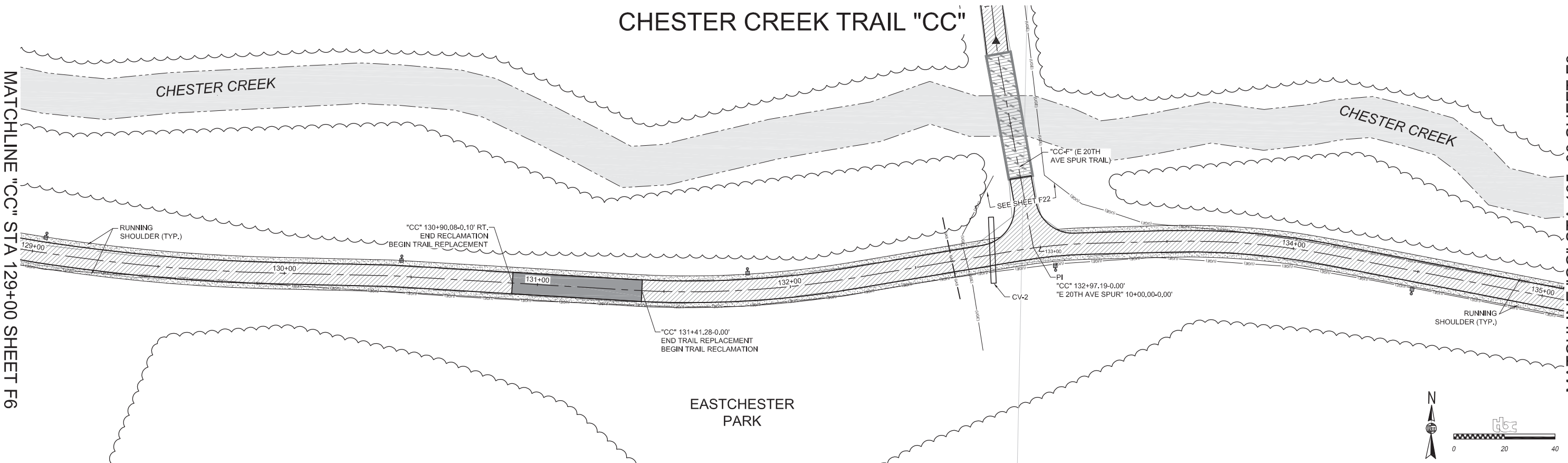
MATCHLINE "CC" STA 129+00 SHEET F7



CHESTER CREEK TRAIL "CC"

MATCHLINE "CC" STA 129+00 SHEET F6

MATCHLINE "CC" STA 135+00 SHEET F8



| FIELD BOOKS | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
|---------------------|--|-----------------|----------|-------|----------------|----------|------------|------------------|----------|------------|-----------|------|-------------|----|-----|
| DESIGN: | | | | | BASE | TBC | | TELEPHONE | -- | | | | | | |
| STAKING: | | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | |
| | | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | |
| | | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | |
| ASBUILT: | | | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | |
| CONTRACTOR: | | BASIS OF DATUM: | | | WATER | -- | | QUANTITIES | TBC | | | | | | |
| INSPECTOR: | | | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | | VERTICAL DATUM | | | PLAN CHECK | | | | | | REVISIONS | | | | |



The Boutet Co.

65%
SUBMITTAL

Department of
Parks and
Recreation

Healthy
Parks



Healthy
People



MUNICIPALITY OF ANCHORAGE

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CHESTER CREEK TRAIL

"CC" 123+00 TO 135+00

SCALE

| | | |
|----------------|------------------|-----------|
| HOR. PER SHEET | DATE: 12/15/2014 | GRID: N/A |
| VER. PER SHEET | STATUS: 65% PS&E | |

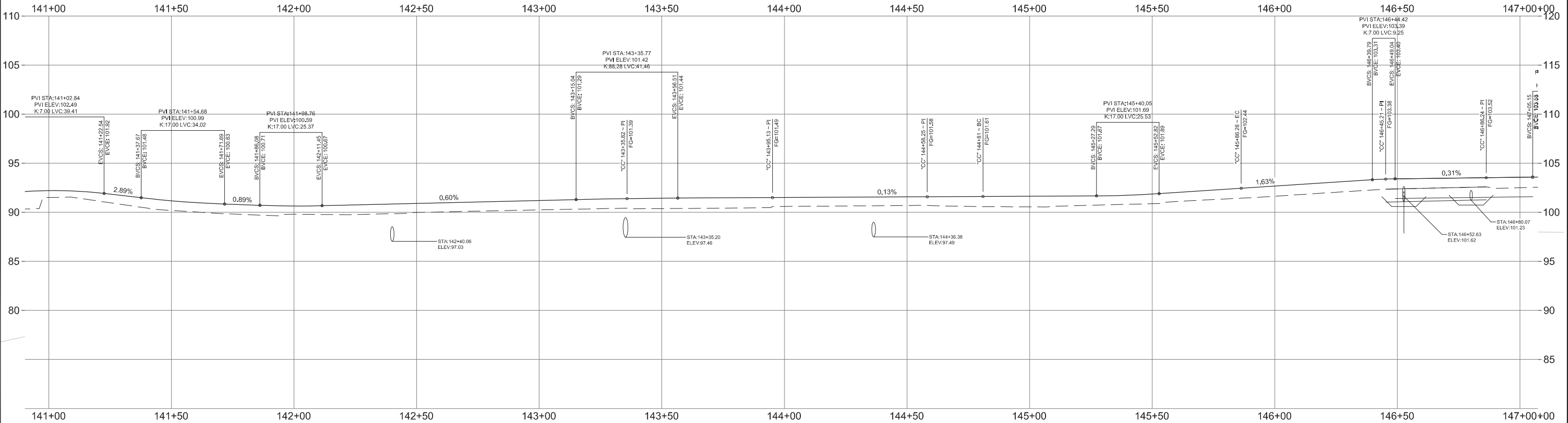
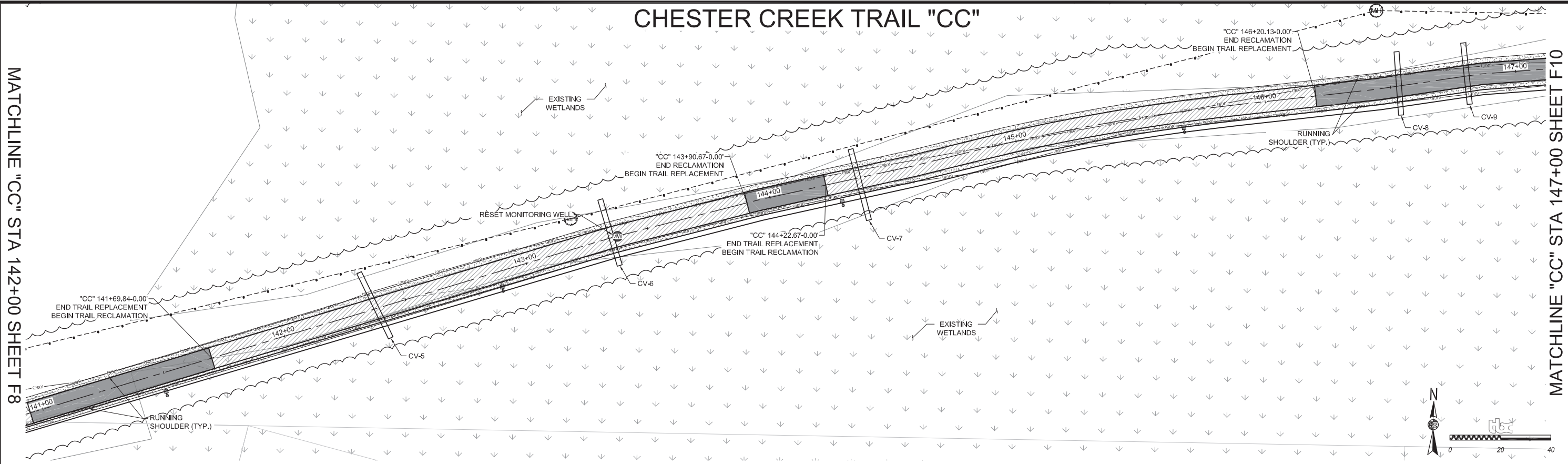
SHEET

F7 of F25

CHESTER CREEK TRAIL "CC"

MATCHLINE "CC" STA 142+00 SHEET F8

MATCHLINE "CC" STA 147+00 SHEET F10



| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
|---------------------|--------|----------|-------|----------------|----------|------------|------------------|----------|------------|-----|------|-------------|----|-----|
| DESIGN: | | | | BASE | TBC | | TELEPHONE | TBC | | | | | | |
| STAKING: | | | | TOPOGRAPHY | TBC | | ELECTRIC | TBC | | | | | | |
| ASBUILT: | | | | PROFILE | TBC | | CABLE TV | TBC | | | | | | |
| CONTRACTOR: | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | |
| INSPECTOR: | | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | |
| | | | | WATER | -- | | QUANTITIES | TBC | | | | | | |
| | | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | | | | | | | | | | | | | | |



The Boutet Co.

65%
SUBMITTAL

Department of
Parks and
Recreation

Healthy
Parks

Municipality of
Anchorage

Healthy
People



PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CHESTER CREEK TRAIL

"CC" 141+00 TO 147+00

SCALE

HOR. PER SHEET

VER. PER SHEET

DATE: 12/15/2014

GRID: N/A

STATUS: 65% PS&E

SHEET

F9

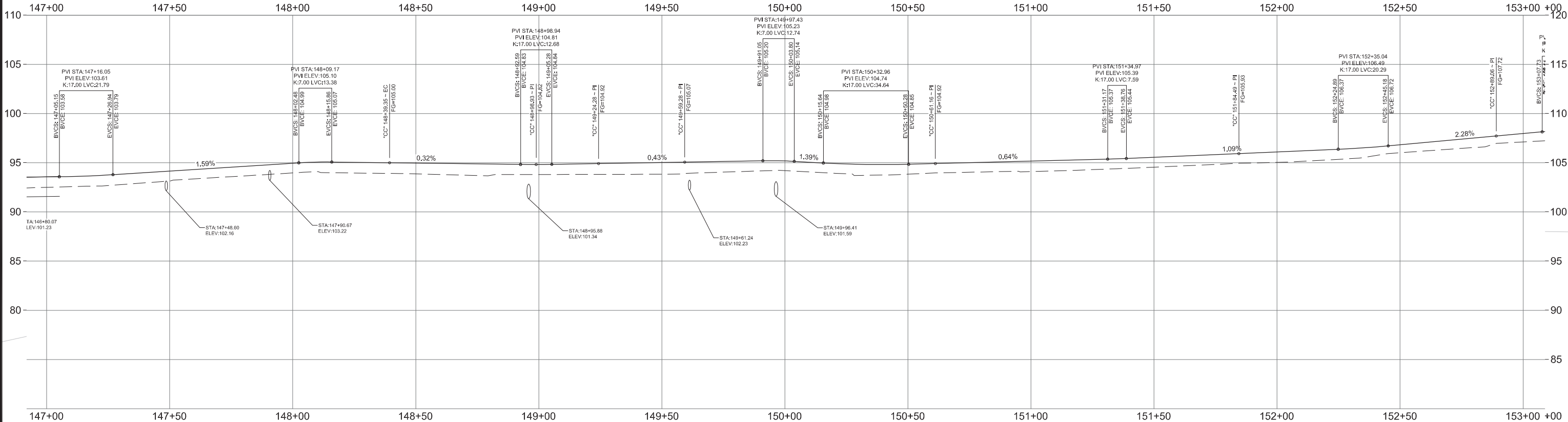
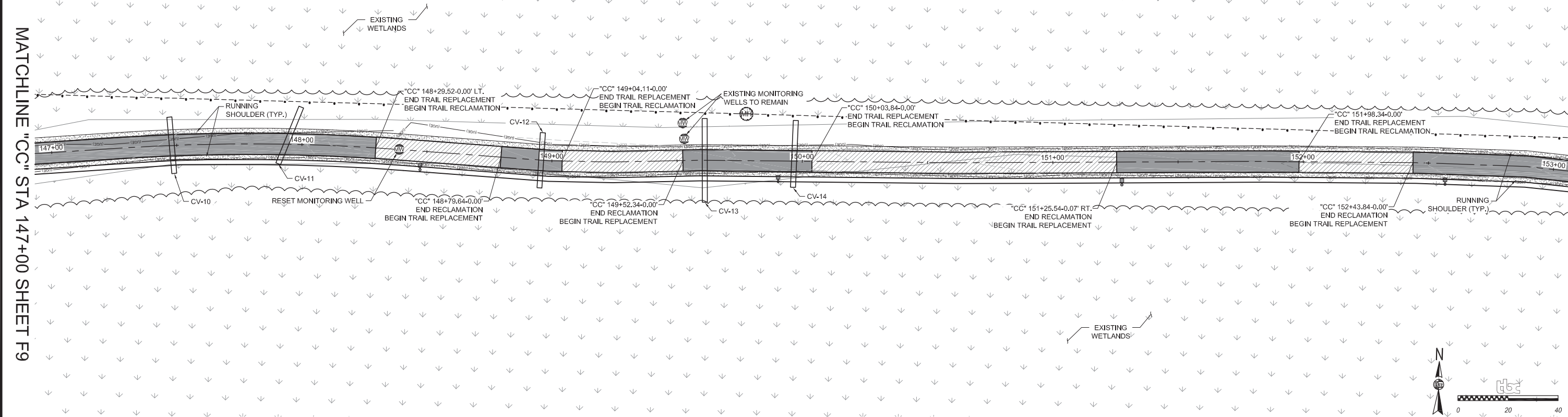
of

F26


CHESTER CREEK TRAIL "CC"

MATCHLINE "CC" STA 147+00 SHEET F9

MATCHLINE "CC" STA 153+00 SHEET F11



| | | | | | | | | | | | | | | | | | | |
|---------------------|--|--|--|-----------------|----------|-------|----------------|-----|----------|------------|------------------|----------|------------|-----|------------|-------------|------|-----|
| FIELD BOOKS | | | | BM NO. | LOCATION | ELEV. | DATA | | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
| DESIGN: | | | | | | | BASE | TBC | | | TELEPHONE | --- | | | | | | |
| STAKING: | | | | | | | TOPOGRAPHY | TBC | | | ELECTRIC | --- | | | | | | |
| | | | | | | | PROFILE | TBC | | | CABLE TV | --- | | | | | | |
| ASBUILT: | | | | | | | SANITARY SEWER | --- | | | TRAFFIC SIGNAL | --- | | | | | | |
| CONTRACTOR: | | | | BASIS OF DATUM: | | | STORM SEWER | --- | | | DESIGN | TBC | | | | | | |
| INSPECTOR: | | | | | | | WATER | --- | | | QUANTITIES | TBC | | | | | | |
| | | | | | | | GAS | --- | | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | PLAN CHECK | | | | REVISIONS | | | | CONSULTANT | | SEAL | |




The Boutet Co.


65%
SUBMITTAL

Department of
Parks and
Recreation

Healthy
Parks



Healthy
People



MUNICIPALITY OF ANCHORAGE

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CHESTER CREEK TRAIL

"CC" 147+00 TO 153+00

SCALE

HOR. PER SHEET

VER. PER SHEET

DATE: 12/15/2014

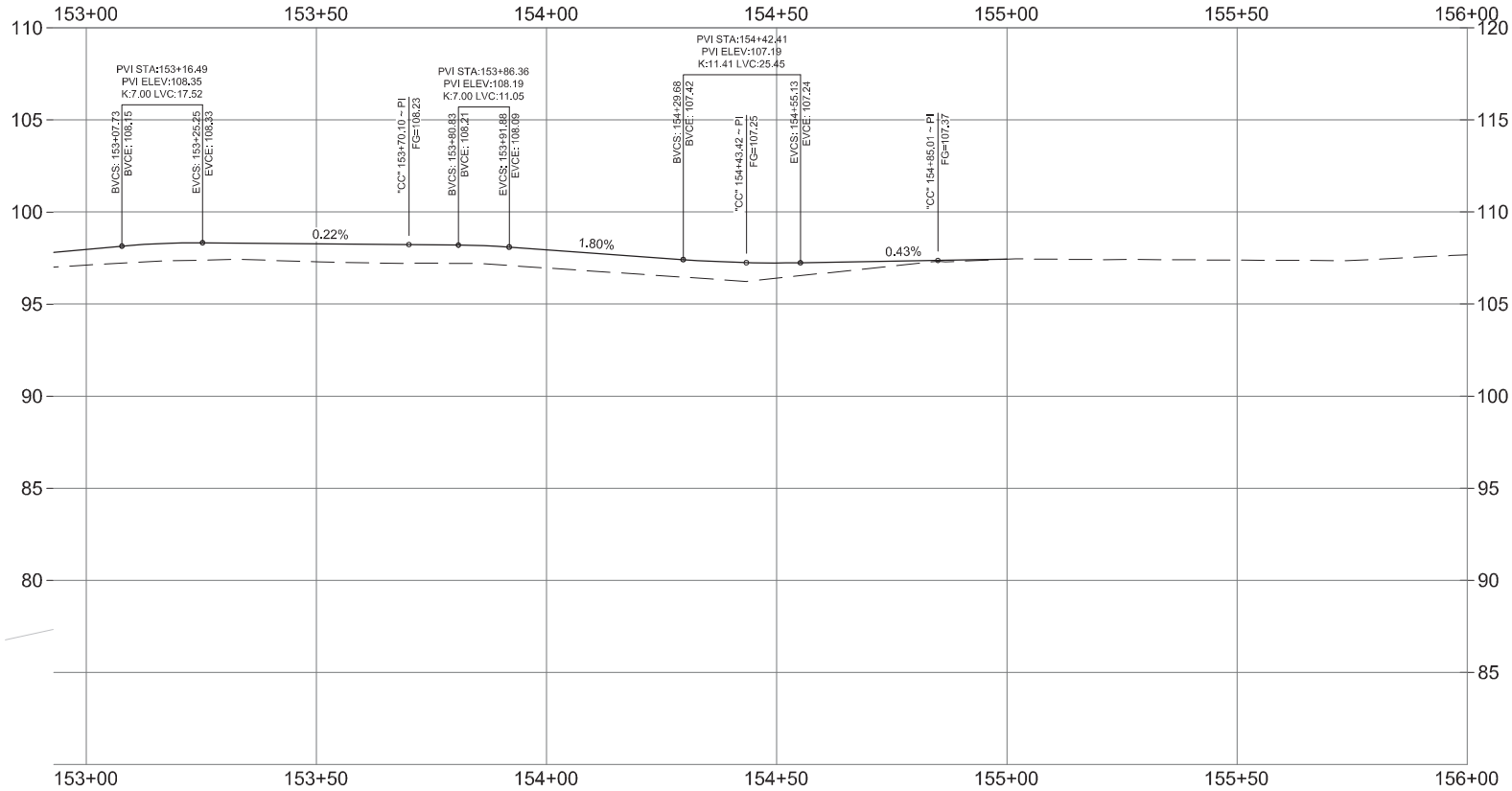
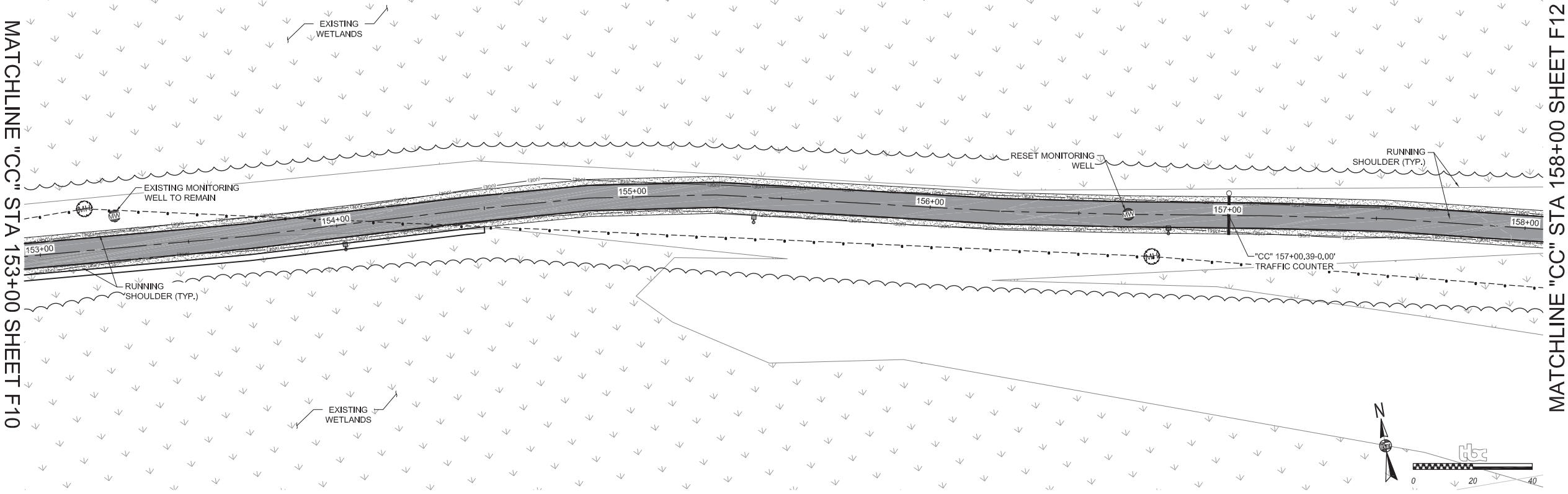
GRID: N/A

STATUS: 65% PS&E

SHEET

F10 of F26

CHESTER CREEK TRAIL "CC"

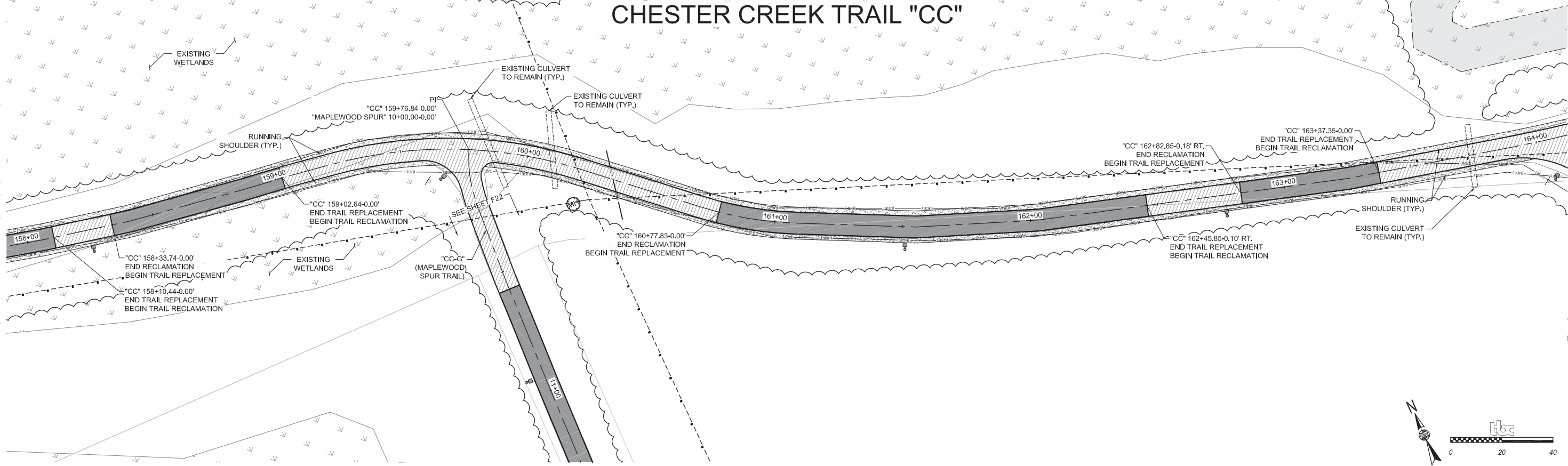


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|--|--------|----------------|-------|----------------|----------|------------|------------------|-----------|------------|-------------|--------|-------------|-------|------|----------|------------|------|----------------------------------|------------|--------------------------------------|------|-------------|----|------------------|---------|--|--|--|------|-----|--|-----------|-----|--|--|--|--|--|--|----------|--|--|--|------------|-----|--|----------|-----|--|--|--|--|--|--|----------|--|--|--|---------|-----|--|----------|-----|--|--|--|--|--|--|-------------|--|--|--|----------------|-----|--|----------------|-----|--|--|--|--|--|--|------------|--|--|--|-------------|-----|--|--------|-----|--|--|--|--|--|--|--|--|--|--|-------|-----|--|------------|-----|--|--|--|--|--|--|--|--|--|--|-----|-----|--|------------------|--|--|--|--|--|--|--|--|--|------------------|--|--|--|--|--|--|--|--|--|--|--|
| <table><tr><td>FIELD BOOKS</td><td>BM NO.</td><td>LOCATION</td><td>ELEV.</td><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td><td>REV</td><td>DATE</td><td>DESCRIPTION</td><td>BY</td><td>REV</td></tr><tr><td>DESIGN:</td><td></td><td></td><td></td><td>BASE</td><td>TBC</td><td></td><td>TELEPHONE</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>STAKING:</td><td></td><td></td><td></td><td>TOPOGRAPHY</td><td>TBC</td><td></td><td>ELECTRIC</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>ASBUILT:</td><td></td><td></td><td></td><td>PROFILE</td><td>TBC</td><td></td><td>CABLE TV</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>CONTRACTOR:</td><td></td><td></td><td></td><td>SANITARY SEWER</td><td>---</td><td></td><td>TRAFFIC SIGNAL</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>INSPECTOR:</td><td></td><td></td><td></td><td>STORM SEWER</td><td>---</td><td></td><td>DESIGN</td><td>TBC</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td>WATER</td><td>---</td><td></td><td>QUANTITIES</td><td>TBC</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td>GAS</td><td>---</td><td></td><td>MUN. FINAL CHECK</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr></table> | | | | | | | | | | FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | DESIGN: | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | STAKING: | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | ASBUILT: | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | CONTRACTOR: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | INSPECTOR: | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | | | | 65% SUBMITTAL | | Department of Parks and Recreation Municipality of Anchorage | | | | | | PARKS AND RECREATION DEPARTMENT 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "CC" 153+00 TO 158+00 | | | |
| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| DESIGN: | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STAKING: | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| ASBUILT: | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONTRACTOR: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| INSPECTOR: | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | VERTICAL DATUM | | PLAN CHECK | | | | REVISIONS | | | | CONSULTANT | | SEAL | | SCALE | | HOR. PER SHEET VER. PER SHEET | | DATE: 12/15/2014 STATUS: 65% PS&E | | GRID: N/A | | SHEET F11 of F26 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

CHESTER CREEK TRAIL "CC"

MATCHLINE "CC" STA 158+00 SHEET F11

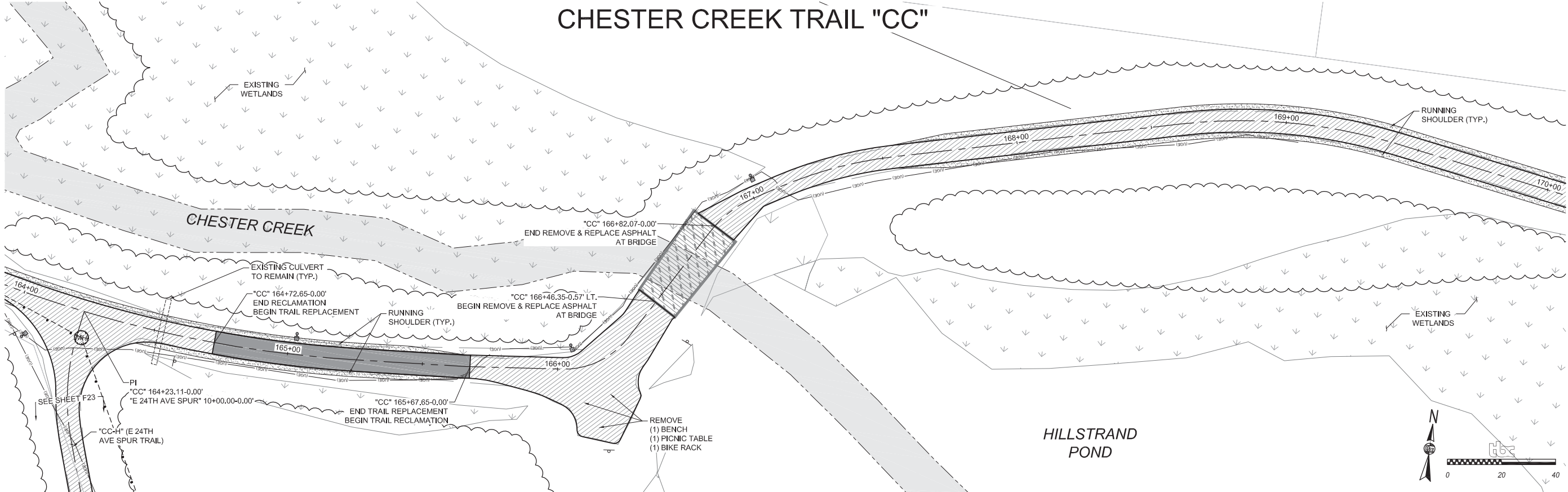
MATCHLINE STA "CC" 164+00 SHEET F12



CHESTER CREEK TRAIL "CC"

MATCHLINE "CC" STA 164+00 SHEET F12

MATCHLINE STA "CC" 170+00 SHEET F13



| FIELD BOOKS | | | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
|---------------------|--|--|--|--------|----------|-------|----------------|----------|------------|------------------|-----------|------------|-----|------|-------------|----|-----|
| DESIGN: | | | | | | | BASE | TBC | | TELEPHONE | --- | | | | | | |
| STAKING: | | | | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | |
| ASBUILT: | | | | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | |
| CONTRACTOR: | | | | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | |
| INSPECTOR: | | | | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | |
| CONSTRUCTION RECORD | | | | | | | WATER | --- | | QUANTITIES | TBC | | | | | | |
| | | | | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | |
| | | | | | | | PLAN CHECK | | | | REVISIONS | | | | | | |
| | | | | | | | VERTICAL DATUM | | | | | | | | | | |



The Boutet Co.

65%
SUBMITTAL

Department of
Parks and
Recreation

Municipality of
Anchorage





PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CHESTER CREEK TRAIL

"CC" 158+00 TO 170+00

SCALE

HOR. PER SHEET
VER. PER SHEET

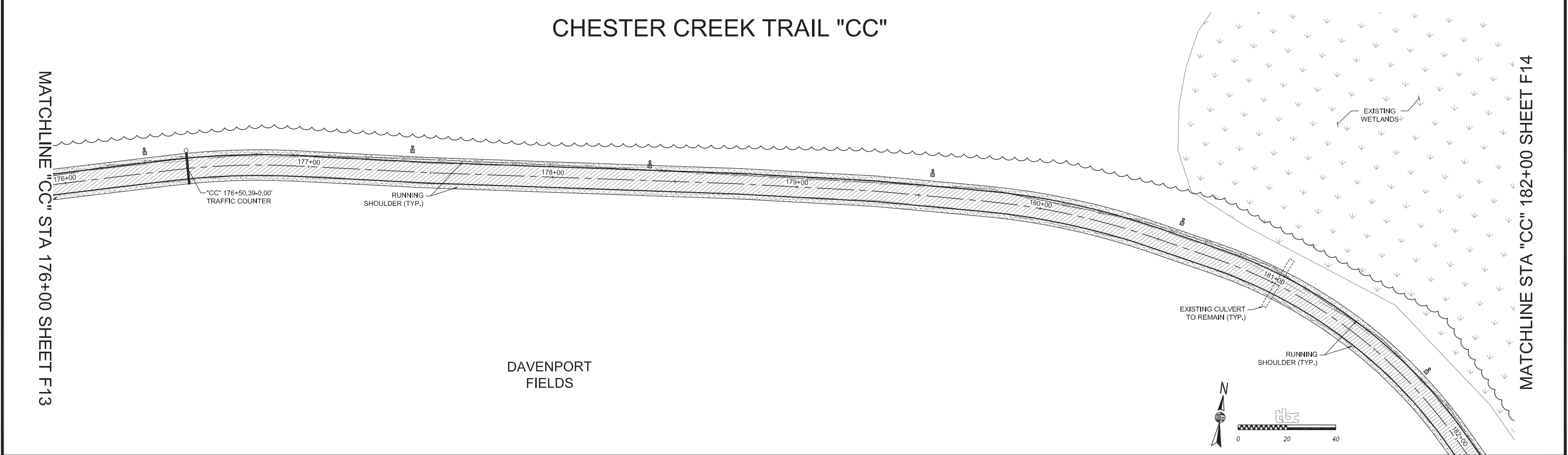
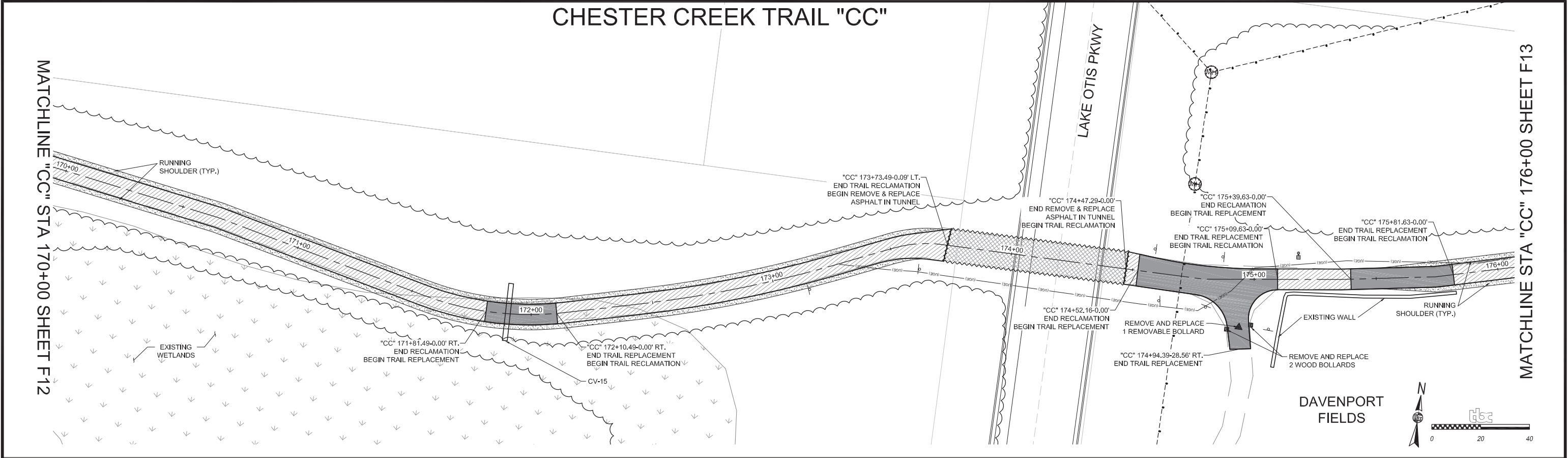
DATE: 12/15/2014




GRID: N/A

STATUS: 65% PS&E

SHEET

F12 of F25



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|---------------------|-----------------|----------|-------|----------------|----------|------------|------------------|------------|------------|-----|------|-------------|----|---|------------------|--|---|---|---|--|--|--|
| | | | | | | | | | | | | | |  The Boutet Co. | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage |  |  | PARKS AND RECREATION DEPARTMENT | | | |
| | | | | | | | | | | | | | | | | | | | 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "CC" 170+00 TO 182+00 | | | |
| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | | | | | | | |
| DESIGN: | | | | BASE | TBC | | TELEPHONE | -- | | | | | | | | | | | | | | |
| | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | | | | | | | | | |
| STAKING: | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | | | | | | | | | |
| | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | | | | | | | | | |
| ASBUILT: | | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | | | | | | | | | |
| CONTRACTOR: | BASIS OF DATUM: | | | WATER | -- | | QUANTITIES | TBC | | | | | | | | | | | | | | |
| INSPECTOR: | | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | | PLAN CHECK | | | | REVISIONS | | | | CONSULTANT | | SEAL | | | | |
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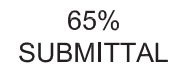
MATCHLINE "CC" STA 182+00 SHEET F-13



MATCHLINE "CC" STA 188+00 SHEET F-14

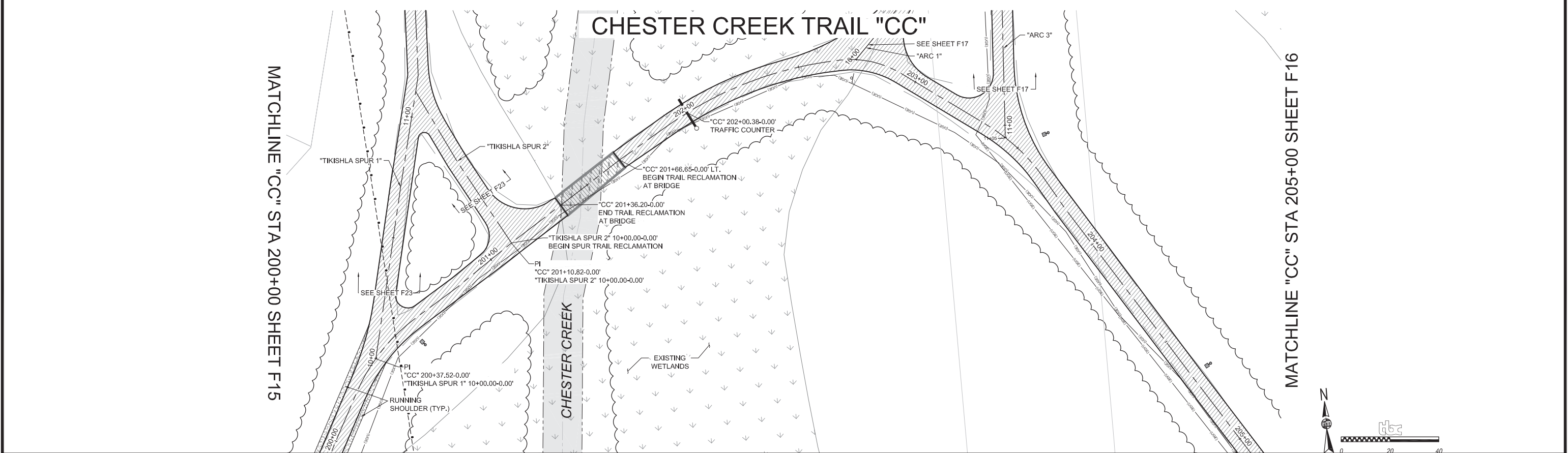
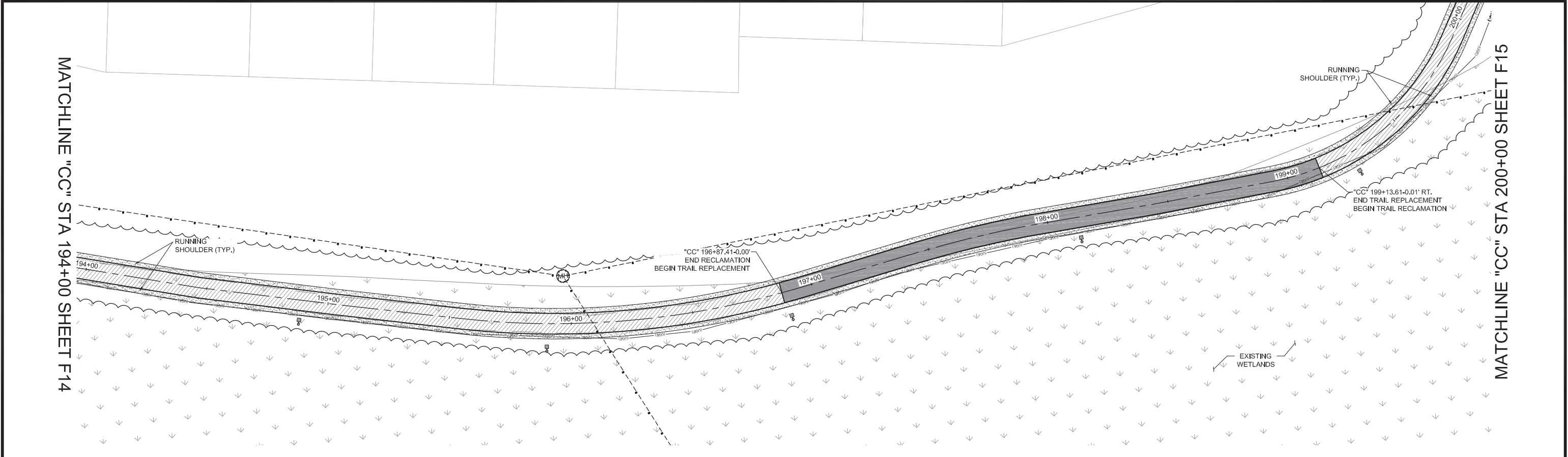


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|---------------------|-----------------|----------|-------|----------------|----------|------------|------------------|------------|------------|-----|------|-------------|----|----|
| | | | | | | | | | | | | | | |
| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | RE |
| DESIGN: | | | | BASE | TBC | | TELEPHONE | -- | | | | | | |
| | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | |
| STAKING: | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | |
| | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | |
| ASBUILT: | | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | |
| CONTRACTOR: | BASIS OF DATUM: | | | WATER | -- | | QUANTITIES | TBC | | | | | | |
| INSPECTOR: | | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | | PLAN CHECK | | | | REVISIONS | | |

Health
Parks

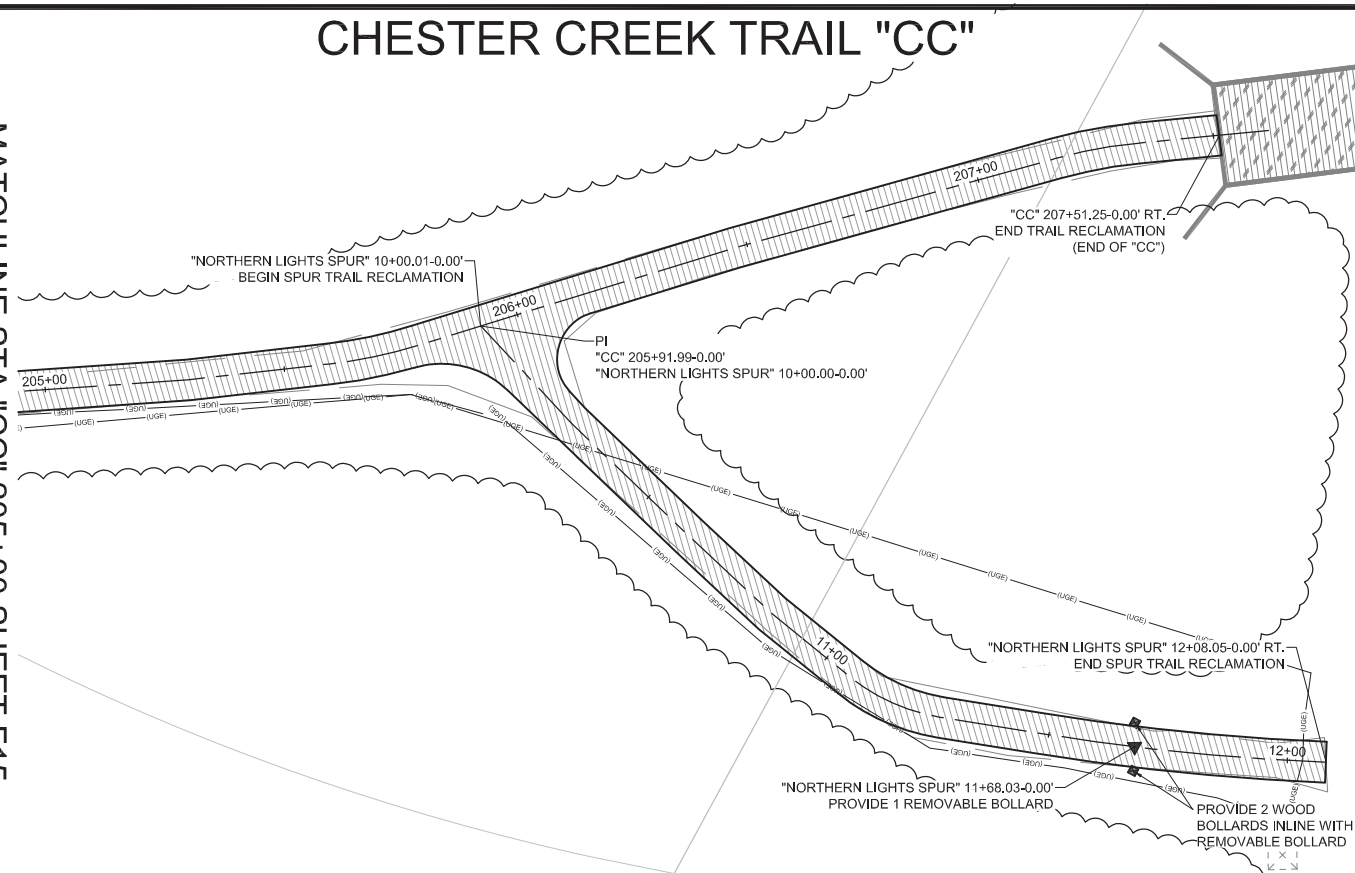
2015 CHESTER CREEK TRAIL IMPROVEMENTS
CHESTER CREEK TRAIL
"CC" 182+00 TO 194+00

| | | | | | |
|-------|----------------|------------------|-----------|-------|---------------|
| SCALE | HOR. PER SHEET | DATE: 12/15/2014 | GRID: N/A | SHEET | F14 of F25 |
| | VER. PER SHEET | STATUS: 65% PS&E | | | |

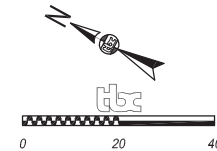






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|---|-----------------|----------------------------------|-------|--------------------------------------|----------|------------|----------------|------------------|------------|-------------|--------|-------------|-------|------|----------|------------|------|----------|------------|-----|------|-------------|----|-----|---------|--|--|--|------|-----|--|-----------|-----|--|--|--|--|--|--|----------|--|--|--|------------|-----|--|----------|-----|--|--|--|--|--|--|--|--|--|--|---------|-----|--|----------|-----|--|--|--|--|--|--|----------|--|--|--|----------------|-----|--|----------------|-----|--|--|--|--|--|--|-------------|-----------------|--|--|-------------|-----|--|--------|-----|--|--|--|--|--|--|------------|--|--|--|-------|-----|--|------------|-----|--|--|--|--|--|--|---------------------|--|--|--|----------------|--|--|--|------------|--|--|--|-----------|--|--|--|--------------------|------------------|--|--|--|---------------------------------|--|--|--|
| <table border="1"><tr><td>FIELD BOOKS</td><td>BM NO.</td><td>LOCATION</td><td>ELEV.</td><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td><td>REV</td><td>DATE</td><td>DESCRIPTION</td><td>BY</td><td>REV</td></tr><tr><td>DESIGN:</td><td></td><td></td><td></td><td>BASE</td><td>TBC</td><td></td><td>TELEPHONE</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>STAKING:</td><td></td><td></td><td></td><td>TOPOGRAPHY</td><td>TBC</td><td></td><td>ELECTRIC</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td>PROFILE</td><td>TBC</td><td></td><td>CABLE TV</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>ASBUILT:</td><td></td><td></td><td></td><td>SANITARY SEWER</td><td>---</td><td></td><td>TRAFFIC SIGNAL</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>CONTRACTOR:</td><td colspan="3">BASIS OF DATUM:</td><td>STORM SEWER</td><td>---</td><td></td><td>DESIGN</td><td>TBC</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>INSPECTOR:</td><td colspan="3"></td><td>WATER</td><td>---</td><td></td><td>QUANTITIES</td><td>TBC</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td colspan="4">CONSTRUCTION RECORD</td><td colspan="4">VERTICAL DATUM</td><td colspan="4">PLAN CHECK</td><td colspan="4">REVISIONS</td></tr></table> | | | | | | | | | | FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | DESIGN: | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | STAKING: | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | ASBUILT: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | CONTRACTOR: | BASIS OF DATUM: | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | INSPECTOR: | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | | PLAN CHECK | | | | REVISIONS | | | | The Boutet Co. | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage | | | PARKS AND RECREATION DEPARTMENT | | | |
| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| DESIGN: | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STAKING: | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| ASBUILT: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONTRACTOR: | BASIS OF DATUM: | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| INSPECTOR: | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | | PLAN CHECK | | | | REVISIONS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "CC" 194+00 TO 205+00 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SCALE | | HOR. PER SHEET VER. PER SHEET | | DATE: 12/15/2014 STATUS: 65% PS&E | | GRID: N/A | | SHEET F15 of F25 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

MATCHLINE STA "CC" 205+00 SHEET F15



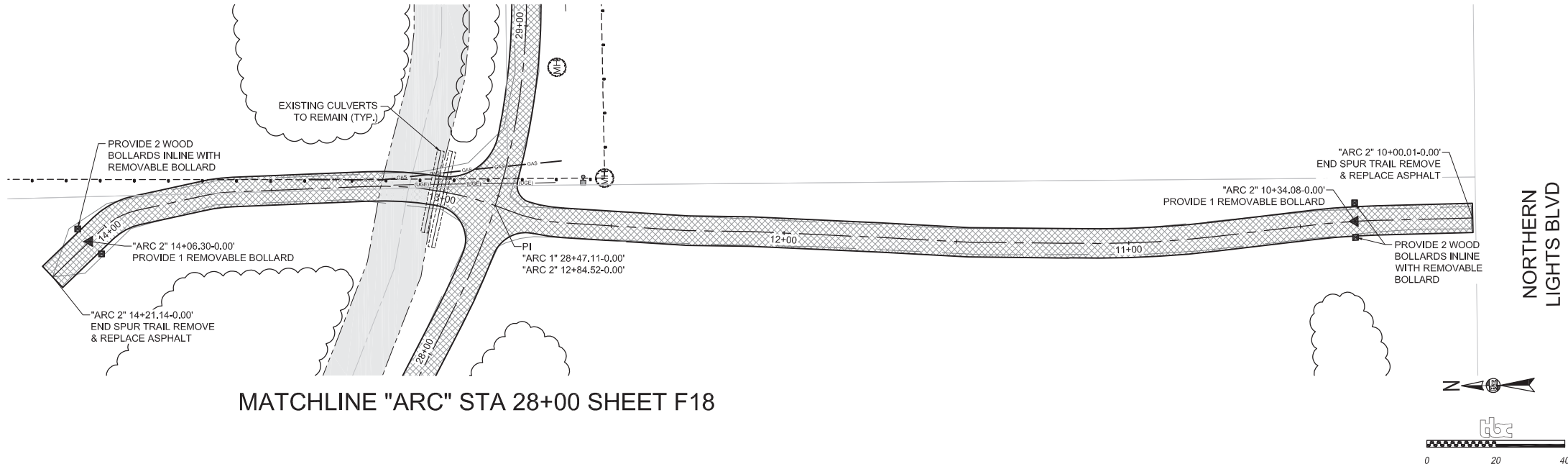
MATCHLINE STA 207+62 END OF "CC" (BASE BID)






| | | | | | | | | | | | | | | | | | | | | | | | | |
|---------------------|--|----------------------------------|--|--------------------------------------|----------|-----------|----------------|----------|------------|------------------|-----------|------------|-----|---|------------------|--|---|--|--|---------------------------------|-------------|----|-----|---|
| | | | | | | | | | | | | | |  The Boutet Co. | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage |  Healthy Parks |  Healthy People |  MUNICIPALITY OF ANCHORAGE | PARKS AND RECREATION DEPARTMENT | | | | |
| FIELD BOOKS | | | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | | | | | | | DATE | DESCRIPTION | BY | REV | 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "CC" 205+00 TO 207+62 (END OF "CC") |
| DESIGN: | | | | | | | BASE | TBC | --- | TELEPHONE | --- | --- | | | | | | | | | | | | |
| | | | | | | | TOPOGRAPHY | TBC | --- | ELECTRIC | --- | --- | | | | | | | | | | | | |
| STAKING: | | | | | | | PROFILE | TBC | --- | CABLE TV | --- | --- | | | | | | | | | | | | |
| | | | | | | | SANITARY SEWER | --- | --- | TRAFFIC SIGNAL | --- | --- | | | | | | | | | | | | |
| ASBUILT: | | | | | | | STORM SEWER | --- | --- | DESIGN | TBC | --- | | | | | | | | | | | | |
| CONTRACTOR: | | | | BASIS OF DATUM: | | | WATER | --- | --- | QUANTITIES | TBC | --- | | | | | | | | | | | | |
| INSPECTOR: | | | | | | | GAS | --- | --- | MUN. FINAL CHECK | --- | --- | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | PLAN CHECK | | | | REVISIONS | | | | CONSULTANT | | SEAL | | | | | | | |
| SCALE | | HOR. PER SHEET VER. PER SHEET | | DATE: 12/15/2014 STATUS: 65% PS&E | | GRID: N/A | | SHEET | | F16 of F25 | | | | | | | | | | | | | | |

BASE BID "ARC2"

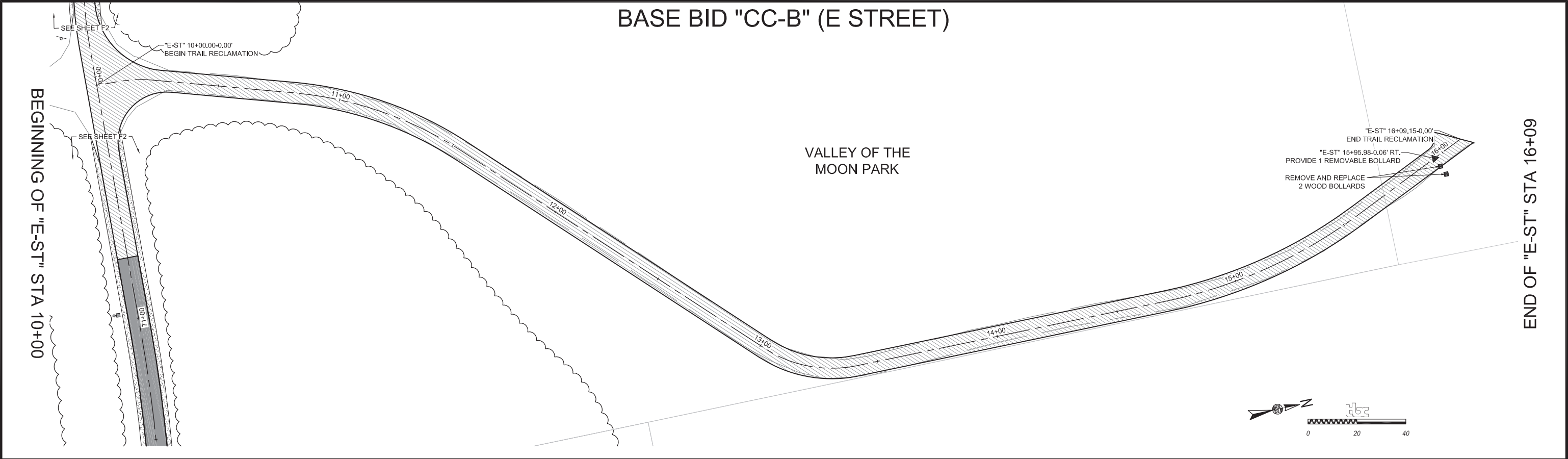
MATCHLINE "ARC" STA 29+00 SHEET F18



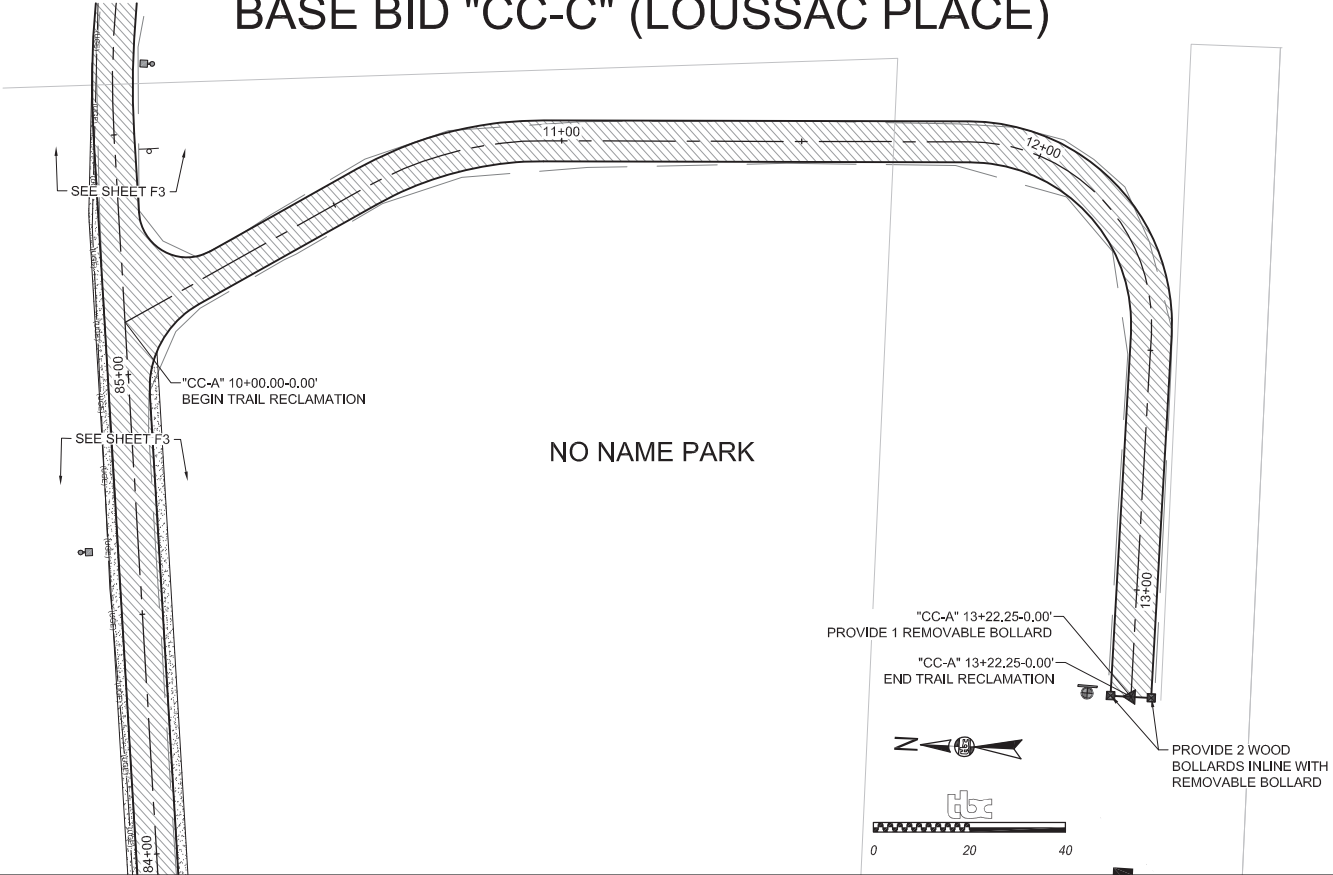
MATCHLINE "ARC" STA 28+00 SHEET F18

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|---------------------|--|-----------------|----------|----------------|----------------|------------------|------------|------------------|------------|------------|--------|------------|-------------|---|-----|------------------|--|--|--|--|--|--|--|--|--|--|--|
| | | | | | | | | | | | | | |  The Boutet Co. | | 65% SUBMITTAL | | Department of Parks and Recreation Municipality of Anchorage | |  Healthy Parks Healthy People | |  MUNICIPALITY OF ANCHORAGE | | PARKS AND RECREATION DEPARTMENT 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "ARC2" 10+00 TO 14+21 | | | |
| FIELD BOOKS | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | | | | | | | | | | | |
| DESIGN: | | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | | | | | | | | | | | | |
| | | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | | | | | | | | |
| STAKING: | | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | | | | | | | | | | | | |
| | | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | | | | | | | | | | | | |
| ASBUILT: | | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | | | | | | | | | | | | |
| CONTRACTOR: | | BASIS OF DATUM: | | | WATER | --- | | QUANTITIES | TBC | | | | | | | | | | | | | | | | | | |
| INSPECTOR: | | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | VERTICAL DATUM | | | PLAN CHECK | | RELATIONS | | CONSULTANT | | SEALED | | | | | | | | | | | | | | | | |
| SCALE | | HOR. PER SHEET | | VER. PER SHEET | | DATE: 12/15/2014 | | GRID: N/A | | SHEET | | F19 of F26 | | | | | | | | | | | | | | | |
| | | | | | | STATUS: 65% PS&E | | | | | | | | | | | | | | | | | | | | | |

BASE BID "CC-B" (E STREET)



BASE BID "CC-C" (LOUSSAC PLACE)



| | | | | | | | | | | | | | | |
|---------------------|-----------------|----------|-------|----------------|----------|------------|------------------|----------|------------|-----------|------|-------------|----|-----|
| | | | | | | | | | | | | | | |
| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
| DESIGN: | | | | BASE | TBC | | TELEPHONE | -- | | | | | | |
| | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | |
| STAKING: | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | |
| | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | |
| ASBUILT: | | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | |
| CONTRACTOR: | BASIS OF DATUM: | | | WATER | -- | | QUANTITIES | TBC | | | | | | |
| INSPECTOR: | | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | VERTICAL DATUM | | | PLAN CHECK | | | | | | REVISIONS | | | | |



The Boutet Co.

65%
SUBMITTAL

Department of
Parks and
Recreation

Municipality of
Anchorage





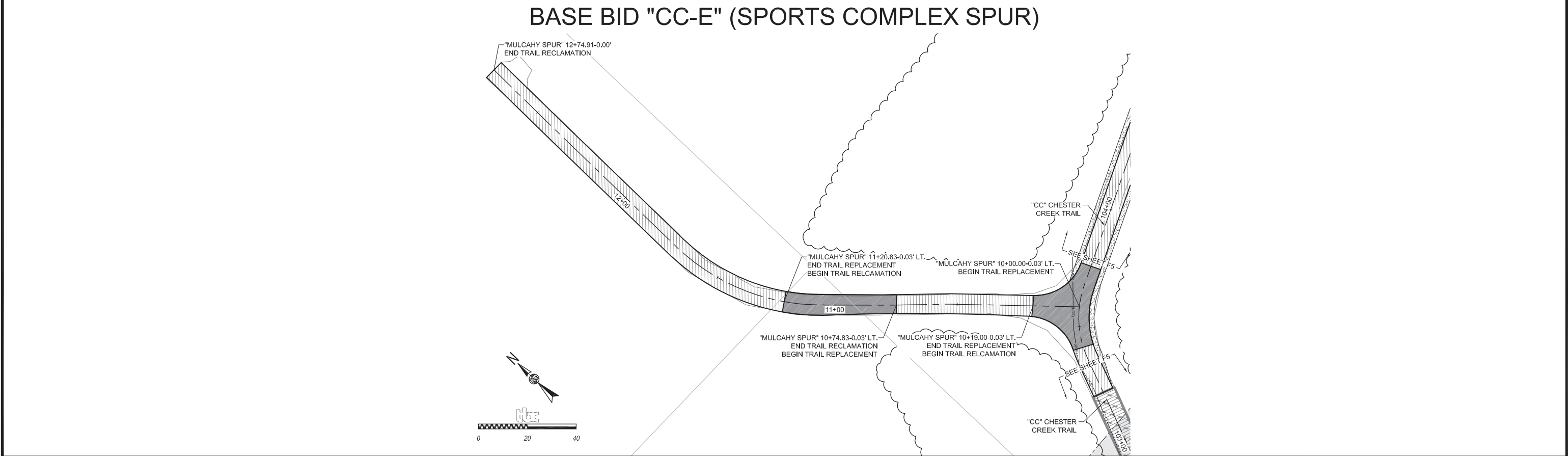
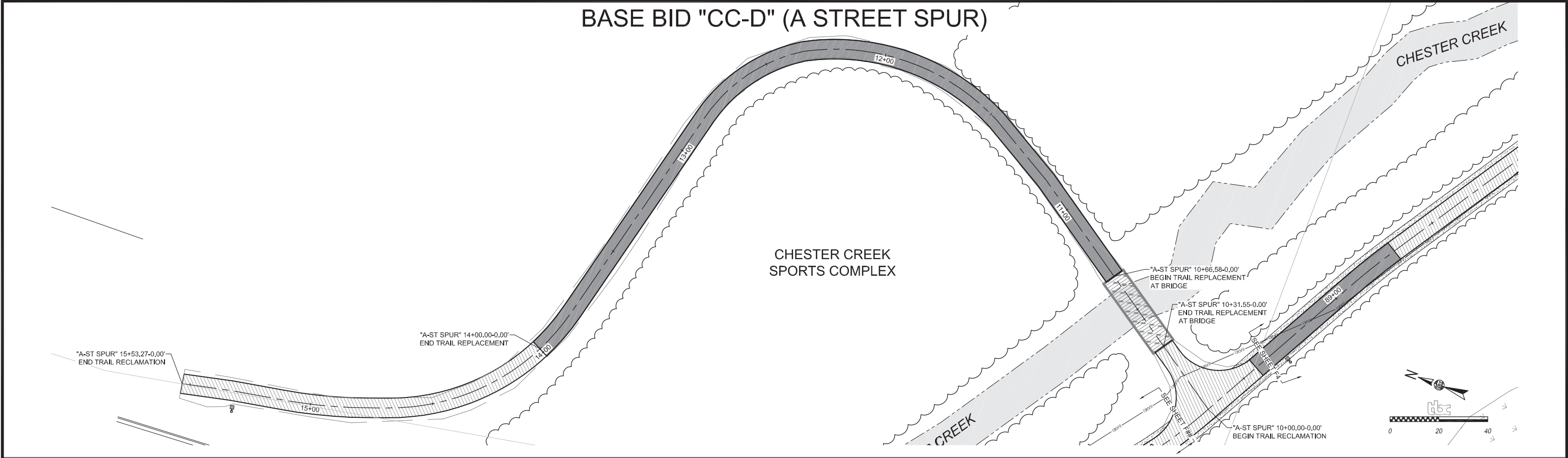
PARKS AND RECREATION DEPARTMENT




2015 CHESTER CREEK TRAIL IMPROVEMENTS

CHESTER CREEK TRAIL

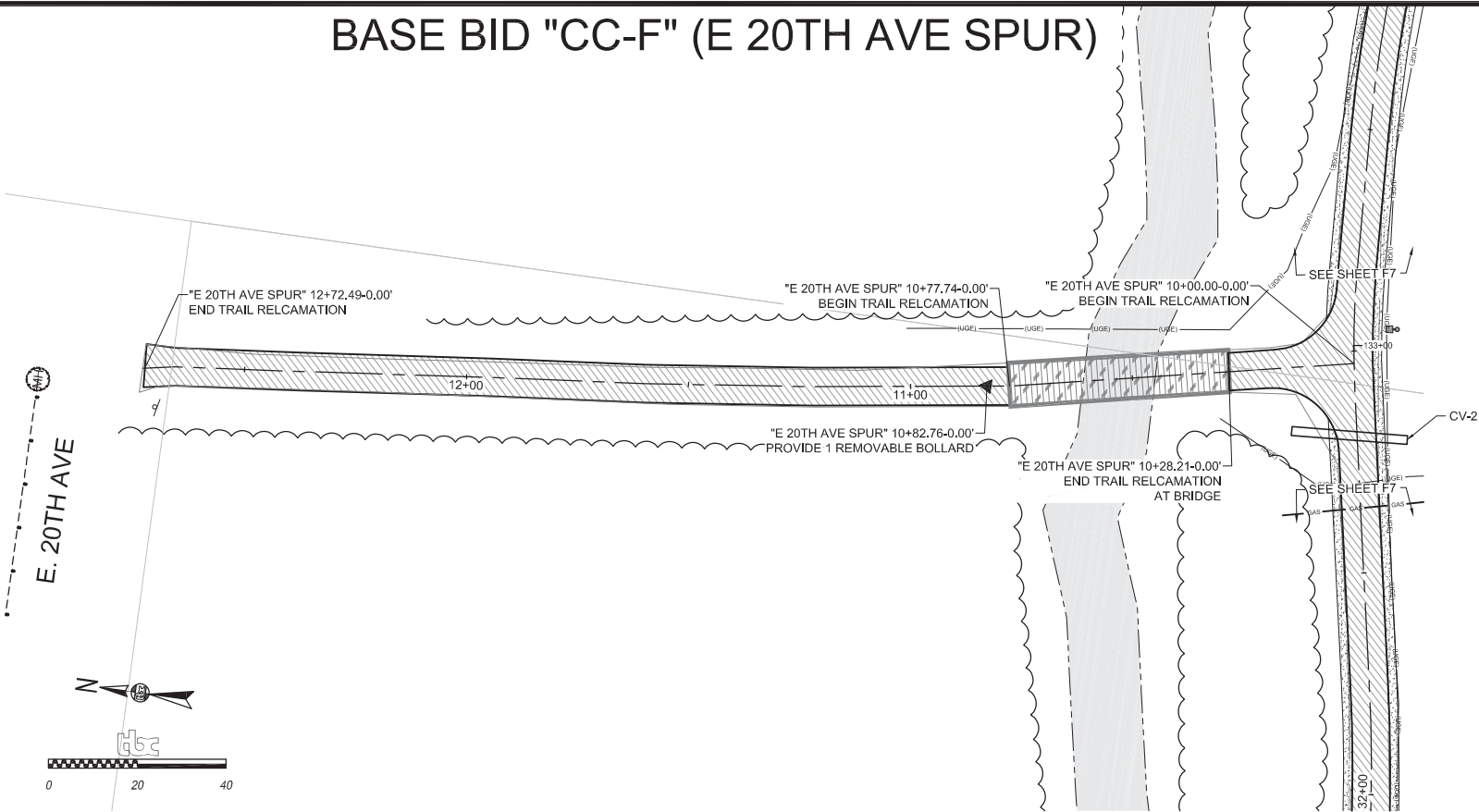
"CC-B" & "CC-C"

| | | | | | |
|-------|----------------|------------------|-----------|-------|-----|
| SCALE | HOR. PER SHEET | DATE: 12/15/2014 | GRID: N/A | SHEET | F20 |
| | VER. PER SHEET | STATUS: 65% PS&E | | of | F25 |

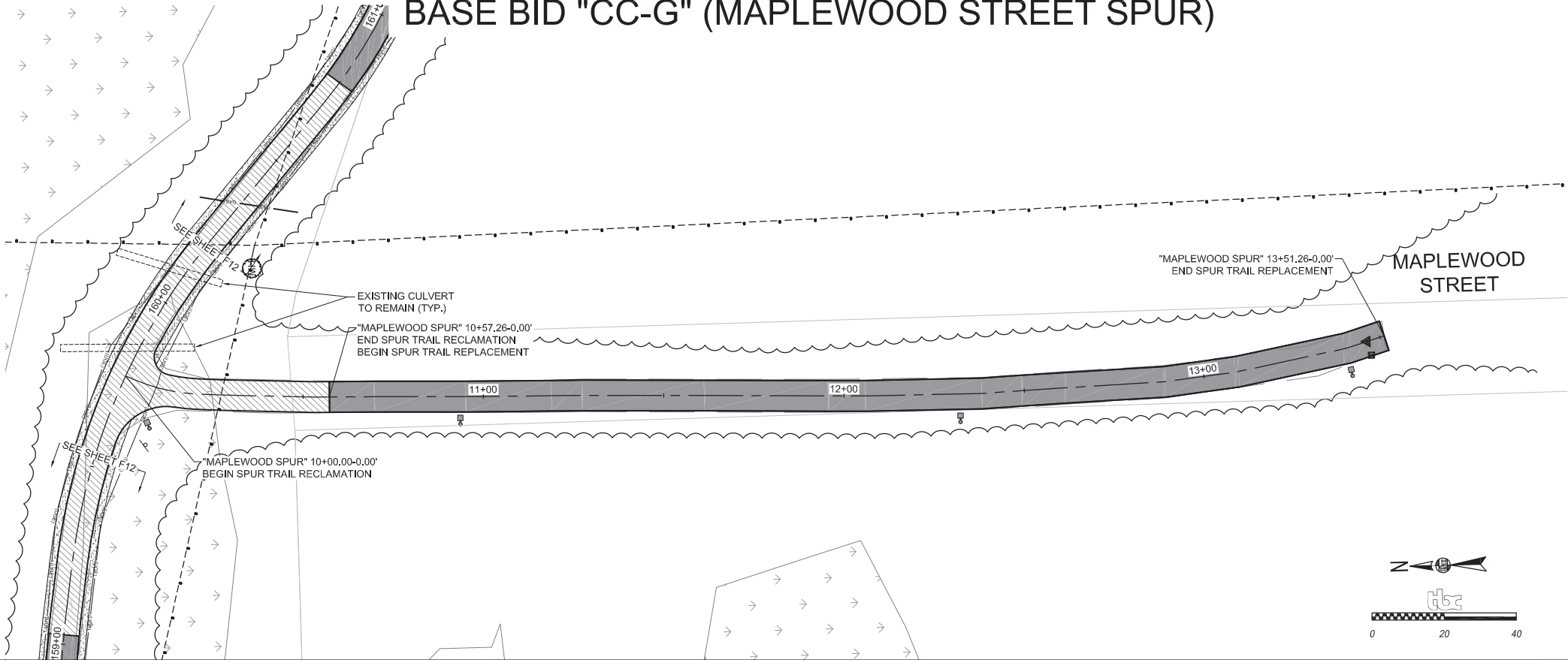





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|---------------------|-----------------|----------|-------|----------------|----------|------------|------------------|------------|------------|-----|------|-------------|----|---|------------------|--|---|---|---|-------|----------------------------------|--------------------------------------|----------|-------|---------------|
| | | | | | | | | | | | | | |  The Boutet Co. | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage |  |  | PARKS AND RECREATION DEPARTMENT | | | | | | |
| | | | | | | | | | | | | | | | | | | | 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "CC-D" & "CC-E" | | | | | | |
| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | | | | | | REV | SCALE | HOR. PER SHEET VER. PER SHEET | DATE: 12/15/2014 STATUS: 65% PS&E | GRID: NA | SHEET | F21 of F25 |
| DESIGN: | | | | BASE | TBC | | TELEPHONE | -- | | | | | | | | | | | | | | | | | |
| | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | | | | | | | | | | | | |
| STAKING: | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | | | | | | | | | | | | |
| | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | | | | | | | | | | | | |
| ASBUILT: | | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | | | | | | | | | | | | |
| CONTRACTOR: | BASIS OF DATUM: | | | WATER | -- | | QUANTITIES | TBC | | | | | | | | | | | | | | | | | |
| INSPECTOR: | | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | | PLAN CHECK | | | | REVISIONS | | | | CONSULTANT | | SEAL | | | | | | | |

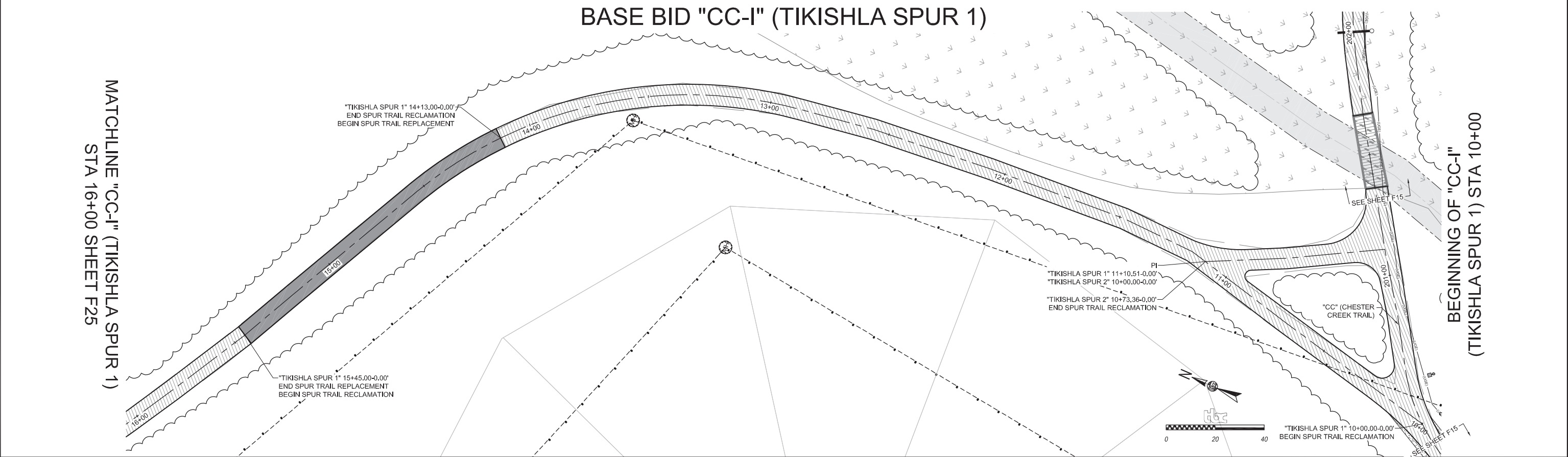
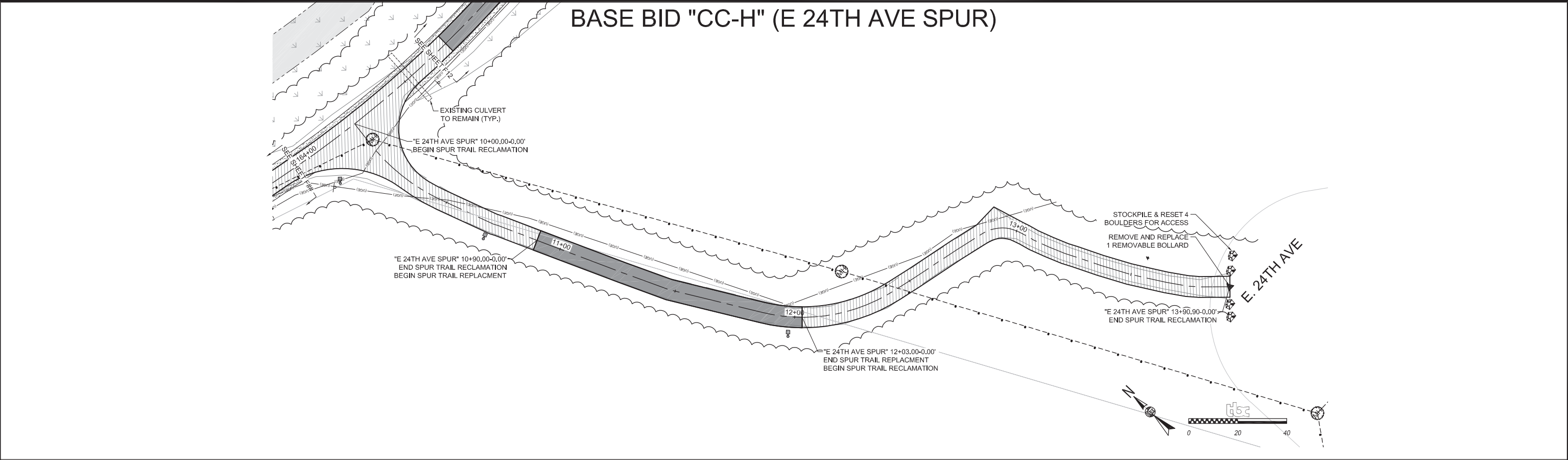
BASE BID "CC-F" (E 20TH AVE SPUR)






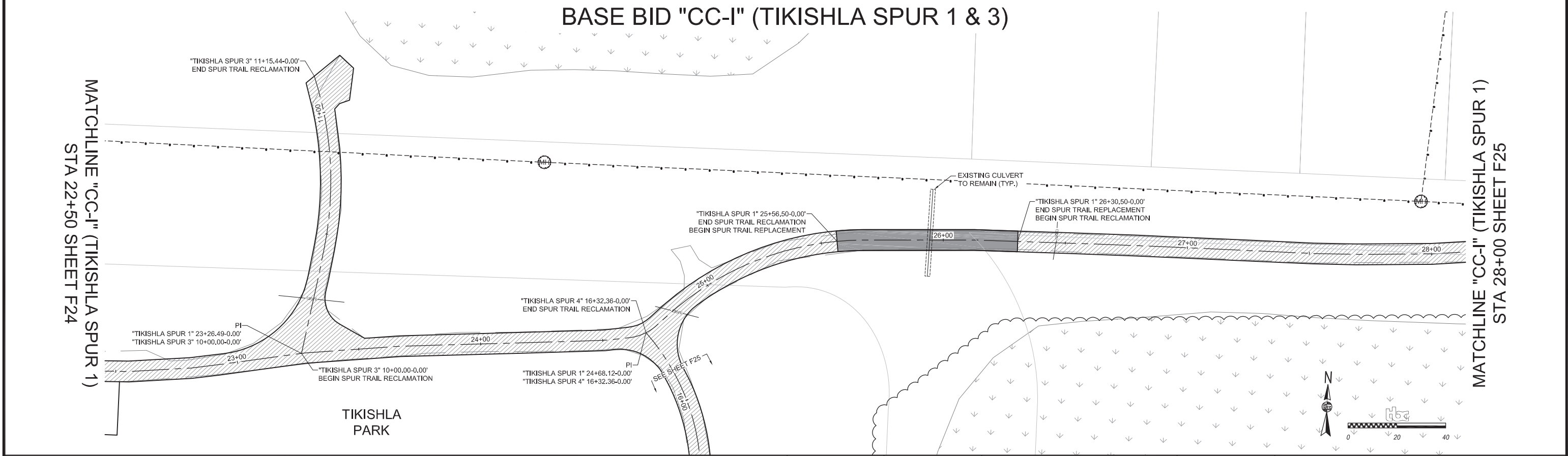
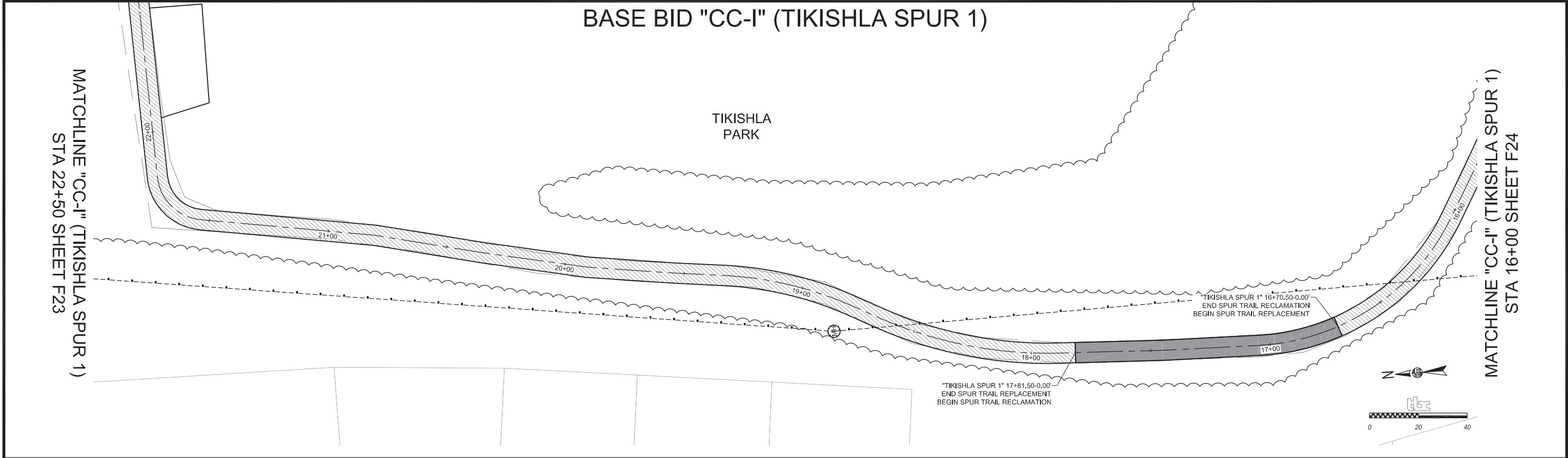
BASE BID "CC-G" (MAPLEWOOD STREET SPUR)



| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|-----------------|----------------------------------|-------|--------------------------------------|----------|------------|----------------|------------------|------------|-------------|--------|-------------|-------|------|----------|------------|------|----------|------------|-----|------|-------------|----|-----|--------|--|--|--|------|-----|--|-----------|-----|--|--|--|--|--|--|---------|--|--|--|------------|-----|--|----------|-----|--|--|--|--|--|--|--|--|--|--|---------|-----|--|----------|-----|--|--|--|--|--|--|---------|--|--|--|----------------|-----|--|----------------|-----|--|--|--|--|--|--|------------|-----------------|--|--|-------------|-----|--|--------|-----|--|--|--|--|--|--|-----------|--|--|--|-------|-----|--|------------|-----|--|--|--|--|--|--|---------------------|--|--|--|----------------|--|--|--|------------|--|--|--|-----------|--|--|--|---|------------------|--|---|---|---------------------------------|--|
| <table border="1"><tr><td>FIELD BOOKS</td><td>BM NO.</td><td>LOCATION</td><td>ELEV.</td><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td><td>REV</td><td>DATE</td><td>DESCRIPTION</td><td>BY</td><td>REV</td></tr><tr><td>DESIGN</td><td></td><td></td><td></td><td>BASE</td><td>TBC</td><td></td><td>TELEPHONE</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>STAKING</td><td></td><td></td><td></td><td>TOPOGRAPHY</td><td>TBC</td><td></td><td>ELECTRIC</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td>PROFILE</td><td>TBC</td><td></td><td>CABLE TV</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>ASBUILT</td><td></td><td></td><td></td><td>SANITARY SEWER</td><td>---</td><td></td><td>TRAFFIC SIGNAL</td><td>---</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>CONTRACTOR</td><td colspan="3">BASIS OF DATUM:</td><td>STORM SEWER</td><td>---</td><td></td><td>DESIGN</td><td>TBC</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>INSPECTOR</td><td colspan="3"></td><td>WATER</td><td>---</td><td></td><td>QUANTITIES</td><td>TBC</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td colspan="4">CONSTRUCTION RECORD</td><td colspan="4">VERTICAL DATUM</td><td colspan="4">PLAN CHECK</td><td colspan="4">REVISIONS</td></tr></table> | | | | | | | | | | FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | DESIGN | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | STAKING | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | ASBUILT | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | CONTRACTOR | BASIS OF DATUM: | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | INSPECTOR | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | | PLAN CHECK | | | | REVISIONS | | | |  The Boutet Co. | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage |  |  | PARKS AND RECREATION DEPARTMENT | |
| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| DESIGN | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STAKING | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| ASBUILT | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONTRACTOR | BASIS OF DATUM: | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| INSPECTOR | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | | | VERTICAL DATUM | | | | PLAN CHECK | | | | REVISIONS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "CC-F" & "CC-G" | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SCALE | | HOR. PER SHEET VER. PER SHEET | | DATE: 12/15/2014 STATUS: 65% PS&E | | GRID: N/A | | SHEET F22 of F25 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |




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|---------------------|-----------------|----------------|----------|----------------|------------|----------|------------|------------------|----------|------------|-----------|------|-------------|----|---|------------------|--|--|--|---|-------|----------------------------------|--------------------------------------|-----------|-------|---------------|
| | | | | | | | | | | | | | | |  The Boutet Co. | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage |  Healthy Parks Healthy People |  MUNICIPALITY OF ANCHORAGE | PARKS AND RECREATION DEPARTMENT | | | | | | |
| | | | | | | | | | | | | | | | | | | | | 2015 CHESTER CREEK TRAIL IMPROVEMENTS CHESTER CREEK TRAIL "CC-H" & "CC-I" | | | | | | |
| FIELD BOOKS | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | | | | | | REV | SCALE | HOR. PER SHEET VER. PER SHEET | DATE: 12/15/2014 STATUS: 65% PS&E | GRID: N/A | SHEET | F23 of F25 |
| DESIGN: | | | | BASE | TBC | | | TELEPHONE | --- | | | | | | | | | | | | | | | | | |
| | | | | TOPOGRAPHY | TBC | | | ELECTRIC | --- | | | | | | | | | | | | | | | | | |
| STAKING: | | | | PROFILE | TBC | | | CABLE TV | --- | | | | | | | | | | | | | | | | | |
| | | | | SANITARY SEWER | --- | | | TRAFFIC SIGNAL | --- | | | | | | | | | | | | | | | | | |
| ASBUILT: | | | | STORM SEWER | --- | | | DESIGN | TBC | | | | | | | | | | | | | | | | | |
| CONTRACTOR: | BASIS OF DATUM: | | | | WATER | --- | | QUANTITIES | TBC | | | | | | | | | | | | | | | | | |
| INSPECTOR: | | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | VERTICAL DATUM | | | PLAN CHECK | | | | | | REVISIONS | | | | | | CONSULTANT | | SEAL | | | | | | | |



| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
|-------------|--------|----------|-------|----------------|----------|------------|------------------|----------|------------|-----|------|-------------|----|-----|
| DESIGN: | | | | BASE | TBC | | TELEPHONE | --- | | | | | | |
| STAKING: | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | |
| | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | |
| ASBUILT: | | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | |
| CONTRACTOR: | | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | |
| INSPECTOR: | | | | WATER | --- | | QUANTITIES | TBC | | | | | | |
| | | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | |

| CONSTRUCTION RECORD | | | | PLAN CHECK | | | | REVISIONS | | | | CONSULTANT | | SEAL | |
|---------------------|--|--|--|------------|--|--|--|-----------|--|--|--|------------|--|------|--|
| | | | | | | | | | | | | | | | |





The Boutet Co.

65%
SUBMITTAL

Department of
Parks and
Recreation

Municipality of
Anchorage





PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CHESTER CREEK TRAIL

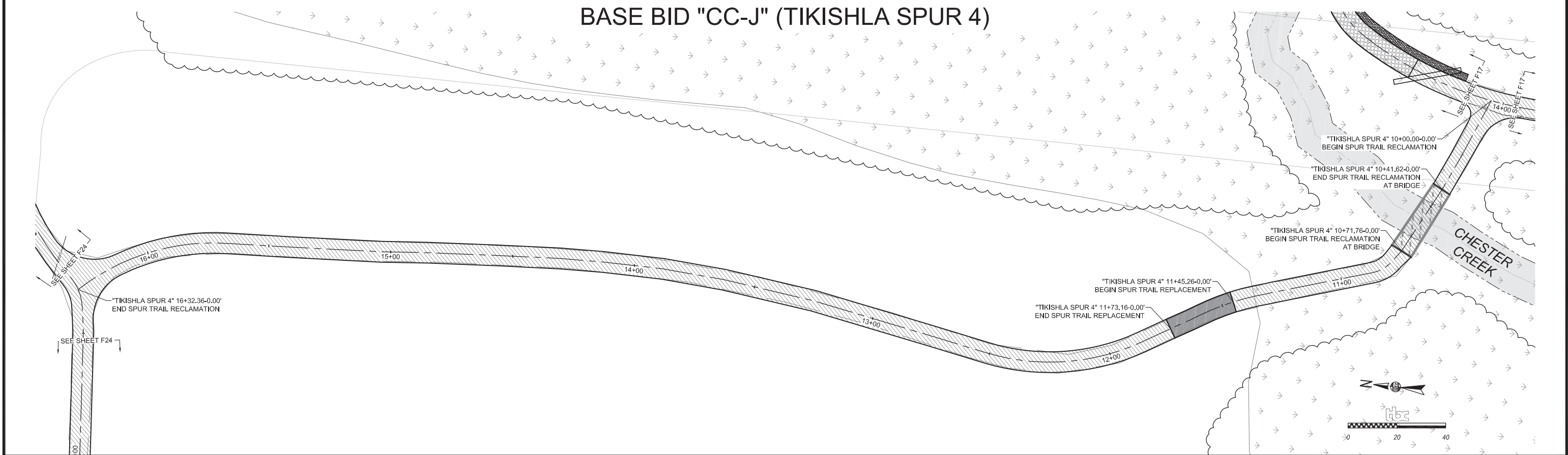
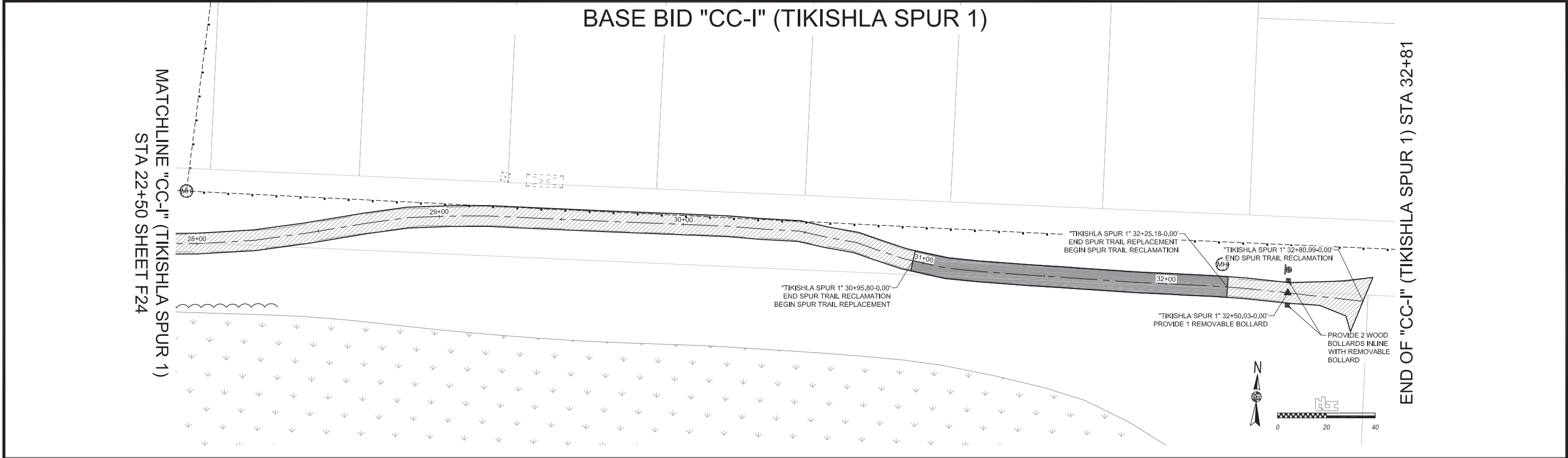
"CC-H" & "CC-I"

SCALE: HOR. PER SHEET
VER. PER SHEET


DATE: 12/15/2014
STATUS: 65% PS&E

GRID: N/A

SHEET: F24 of F25



| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---------------------|--|--|--|--|--|--|--|--|--|--|--|-----------------|----------|-------|------------|------------|------------|-----------|----------------|------------|-----|------------------|-------------|------------|-----|--|--|------|--|--|--|
| FIELD BOOKS | | | | | | | | | | | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | | | | | | |
| DESIGN: | | | | | | | | | | | | | | | BASE | TBC | | TELEPHONE | --- | | | | | | | | | | | | |
| STAKING: | | | | | | | | | | | | | | | TOPOGRAPHY | TBC | | ELECTRIC | --- | | | | | | | | | | | | |
| ASBUILT: | | | | | | | | | | | | | | | PROFILE | TBC | | CABLE TV | --- | | | | | | | | | | | | |
| CONTRACTOR: | | | | | | | | | | | | BASIS OF DATUM: | | | | --- | | | SANITARY SEWER | --- | | TRAFFIC SIGNAL | --- | | | | | | | | |
| INSPECTOR: | | | | | | | | | | | | | | | | --- | | | STORM SEWER | --- | | DESIGN | TBC | | | | | | | | |
| CONSTRUCTION RECORD | | | | | | | | | | | | VERTICAL DATUM | | | | --- | | | WATER | --- | | QUANTITIES | TBC | | | | | | | | |
| | | | | | | | | | | | | | | | | --- | | | GAS | --- | | MUN. FINAL CHECK | | | | | | | | | |
| | | | | | | | | | | | | | | | | PLAN CHECK | | | | REVISIONS | | | | CONSULTANT | | | | SEAL | | | |




The Boutet Co.


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SUBMITTAL

Department of
Parks and
Recreation

Healthy
Parks



Healthy
People



MUNICIPALITY OF ANCHORAGE

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS

CHESTER CREEK TRAIL

"CC-I" & "CC-J"

SCALE

HOR. PER SHEET

VER. PER SHEET

DATE: 12/15/2014

GRID: N/A

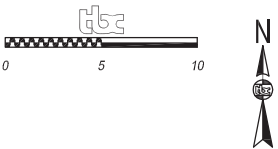
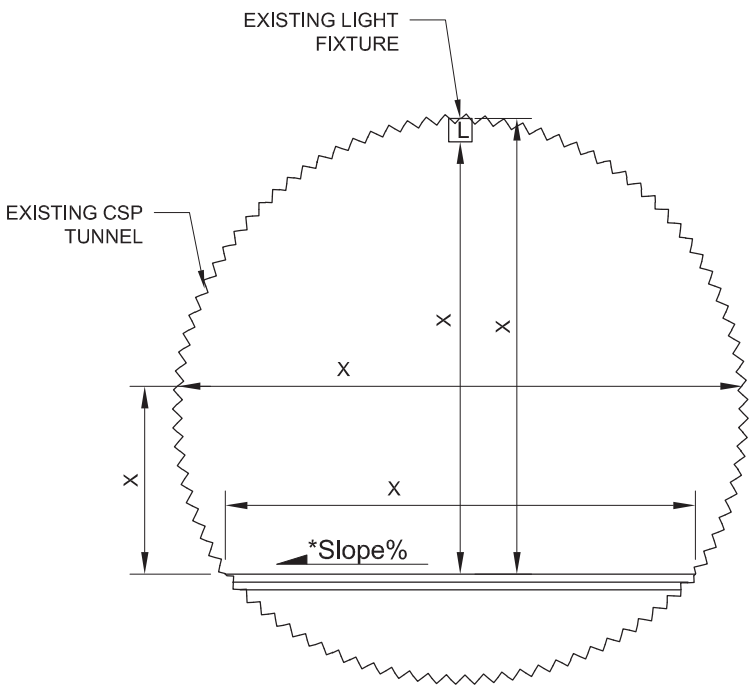
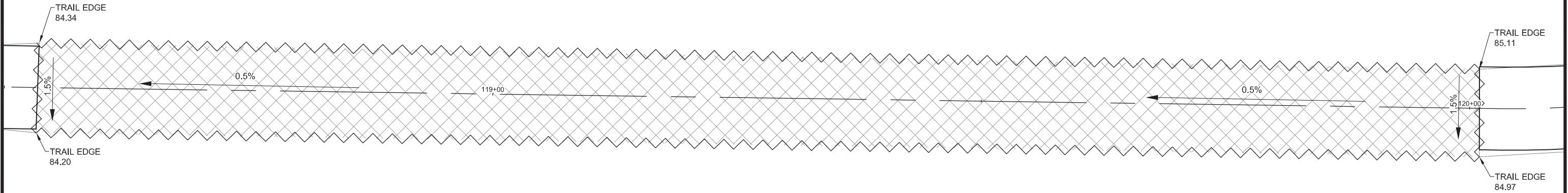
STATUS: 65% PS&E

SHEET

F25

of

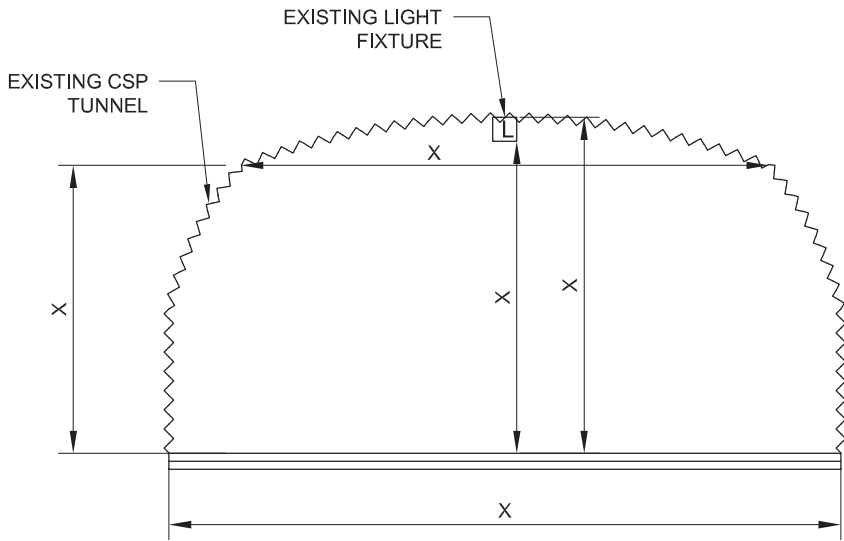
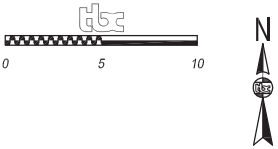
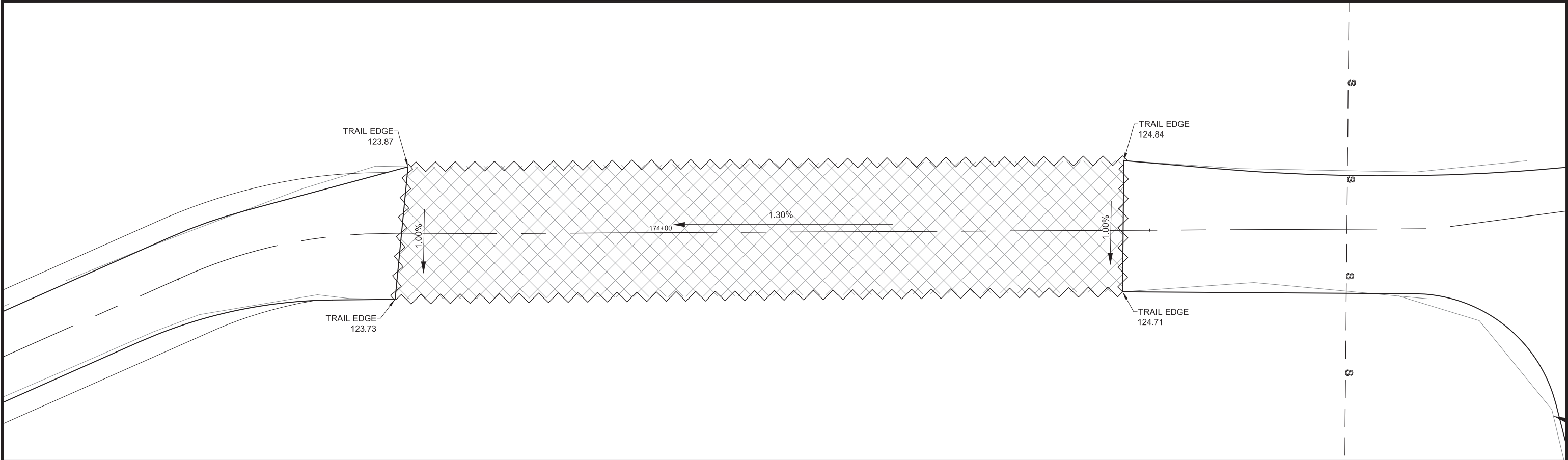
F25



NEW SEWARD TUNNEL CLEARANCES

N.T.S.

| | | | | | | | | | | | | | | | | | | | | | |
|---------------------|--|-----------------|----------|-------|----------------|----------|------------|------------------|----------|------------|-----|------|------------------|--|------------|-------|--|--------------------------------------|-----------|----------------|--|
| | | | | | | | | | | | | | 65% SUBMITTAL | Department of Parks and Recreation Municipality of Anchorage | | | PARKS AND RECREATION DEPARTMENT | | | | |
| | | | | | | | | | | | | | | | | | 2015 CHESTER CREEK TRAIL IMPROVEMENTS NEW SEWARD HIGHWAY TUNNEL | | | | |
| FIELD BOOKS | | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV | SCALE | HOR. PER SHEET VER. PER SHEET | DATE: 12/15/2014 STATUS: 65% PS&E | GRID: N/A | SHEET T1 of T2 | |
| DESIGN: | | | | | BASE | TBC | | TELEPHONE | -- | | | | | | | | | | | | |
| STAKING: | | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | | | | | | | |
| | | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | | | | | | | |
| ASBUILT: | | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | | | | | | | |
| CONTRACTOR: | | BASIS OF DATUM: | | | STORM SEWER | -- | | DESIGN | TBC | | | | | | | | | | | | |
| INSPECTOR: | | | | | WATER | -- | | QUANTITIES | TBC | | | | | | | | | | | | |
| CONSTRUCTION RECORD | | VERTICAL DATUM | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | | | | | | | |
| | | | | | PLAN CHECK | | | | | REVISIONS | | | | | CONSULTANT | | SEAL | | | | |



LAKE OTIS TUNNEL CLEARANCES
N.T.S.

| FIELD BOOKS | BM NO. | LOCATION | ELEV. | DATA | DRAWN BY | CHECKED BY | DATA | DRAWN BY | CHECKED BY | REV | DATE | DESCRIPTION | BY | REV |
|---------------------|-----------------|----------|-------|----------------|----------|------------|------------------|----------|------------|-----------|------|-------------|----|-----|
| DESIGN: | | | | BASE | TBC | | TELEPHONE | -- | | | | | | |
| | | | | TOPOGRAPHY | TBC | | ELECTRIC | -- | | | | | | |
| STAKING: | | | | PROFILE | TBC | | CABLE TV | -- | | | | | | |
| | | | | SANITARY SEWER | -- | | TRAFFIC SIGNAL | -- | | | | | | |
| ASBUILT: | | | | STORM SEWER | -- | | DESIGN | | TBC | | | | | |
| CONTRACTOR: | BASIS OF DATUM: | | | WATER | -- | | QUANTITIES | TBC | | | | | | |
| INSPECTOR: | | | | GAS | -- | | MUN. FINAL CHECK | | | | | | | |
| CONSTRUCTION RECORD | VERTICAL DATUM | | | PLAN CHECK | | | | | | REVISIONS | | | | |



The Boutet Co.

65%
SUBMITTAL

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Parks and
Recreation

Healthy
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MUNICIPALITY OF ANCHORAGE

PARKS AND RECREATION DEPARTMENT

2015 CHESTER CREEK TRAIL IMPROVEMENTS
LAKE OTIS BLVD. TUNNEL

SCALE

| | | |
|----------------|------------------|-----------|
| HOR. PER SHEET | DATE: 12/15/2014 | GRID: N/A |
| VER. PER SHEET | STATUS: 65% PS&E | |

SHEET

T2 of T2